



**PLANNING COMMISSION  
STAFF REPORT**

June 20, 2024

AGENDA ITEM: Planning File No. **MAJ-23-0011**, a final review of a Major Site Plan application for a new mixed-use/affordable housing development.

LOCATION: 101 W Main Street / LOT B-1 WEST FRISCO 70 SUB # 2

ZONING: Planned Unit Development (PUD)

APPLICANT & ARCHITECT: Andy Stabile  
Allen-Guerra Architecture  
PO Box 5540  
Frisco, CO 80443

OWNER: NHPF WEST MAIN LLC  
C/O NHP Foundation  
122 E 42<sup>nd</sup> Street, Suite 4900  
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TOWN STAFF: Katie Kent, Community Development Director  
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**PROJECT DESCRIPTION**

The applicant, Allen-Guerra Architecture, representing NHPF WEST MAIN LLC, is proposing a new, mixed-use/affordable housing development at 101 West Main Street. The project involves:

- 52 deed restricted residential dwelling units
- 1,546 square feet of leasable nonresidential space

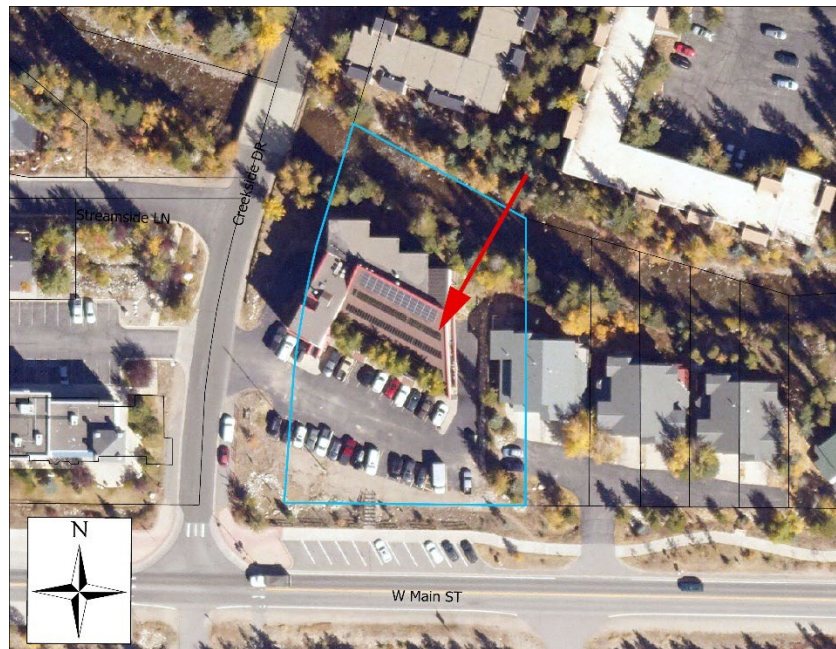
The site plan consists of a single building fronting Main Street with surface parking behind the building and access off Creekside Drive. The building footprint is 14,086 square feet and totals approximately 39,800 square feet of finished area. The proposed development will consist of 52 deed restricted rental units, with all units being affordable for households earning between 30 and 80 percent of the Area Median Income (AMI).

**BACKGROUND**

The subject property is a 0.83-acre lot located on the northeast corner of West Main Street and Creekside Drive. The existing site consists of the Westmain Professional Building, which was built in 1979. The site is zoned Planned Unit Development (PUD) with the adjacent properties to the west and east zoned Mixed-Use (MU) and the property to the north zoned Residential Medium

Density. There are a variety of surrounding uses including but not limited to residential, office/retail, and restaurant users. The site has Ten Mile Creek on the north side of the property and is across Main Street from the Summit County RecPath.

The NHP Foundation bought the site in June 2023 and in partnership with the Town of Frisco, is seeking funding opportunities to develop the site. Below is a vicinity map of the subject property with an aerial photography base layer. The location of the property lines shown on this map vary in accuracy and should only be used for reference purposes.



Vicinity Map

In February 2024, the Frisco Town Council approved a rezoning application for the site that changed the zoning from MU to PUD. A PUD is a zoning document that outlines what uses and development standards are permitted on a property, which may differ from the underlying zone district. Where the PUD is silent, the underlying zoning comes into effect. Pursuant to Town Code §180-2.4.1, rezoning requests are subject to two public hearings at the Planning Commission where the Planning Commission makes a recommendation to the Town Council. After such recommendation, the Town Council shall consider the rezoning ordinance at a public hearing where Town Council shall approve, approve with conditions, or deny the application.

The Planning Commission held a preliminary public hearing on [November 16, 2023](#) to consider the proposed PUD plan in conjunction with a submitted sketch plan for the property. The applicant received feedback from the Planning Commission and revised the proposed PUD Plan. The Planning Commission held a final public hearing [January 4, 2024](#) where the Planning Commission recommended conditional approval of the application to Town Council. The Town Council held three public hearings for the rezoning request on [January 23, 2024](#), [February 13, 2024](#), and [February 27, 2024](#). The Town Council heard public comments, consisting of but not limited to the following:

- Concerns with the number of offices being displaced and asked for assistance from the Town to relocate.

- Concerns with the reduced setback between the building to the east of the site and a request to require the setback along Main Street be in line with the existing multifamily development east of the site.
- Concerns with the limited parking.
- Request to maintain trees within the ROW where additional ROW parking is proposed to be constructed.

For a complete record of all public comments, please utilize the links above.

## FINAL MAJOR SITE PLAN REVIEW

The Major Site Plan application requires Planning Commission review and decision. The proposal is reviewed in detail for conformance with the Frisco Community Plan and compliance with the Frisco Unified Development Code (UDC) and recorded Planned Unit Development (PUD).

## ANALYSIS - FRISCO COMMUNITY PLAN

The following elements of the Frisco Community Plan are applicable to the review of the proposed development:

### Vision and Guiding Principles (excerpts)

*The vision and guiding principles are a statement of community values. Together, they reflect characteristics that residents value about Frisco today, and the kind of community that residents would like to see Frisco become as it continues to grow and evolve over time. The vision and guiding principles serve as an organizing framework for subsequent chapters and policy guidance in the Community Plan, as well as for the Town Council's Strategic Plan.*

### **Guiding Principle 1: Inclusive Community**

*Frisco cares about our neighbors, visitors, and the whole of our community. We are an inclusive community that welcomes people of all backgrounds and income levels. We support a balance of housing options to create opportunities for a diverse population to reside here. Our history is integral to our identity and it is also a guiding principle for our future. As the Town grows and changes, we need to be rooted in the values of our past. The Ten Mile Range mountain backdrop, historic structures, vibrant neighborhoods, and a lively Main Street characterize Frisco along with the friendly people and welcoming vibe. As Frisco grows, this character and identity should be preserved and enhanced throughout Town.*

#### **1.1: Protect the character and livability of Frisco's residential neighborhoods**

- **1.1A** *Ensure new housing complements adjacent properties and neighborhoods through appropriate mass, scale, and design. See page 68 for Area Specific Policies/Design Principles to help encourage compatible neighborhood development.*
- **1.1B** *Invest in targeted improvements (e.g., trail connections, bike paths, sidewalks, and drainage improvements) that enhance the safety and quality of life of residents.*
- **1.1C** *Strive to create an appropriate balance of full-time residents, second homes, and short-term rentals to maintain a diverse and vibrant community.*

#### **1.4: Reinforce Main Street as the heart of the Community, from the lake to the mountains**

- **1.4A Support the creation of an attractive community entrance along West Main Street to create a gateway that is inviting to visitors, highlights the Tenmile Creek, and is an extension of the history and vision of Frisco.**
- *1.4B Encourage infill and redevelopment that complements the character, scale, and massing of historic structures.*
- **1.4C Ensure ground floor uses on Main Street create a balanced mix of uses that support a thriving commercial atmosphere. Housing may be located above the ground floor along Main Street, to help support the vitality of downtown.**
- **1.4D Preserve Frisco’s historic street grid to retain the character and walkability of the town core.**
- **1.4E Design and orient buildings to maintain historic development patterns along public rights of way.**

**Guiding Principle 2: Thriving Economy**

*The Frisco economy is built upon a unique balance of tourism and its role as a commercial-service hub for the region. Tourism, driven by recreational opportunities, and the small, mountain town appeal of Main Street, creates jobs and revenue. Locals and visitors utilize the large retailers, grocery stores, and services located along Summit Boulevard, and support the small businesses on Main Street. While the Frisco economy has seen steady growth in the past decade, diversification of the economy is important. More year-round opportunities will provide stability through the seasons and economic downturns. The Town should focus on small, incremental changes that preserve the town character and a healthy small business community that attracts residents and visitors.*

**2.1: Maintain a diverse and strong economy**

- **2.1A Continue to attract and retain businesses that support and enhance Frisco’s tourism revenues, while also seeking to build upon entrepreneurship and new or emerging niches.**
- **2.1B Provide opportunities for a balanced mix of housing and services to support local businesses, employees, residents and visitors.**
- **2.1C As development and infill occurs, ensure that the Town’s overall mix of land uses remains aligned with community goals.**
- *2.1D Participate in regional and state economic development efforts that support a diverse economy.*
- *2.1E Support the creation of home-based businesses and remote workers.*

The proposed application furthers the highlighted statements above. The applicant is proposing a mixed-use development that includes leasable, non-residential commercial space along with multifamily residential space. While residential is the primary proposed use of the site with all units being deed restricted rentals, the site will also provide 1,546 square feet of commercial area.

The commercial uses are proposed on the ground level facing West Main Street and will have direct access to the sidewalk along West Main Street. These uses will help support local employees, residents, and visitors. Additionally, the Community Plan specifically references reinforcing Main Street as the heart of the community along with encouraging infill and redevelopment. The proposed design of the site moves the building closer to West Main Street and will contribute to the streetscape and pedestrian-friendly corridor.

The proposed development includes a building design that is unique to this project but that is also reflective of the architectural elements and styling of other buildings in Frisco. This application

facilitates the construction of new residential units that add variety to the sizes and types of homes in this area of Frisco. The residential dwelling units range from 481 to 882 square feet.

The proposed construction of 52 deed restricted, residential dwelling units on the subject property is permitted through the Affordable Housing Development Incentive Program and PUD designation in the Unified Development Code. The application is in general conformance with the purpose and recommendations of the Frisco Community Plan.

**ANALYSIS:  
PLANNED UNIT DEVELOPMENT**

The zoning of this site is PUD and follows the standards described in the approved and recorded PUD document, see attached. Where the PUD is silent, the underlying zone district, Mixed-Use (MU) and the Affordable Housing Development Incentive Program (AHDIP), comes into effect.

**Purpose:** The purpose of the PUD is as follows:

*“This Designation is proposed to establish a mixed-use development that blends into the surrounding Town of Frisco. The residential portion of this development will consist entirely of deed-restricted, Affordable Workforce Housing units as defined in Section C.2 of the PUD Designation. The two primary goals of the development are to provide affordable housing for the local workforce and to ensure compatibility with the surrounding neighborhood.”*

The proposed development provides a mix of residential and commercial uses, with all residential units being provided at rents affordable to households earning between 30 to 80% Area Median Income (AMI). The surrounding area consists of residential and mixed-use development, making the development compatible with the area. The application meets the purpose and intent of the PUD.

**Maximum Density:** Per the PUD, a minimum of 45 Affordable Workforce Housing units serving up to 80% of the AMI are required. The proposed site plan shows 52 residential units being provided, utilizing a mix of studios, one-bedrooms, and two-bedrooms rented at rates affordable to households earning between 30 to 80% AMI. Below is a chart outlining the proposed unit count and the proposed affordability levels. The application meets this standard.

Unit Type	Total Units	Units up to 30% AMI	Units up to 50% AMI	Units up to 60% AMI	Units up to 80% AMI
Studio	24	2	11	3	8
1 Bedroom	17	2	8	1	6
2 Bedroom	11	1	5	1	4
Total	52 units	5	24	5	18

**Minimum Lot Area:** None required.

**Minimum Lot Frontage:** There is no minimum lot frontage required.

**Minimum Open Space:** The PUD is silent on minimum open space and so the property shall comply with the MU District requirement. There is a minimum open space requirement of 10% of ground floor area. The application meets this requirement.

**Maximum lot coverage:** The PUD states that the maximum lot coverage shall not exceed 80%. The overall site is 36,248 square feet making the maximum lot coverage permitted 28,998 square feet. The proposed lot coverage is 21,641 square feet, meeting the requirements.

**Setbacks:** The required and proposed setbacks for the site are as follows:

	<b>Required</b>	<b>Proposed</b>
<b>Main Street</b>	3 feet	3 feet
<b>Side</b>	5 feet	5 feet
<b>Rear (Wetland Setback)</b>	25 feet	25 feet

Due to the proximity of the Ten Mile Creek and delineated wetlands, the site is required to have a 25-foot setback from the wetlands on site. The wetland boundary is determined by the ordinary high-water mark (OHWM) of the waterbody as shown on the water survey prepared by a wetlands consultant. This line is shown as “25’ wetland setback” on the attached plans. Further description of water quality protection is later in this staff report.

The application meets all setback requirements.

**Maximum Building Height:** The maximum height outlined in the PUD is 49.5 feet for pitched roofs and 38.5 feet for flat roofs. The applicant is proposing a roof height of 45.05 feet for pitched roofs and 35.12 feet for flat roofs, meeting the requirements.

The applicant is proposing to measure building height from the proposed finished grade rather than using the existing grade per the exception listed in §180-9.3 of the UDC. The exception states that:

*“If fill must be placed on the site for drainage purposes, or to meet some other similar requirement such as the placement of utility services, building height may be measured from the finished grade within three feet of the location of the foundation. However, for the purposes of calculating the building height, the finished grade elevation cannot exceed any adjacent, paved street or alley elevation. If additional fill is needed above any adjacent right-of-way elevation, then the average right-of-way elevation as measured at the edge of the adjacent asphalt shall be used as the finished grade to determine building height. In the case where a building is proposed adjacent to two or more rights-of-way, then the building height will be calculated using the average grade of each right-of-way as measured at the edge of adjacent asphalt. For the purposes of calculating building height, any placement of fill on a lot must be determined as necessary, showing there are no other viable alternatives. Said determination and showing must be verified by a professional engineer licensed in the State of Colorado and approved by the Frisco Town Engineer.”*

The project was determined to meet the exemption above since the existing site grades are lower than the adjacent roads, properties, and riverbank. These existing grades would have resulted in a site without positive drainage, ponding of water, and increased flooding risk. The applicant has provided a letter from a licensed engineer, describing the need for fill and the absence of a viable alternative, which was approved by the Town Engineer per the memo dated May 21, 2024.

The PUD also states that the first-floor ceiling height shall be a minimum of 9 feet in height. The application meets this standard.

The application meets all height requirements. An Improvement Location Certificate (ILC) will be required during construction to ensure the structure meets the height requirements.

**District Standards:** For properties with frontage on Main Street, on-site parking shall not be located along the part of the property abutting Main Street. 180-3.12.2.B.1. The applicant is proposing a parking lot behind the building and away from Main Street, meeting the requirements.

**ANALYSIS – USE STANDARDS PER PUD**

**Permitted Uses:** The PUD states the following permitted uses on site:

Planning Area 1 – Mixed Use	
Permitted Uses	<p>Residential Uses:</p> <ol style="list-style-type: none"> <li>1. Permitted as a primary use</li> <li>2. Permitted on the ground floor</li> </ol> <p>Commercial Uses:</p> <ol style="list-style-type: none"> <li>3. Offices</li> <li>4. Personal Services</li> <li>5. Recreation and Entertainment</li> <li>6. Retail</li> <li>7. Arts &amp; Entertainment Facility</li> <li>8. Radio &amp; Television Broadcasting</li> </ol> <p>Public or Quasi-Public:</p> <ol style="list-style-type: none"> <li>9. Community Facilities</li> <li>10. Civic, Youth, Social Organization</li> <li>11. Medical Clinic</li> </ol> <p>Recreation:</p> <ol style="list-style-type: none"> <li>12. Community Center</li> <li>13. Health Spa</li> <li>14. Open Space</li> <li>15. Trails</li> <li>16. Community Gardens</li> <li>17. Shared Public Space</li> </ol>

Additionally, the PUD states that ground floor residential shall be permitted within the PUD and that there shall be a minimum of 1,500 square feet of commercial space on site. The applicant is proposing 1,546 square feet of commercial space.

The application meets the permitted uses as outlined in the PUD.

**ANALYSIS - DEVELOPMENT STANDARDS [180-6]**

**Site Grading and Drainage Plan** (§180-6.5 and §180-6.6): The Town Engineer has reviewed the submitted grading and drainage plans and has provided a memo of approval, see attached. The memo outlines some outstanding comments that have since been addressed. When building plans are submitted, the Town Engineer will review the plans again to ensure grading and drainage is functional and in compliance with all requirements.

**Water Quality Protection** (§180-6.7): Development and soil disturbance are generally prohibited in or within 25 feet of all waterbodies and wetlands. The site is adjacent to Ten Mile Creek and consists of delineated wetlands. The applicant has submitted a waterbody survey prepared by a licensed wetland consultant that shows the wetland boundary and the required 25-foot non-disturbance setback.

The existing building on site currently sits within the 25-foot non-disturbance setback. The applicant is proposing to demo the existing building and therefore approval of a wetland

disturbance application is required prior to issuance of a demolition permit. The applicant has submitted the wetland disturbance application and it is pending review from the Town. The new proposed building is outside of the non-disturbance setback. There is one proposed encroachment within the water quality setback:

- The installation of a water quality rain garden. The proposed water quality pond is designed to detain up to a 25-year (24 hour) storm runoff event. The proposed water quality pond was reviewed and approved by the Public Works Director/Town Engineer per memo dated May 21, 2024.

During previous reviews, Staff found additional encroachments but after the 25' water quality setback was clearly shown on submitted plans, it has been verified that the only encroachment is the above stated water quality pond. The proposed encroachment has been reviewed and approved by the Town Engineer. The application meets the approval requirements per §180-6.7.

**Access** (§180-6.11): All vehicle access shall comply with the standards set forth in Chapter 155, Minimum Street Design and Access Criteria. Where development abuts a Town road, the location and design of access points to the road must be approved by the Frisco Public Works Director. Commercial/Business projects shall have a driveway width of twelve (12) feet for one-way and twenty-four (24) feet for two-way. Additionally, the UDC states the following requirements for non-vehicular access:

#### 180-6.11.2, Non-vehicular Access Requirements

*It is the purpose of this section to promote the use of non-vehicular modes of transportation through a Town- wide network of connecting non-vehicular pathways and provide safe access year round. All site plans shall provide for and show non-vehicular access in accordance with the standards set forth in the Frisco Trails Master Plan and Chapter 155, Minimum Street Design and Access Criteria. In addition, all non-vehicular access shall meet the following standards:*

- A. All multi-family, mixed-use, non-residential developments, and residential subdivisions shall provide safe and convenient non-vehicular access to a public street or road year-round. Developments shall install paved, year round access from and through the development to adjacent public sidewalks, bicycle and pedestrian facilities, or right of way both existing and proposed pursuant to the Frisco Trails Master Plan and in accordance with the Standards of Chapter 155, Minimum Street Design and Access Criteria.*
- B. Every principal structure shall provide access to adjacent trail systems or public open space usable for recreation activities.*
- C. Developments shall integrate pedestrian ways, trails, and/or bicycle paths with similar existing and planned facilities on adjacent properties. The Frisco Trails Master Plan should be used as a reference when planning for the integration of these facilities.*

The application materials show one access off Creekside Drive measuring 24 feet, meeting the requirements.

The application materials also show internal sidewalks and connections to the public right-of-way, meeting the requirements for mixed-use developments.

**Traffic Study** (§180-6.12): Frisco Town Code requires a traffic study, prepared by a professional engineer licensed in the State of Colorado, be submitted for any large project that:



- A. Requires a conditional use or rezoning approval;
- B. Is located adjacent to either Main Street or Summit Boulevard;
- C. Contains only one point of access;
- D. Contains an access point off an unimproved roadway or unincorporated area;
- E. Contains an access point off a road with a service level of D or F; or
- F. Is expected to generate 400 or more daily trips per day.

A large project is defined in Frisco Town Code, 180-9.3 as:

*Any commercial or mixed-use project, occurring on a lot of 10,500 square feet or greater or occurring on a group of lots combined for a unified development project which contain a total lot area of 10,500 square feet or greater; or any residential development occurring on a lot of 21,000 square feet or greater or any development of 5 or more dwelling units.*

A traffic study has been submitted and reviewed for this project. The project is anticipated to generate an increase of 223 vehicles per day for a total of 409 anticipated vehicles per day. While this is an increase in traffic, the traffic study states that all adjacent intersections are anticipated to operate at an acceptable level of service through 2045.

The study analyzed the current site access and the proposed site access in relation to the West Main Street and Creekside Drive intersection.



The application meets this requirement.

**On-Premise Parking Requirements** (§180-5.5.1 C): The PUD outlines the following parking requirements:

- 0.5 parking space per unit for studios and 1-bedroom units
- 1 parking space per unit for 2-bedroom units
- 2 parking spaces per unit for 3 bedrooms or greater

- 1 visitor parking space required per 7 dwelling units
- On-street parking. Required parking, excluding ADA and handicap accessible spaces, may be accommodated on streets proximate to the property, on a one-for-one basis, subject to construction of any needed improvements, and Town approval of an acceptable agreement to ensure adequate maintenance and snow removal procedures and a permit system for resident use.
- Car-sharing service. Each car-sharing space provided shall count as four parking spaces, up to 20% of the parking requirement. The car-sharing program details and agreement shall be provided as part of the major site plan application and shall include provisions and alternative options to ensure operation for the duration of the project.
- Parking may be accommodated on streets proximate to the property on a one-for-one basis.

The application materials show 24 studio units, 17 1-bedroom units, and 11 2-bedroom units. With the proposed number of units, the residential parking calculations are as follows:

Unit Type	Number of Units	Parking Per PUD Requirements
Studios	24	12
1-Bedroom	17	8.5
2-Bedroom	11	11
Visitor Spaces		7.4
<b>Total</b>	52 Units	<b>38.9 or 39 Parking Spaces</b>

In addition to the residential parking spaces, the project is required to provide parking for the nonresidential space. Per the UDC, office parking in the MU zone district is required at a rate of one (1) space per every 450 square feet. Using the standard UDC office parking requirements, the site plan requires 3.4 office parking spaces, or 3 spaces. Adding that requirement to the parking per the PUD Plan, the total required parking for the site is **42 spaces**.

As stated in the PUD Plan, the applicant is proposing to utilize on-street parking and car sharing parking to meet the parking requirements of the PUD Plan. The applicant is proposing 21 spaces on site (which includes the “bonus spaces” for the two car-sharing spaces) and is proposing to construct 31 on-street parking spaces. The application materials show **52 parking spaces**, meeting the requirements.

The additional parking spaces constructed within the public ROW will act like all other public parking spaces along Main Street.

Per PUD requirements, there is an Operating Agreement in place with the Town that allows overnight parking in the public right-of-way. The Public Works Director has reviewed and approved the parking management plan per the memo dated May 21, 2024.

**Electric Vehicle Charging Stations:** Chapter 65 of the Code of Ordinances of the Town of Frisco concerning Building Construction and Housing Standards, Section C405.10.1 references electric vehicle charging stations for new construction. The property will be required to comply with requirements as outlined in Section C405.10.1 at the time of building permit submittal.

**Accessible Parking Space Requirements:** With the required parking totaling 42 spaces, the required number of accessible parking spaces is two (2) spaces. The application meets this standard.

**Bicycle Parking** (§180-6.13.4): All mixed-use developments shall provide bicycle parking facilities, in an appropriate location, with bicycle spaces in the amount of not less than 20% of the total number of parking spaces required for the project, with a minimum of five (5) bicycle spaces.

The applicant is showing enclosed bike storage facilities inside the building and outdoor bike racks. The PUD states that bicycle parking calculations shall follow mixed-use and nonresidential bicycle parking requirements as outlined in the Unified Development Code at the time of project construction. The application meets this standard.

**Tandem Parking** (§180-6.13.4): The applicant is not proposing any tandem parking spaces.

**Snow Storage Areas** (§180-6.13.7): Per the PUD, snow storage shall be provided on site at the rate identified in the UDC, which is 25%. The PUD allows snow storage within 15 and 25 feet from the 100-year floodplain with engineer approval. The applicant is not proposing to utilize this allowance. The application materials show 7,375 square feet of paved surfaces, requiring 1,844 square feet of required snow storage area. The application materials show 1,875 square feet of snow storage, meeting the requirements.

**Landscaping and Revegetation** (§180-6.14): The landscaping requirements listed in the PUD are as follows:

- a. Plant material quantities may be reduced by up to 20% from the requirement per §180-6.14.3.
- b. Species mix may be increased to a 50% maximum for each species, and minimum tree caliper size may be reduced to 50% of the caliper size requirement in §180-6.14.4.

For commercial and mixed-use large projects, for every 1,500 square feet of project lot area, a minimum of one (1) tree shall be planted on site and one (1) shrub shall be required for every 2,500 square feet of project lot area. Based on the size of the site (36,248 square feet) and the above standards, the project is required to have nineteen (19) trees and eleven (11) shrubs.

	Per UDC: MU Large Project	20% Reduction per PUD
<b>Trees</b>	24	<b>19</b>
<b>Shrubs</b>	14	<b>11</b>

The application is meeting the required trees and shrubs on site. Additionally, they are proposing to plant trees and shrubs within the right-of-way to allow for more screening of the site. The Town is agreeable to the proposed plantings within a ROW; however, a Revocable License Agreement will be required at time of building permit submittal.

**Refuse Management** (§180-6.17): All commercial, mixed-use and multi-family residential development projects containing five (5) or more units shall utilize dumpster enclosures for the storage of all refuse, recycling, grease traps, and compost collection. The application materials show a trash enclosure in an accessible location and is proposed to have materials consistent with the primary structure on site. The application meets this requirement.

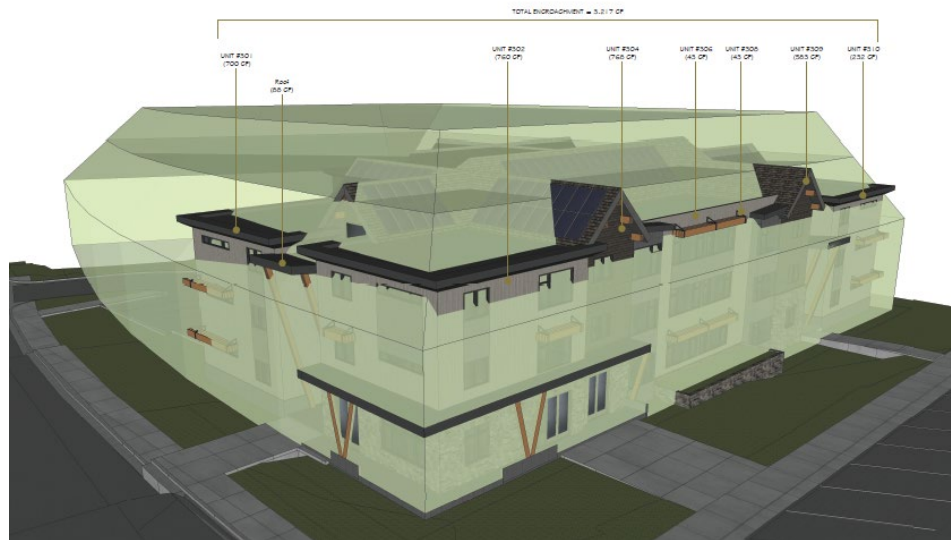
**Design Guidelines:** The PUD outlines specific design guidelines for the development. For a complete list of the criteria, please refer to the attached PUD. Below is an analysis of each section as indicated in the PUD.

A. Façade Standards

All building elevations shall use at least four of the indicated techniques to employ varied articulation of wall surfaces. Each building façade shows offsets and insets to add depth to the wall plane, a change in material, variation in roof planes or forms, and building elements that provide shelter from natural elements. The application meets this standard.

## B. Bulk Plane Standards

Per the PUD, bulk plane encroachments shall be allowed 1,000 cubic feet per 10,000 square feet of lot area. With a site of 36,248 square feet, that would allow 3,625 cubic feet of encroachment. The applicant is proposing 3,217 cubic feet of bulk plane encroachment. The PUD also states that bulk plane encroachments are permitted for livable spaces. The application meets this standard.



## C. Roof Standards

The PUD requires that flat roofs be augmented with pitched roof elements and that a minimum roof pitch of 6:12 is encouraged. The application materials show flat roofs and pitched elements with a pitch of 12:12, meeting the standards.

## D. Roof Design

The PUD requires that roof lines shall be designed in a manner where they do not substantially deposit snow onto required parking areas, sidewalks, or entryways. The application meets this standard.

## E. Roof Materials

The applicant is proposing asphalt shingle roofing in a charcoal color, meeting the requirements of the PUD.

## F. Building Material Standards

The applicant is proposing a variety of building materials including horizontal siding in a coffee color, vertical siding in terra bronze, accent metal with a matte finish, dry stacked tone veneer, and exposed timbers. All materials meet the PUD requirements.

## PUBLIC COMMENT

The Community Development Department has not received any formal public comments concerning this project as of June 11, 2024.

## STAFF RECOMMENDATIONS

### Recommended Findings

Should the Planning Commission determine that the application meets the criteria of approval pertaining to the Major Site Plan application for the proposed mixed-use/affordable housing development at 101 West Main Street / LOT B-1 WEST FRISCO 70 SUB # 2, staff recommends the following findings:

*Based upon the review of the Staff Report dated June 20, 2024, and the evidence and testimony presented, the Planning Commission finds:*

- 1. The site plan application is in general conformance with the principles and policies in the Frisco Community Plan, and residential development of this lot is supported by the Frisco Community Plan Guiding Principles of Inclusive Community and Thriving Economy. The proposed development includes a building design that is unique to this project but that is also reflective of the architectural elements and styling of other buildings in Frisco. The location and orientation of this building creates a design that enhances the overall character of the community. This application facilitates the construction of new, affordable residential units that add variety to the sizes and types of homes in this neighborhood.*
- 2. The site plan application is in general conformance with the recorded Planned Unit Development associated with 101 W Main Street since all the applicable requirements have been met by the submittal: including lot area, lot frontage, setbacks, building height, density, lot coverage requirements, and bulk plane encroachment requirements.*
- 3. The site plan application is in general conformance with the recorded Planned Unit Development associated with 101 W Main Street; specifically the Design Guidelines section because all applicable requirements have been met by the submittal; including: that the development meets the façade standards and varied articulation requirements, the bulk plane standards are met, roof standards and materials are met, and that the building materials meet the materials required per the PUD.*
- 4. The site plan application is in general conformance with the recorded Planned Unit Development associated with 101 W Main Street; specifically the parking calculations and requirements, landscaping, and snow storage meet the requirements as stated in the PUD.*

### **Recommended Action**

*Based upon the findings above, Staff recommends APPROVAL of the proposed Major Site Plan application for the proposed multi-use development consisting of 52 deed restricted residential units and 1,546 square feet of commercial space located at 101 W Main Street / LOT B-1 WEST FRISCO 70 SUB # 2, subject to the following conditions:*

- 1. Prior to issuance of a building permit, the applicant shall satisfy the conditions of Frisco Public Works and the Town Engineer.*
- 2. Prior to issuance of a building permit, the applicant shall apply for, and be issued, the required revocable license agreement for landscaping within the ROW.*
- 3. Prior to issuance of a building permit, the applicant shall satisfy requirements from Xcel Energy, Frisco Sanitation, Summit Fire & EMS, and Summit County.*

### **Recommended Motion**

Should the Planning Commission choose to approve this Major Site Plan application, the Community Development Department recommends the following motion:

***With respect to File No. MAJ-23-0011, I move that the recommended findings set forth in the June 20, 2024 staff report be made and that the recommended conditions set forth therein be taken and that the Planning Commission hereby APPROVES the request for the Major Site Plan application for the proposed multi-use development consisting of 52 deed restricted residential units and 1,546 square feet of commercial space located at 101 W Main Street / LOT B-1 WEST FRISCO 70 SUB # 2.***

### **ATTACHMENTS**

- Attachment 1: Planned Unit Development
- Attachment 2: Applicant Narrative
- Attachment 3: AMI Levels
- Attachment 4: Referral Comments
- Attachment 5: Applicant's Response Letter
- Attachment 6: Site Plan
- Attachment 7: Traffic Study

cc: Allen-Guerra Architecture

**TOWN OF FRISCO  
COUNTY OF SUMMIT  
STATE OF COLORADO  
ORDINANCE 24-01**

AN ORDINANCE REZONING FROM THE MIXED-USE (MU) ZONE DISTRICT TO PLANNED UNIT DEVELOPMENT (PUD) AT CERTAIN REAL PROPERTY LOCATED AT 101 WEST MAIN STREET AND LEGALLY DESCRIBED AS LOT B-1, WEST FRISCO 70 SUBDIVISION # 2.

WHEREAS, the Town of Frisco, Colorado (“Town”) is a home rule municipality, duly organized and existing under Article XX of the Colorado Constitution; and

WHEREAS, this ordinance is adopted pursuant to the Town’s home rule authority, Section 1-4 of the Charter for the Town of Frisco, the Town’s authority under Colorado Revised Statutes Section 31-23-301, and the authority of the Town Council under Section 180-2.4.1D. of the Code of Ordinances of the Town (“Code”); and

WHEREAS, the owner of certain real property located at 101 W Main Street legally described as LOT B-1 WEST FRISCO 70 SUBDIVISION # 2 (hereinafter the “Property”) has made application to the Town for a changes of zoning classification from Mixed-Use (MU) Zone District to Planned Unit Development (PUD); and

WHEREAS, pursuant to the provisions of Section 180-2.4.1.D.3.a. of the Code, on November 16, 2023, the Frisco Planning Commission conducted a preliminary public hearing concerning the owner’s application for the rezoning and made preliminary comments on the application; and

WHEREAS, pursuant to the provisions of Section 180-2.4.1.D.3.b. of the Code, on January 4, 2024, the Frisco Planning Commission conducted a final public hearing concerning the owner’s application for the rezoning and made a recommendation for approval with conditions of the application; and

WHEREAS, the Town Council held a public hearing on January 23, 2024 and February 13, 2024, to receive public comment, evidence and testimony relative to the proposed amendments to the Frisco Town Code; and

NOW, THEREFORE, BE IT ORDAINED BY THE TOWN COUNCIL OF THE TOWN OF FRISCO THAT:

Section 1. Findings. Based on the testimony and documents received and considered at the public hearing on this matter, the Town Council FINDS THAT:

- A. The rezoning is necessary to provide land for a community benefit that includes an increased density of affordable housing units and is in promotion of public health, safety, and welfare.
- B. That the project is beneficial or necessary for the economic development of Frisco by supporting the development of housing that will be available to the local workforce and therefore supports local businesses, employees, and residents.
- C. That the application is providing a community benefit in the form of workforce housing.
- D. That the application achieves a compatibility of land uses with neighboring land uses by listing a variety of permitted uses within the PUD that are consistent with the underlying zoning of the site including residential and office uses.

- E. That the modifications to the underlying zoning are in the best interest of the Town, and neighborhood in which the development is planned by furthering the goals within the guiding principles of the Frisco Community Plan. This includes providing opportunities for a balanced mix of housing and to maintain a diverse and vibrant community.
- F. That the projected capacity to fully serve the project site with water and sewer is available, which has been reviewed and approved by the Town Engineer and will be furthered reviewed by the Public Works Department and Frisco Sanitation upon subsequent site plan submittal.
- G. That Town services shall be provided in the most efficient manner practicable.
- H. That more than one housing type and housing price is offered to satisfy the needs of more than one segment of the community and the PUD includes requirements for a range of pricing based on Average Median Income level.
- I. That the project contributes amenities to the development itself and to the community at large by providing a 25-foot-wide non-exclusive public walkaway easement dedicated to the Town of Frisco that creates public access to Ten Mile Creek and by providing a car sharing program for residents.
- J. That an owners association will be established with the development of the project which will promote a sense of community and ensure the continued existence of a viable entity responsible for maintenance of private open space and other similar duties.
- K. That the project meets all the applicable requirements of this Chapter that are not expressly varied in the final PUD plan, contributes to design aesthetics and layout, and promotes efficient use of land by providing a PUD plan that is clear and consistent with the Frisco Community Plan.

Section 2. Designation. The real property located at 101 W Main Street legally described as LOT B-1 WEST FRISCO 70 SUBDIVISION # 2, is hereby designated as being designated as a Planned Unit Development (PUD) in accordance with the PUD Plan that is attached hereto.

Section 3. Zoning Map. The Director of the Community Development Department shall cause the Frisco Zoning Map to be amended to reflect the PUD designation approved by this ordinance, and shall cause the PUD Plan to be filed in the real property records for Summit County, Colorado.

Section 4. Effective Date. This ordinance shall become effective in accordance with the home-rule Charter of the Town of Frisco, Colorado.

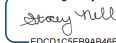
INTRODUCED, PASSED ON FIRST READING AND PUBLICATION AND POSTING ORDERED THE 13<sup>TH</sup> DAY OF FEBRUARY 2024.


ADOPTED ON SECOND AND FINAL READING AND PUBLICATION BY TITLE ORDERED THIS 27<sup>TH</sup> DAY OF FEBRUARY 2024.



TOWN OF FRISCO, COLORADO:

ATTEST:

DocuSigned by:  
  
 Stacey Nell, Town Clerk

DocuSigned by:  
  
 Rick Ihnken, Mayor Pro Tem





1330940

Taryn Power - Summit County Recorder

### 101 W Main Housing Planned Unit Development

This Planned Unit Development Designation, to be known as the 101 W Main Housing Planned Unit Development, hereinafter referred to as the (“Designation”), was approved this day of 2024, by the Town Council of Frisco, Colorado, for certain real property located in Frisco, Lot B-1, amended West Frisco 70, Filing No. 2, according to the plat filed April 16, 1974 under Reception NO. 140796, County of Summit, State of Colorado and described in attached Exhibit A, hereinafter referred to as the “Property”. This Designation establishes the land uses that shall be permitted on the Property, a Conceptual Development Plan (“Plan”) and development guidelines and conditions, which must be adhered to by the owner/developer of the Property, NHPF West Main, LLC, hereinafter referred to as the “Owner/Developer” and successors and assigns of the Owner/Developer. This Designation also specifies improvements which must be made and conditions which must be fulfilled in conjunction with this designation by the Owner/Developer.

Where this Designation does not address a specific development standard or requirement of the Frisco Unified Development Code (“UDC”) currently in effect, the Development Code shall apply. Where the Designation addresses a specific development standard or requirement, the provisions of this Designation shall supersede the provisions of the Unified Development Code. Use and development of the Property shall be in accordance with the specific requirements of this Designation, and in substantial compliance with the Plan attached as Exhibit B. This Designation is intended to further the objectives of blending into the development pattern of the adjacent areas and increasing the supply of affordable workforce housing for the community.

#### A. Permitted Uses and Development Plan

##### 1. Purpose and Intent

This Designation is proposed to establish a mixed-use development that blends into the surrounding Town of Frisco. The residential portion of this development will consist entirely of deed-restricted, Affordable Workforce Housing units as defined in Section C.2 of this PUD Designation. The two primary goals of the development are to provide affordable housing for the local workforce and to ensure compatibility with the surrounding neighborhood. The exceptions to current Unified Development Code requirements proposed here are necessary to allow for the construction and operation of at least 45 deed-restricted affordable units, serving households up to 80% of the area median income. Minor deviations from this area median income will be allowed up to 100% of the area median income as outlined in the Development Agreement between the Town of Frisco and the NHP Foundation, but no modification to this Designation shall be required. More specifically, this development will further the following goals and Guiding Principles as reflected in the Town of Frisco Community Plan.

- 1.4: Reinforce Main Street as the heart of the community, from the lake to the mountains.
  - 1.4D: Preserve Frisco’s historic street grid to retain the character and walkability of the town core.
  - 1.4E: Design and orient buildings to maintain historic development patterns along public rights of way
- 1.5: Promote a balanced community through support for diverse and equitable housing options.

- 1.5A: Develop a diverse portfolio of workforce housing programs to support a broad range of housing needs—type of unit, rental vs. ownership, and price points—at the community and neighborhood level.
- 1.5B: Stimulate the creation of workforce housing through town policies, incentives, and regulatory procedures.
- 1.5C: Collaborate with community partners and the private sector to expand the reach of Frisco’s workforce housing efforts.
- 2.1: Maintain a diverse and strong economy.
  - 2.1B: Provide opportunities for a balanced mix of housing and services to support local businesses, employees, residents, and visitors.
- 2.3 Facilitate the retention and expansion of locally owned businesses.
  - 2.3C: Expand workforce housing options to support small businesses and local employees.
- 4.1: Provide a safe and efficient multimodal transportation system.
  - 4.1D: Manage parking resources downtown to promote the efficient use of available spaces.
- 5.1: Provide high quality, year-round recreational amenities that appeal to both residents and visitors.
  - 5.1D: Coordinate new facilities with the Trails Master Plan (and other Town Master Plans) to ensure appropriate connections and access.

## 2. Development Plan and Permitted Uses

- a. Development Plan: development of the Property shall be in general accordance with the associated Site Plan application and the following specific requirements of this Designation. There is one (1) Planning Area on the property as specified below, which encompasses all property within Lot B-1 West Frisco 70 Sub #2:

Permitted Uses for Planning Area 1 Include:

1. At least forty-five (45) Units of Affordable Workforce Housing.
  - a. Minor deviations from the 52-unit configuration shown on Exhibit B may be considered as part of future Site Plan Review, but no modification to this Designation shall be required.
  - b. Ground floor residential uses shall be permitted within this Designation.
  - c. The commercial space on-site shall be a minimum of 1,500 square feet.
  - d. Affordable Housing incentives per the Affordable Housing Incentive Program shall be applied in accordance with Section 180-5.5 of the Unified Development Code with exceptions as stated in this Planned Unit Development Designation.
- b. Permitted Uses: Uses which are permitted or accessory within the Mixed Use (MU) Zoning District, as set forth in Figure 1 and shall be calculated pursuant to the methodologies and procedures set forth in the Unified Development Code.

Figure 1 Permitted Use table is meant to be inclusive rather than exhaustive. If a use that is not listed in the table below meets the intent of the permitted uses listed, the use can be permitted at the discretion of the Community Development Director.

**Figure 1**

<b>Planning Area 1 – Mixed Use</b>	
Permitted Uses	<p>Residential Uses:</p> <ol style="list-style-type: none"> <li>1. Permitted as a primary use</li> <li>2. Permitted on the ground floor</li> </ol> <p>Commercial Uses:</p> <ol style="list-style-type: none"> <li>3. Offices</li> <li>4. Personal Services</li> <li>5. Recreation and Entertainment</li> <li>6. Retail</li> <li>7. Arts &amp; Entertainment Facility</li> <li>8. Radio &amp; Television Broadcasting</li> </ol> <p>Public or Quasi-Public:</p> <ol style="list-style-type: none"> <li>9. Community Facilities</li> <li>10. Civic, Youth, Social Organization</li> <li>11. Medical Clinic</li> </ol> <p>Recreation:</p> <ol style="list-style-type: none"> <li>12. Community Center</li> <li>13. Health Spa</li> <li>14. Open Space</li> <li>15. Trails</li> <li>16. Community Gardens</li> <li>17. Shared Public Space</li> </ol>

3. Supplements:

- a. Exhibit A – Site Plan
- b. Exhibit B – Housing Covenant
- c. Exhibit C – Parking Management
- d. Exhibit D – Car Share

**B. Development Standards**

1. Building Height: all structures shall be a maximum of 45 feet in height for pitched roofs, and 35 feet in height for flat roofs as defined in the Unified Development Code.
  - a. A ten percent increase in maximum building height as defined by the Mixed Use Zone in the Unified Development Code at the time of project construction shall be permitted for this Designation.
  - b. First floor building height for structure(s) shall be 9’ minimum in height.

2. Setbacks:
  - a. 5' from side property lines
  - b. 3' from front property lines along Main Street
  
3. Lot Coverage: maximum lot coverage shall not exceed 80%. Lot coverage calculation shall be evaluated per the definition of lot coverage in the Unified Development Code.
  
4. Landscaping: landscaping shall be in general accordance with the Site Plan and shall follow Town Code with the following exceptions:
  - a. Plant material quantities may be reduced by up to 20 percent from the Mixed-Use Development as outlined in the Unified Development Code at the time of project construction.
  - b. Species mix may be increased to a 50-percent maximum for each species, and minimum tree caliper size may be reduced to 50 percent of the caliper size requirement as outlined in the Unified Development Code at the time of project construction.
  
5. Parking Calculations and Requirements. Vehicular parking, on-site and off-site, shall be constructed and maintained in general accordance with the Site Plan and On-Street Parking Plan.
  - a. Residential Parking Requirement: one-half parking space per unit for studios and one-bedroom units, one parking space per unit for two-bedroom units, and two parking spaces per unit for three bedrooms or greater. One visitor parking space is required per seven dwelling units.
  - b. On-Street Parking: required parking, excluding ADA and handicap accessible spaces, may be accommodated on streets proximate to the property, on a one-for-one basis. Refer to *Section 8. Dedications and Other Agreements*, for description and approval timeline in "On-Street Parking Plan" for approval and management of on-street parking.
  - c. Car-Sharing Service: each car-sharing space provided shall count as four parking spaces, up to 20 percent of the parking residential parking requirement. Refer to *Section 8. Dedications and Other Agreements*, for the description and approval timeline for "Car Share Management" for approval, management and enforcement of car-sharing.
  
6. Bicycle Parking: Calculations for bicycle parking shall follow mixed-use and nonresidential bicycle parking requirements as outlined in the Unified Development Code at the time of the project construction.
  
7. Public Walkway Easement for Access to Ten Mile Creek. A 25' wide non-exclusive public walkway easement dedicated to the Town of Frisco shall be provided for future installation of trail, rec path or other improvement by the Town of Frisco. Refer to *Section 8. Dedications and Other Agreements*, "Public Walkway Easement" for description and approval timeline. Refer to Exhibit A for proposed easement location.

8. **Snow Storage.** Snow storage shall be accommodated at the rate per the Unified Development Code and shall be provided on-site. No snow storage or detention facilities will be allowed within 15' of the 100 year floodplain as shown on Exhibit A as approved at the time of approval. The Town Engineer may approve snow storage, detention, and water quality facilities between 15' and 25' from the 100 year floodplain. Owner/Developer shall comply with any other conditions required by the Town Engineer for improvements within the 25' setback from the 100 year floodplain.
9. **Design Guidelines:** The overall goal is to complement the surrounding neighborhood and to minimize and mitigate physical and visual impacts of the development. The development is subject to the following design guidelines.
  - A. **Façade Standard:**
    - i. **Intent:** To ensure that the façade design of development is compatible with Frisco's small mountain town character and provides a human scale to enhance the walking experience in the neighborhood.
    - ii. **Building Elements:** All building elevations shall employ varied articulation of wall surfaces. Each façade shall be articulated using at least four of the following techniques:
      - Deep eaves or overhangs, at least 24 inches in depth;
      - Balconies, porches, or patios;
      - Building elements that provide shelter from natural elements;
      - Offsets, insets, bays, or other similar architectural features to add a variety of depths to the wall plane;
      - A change in texture or material, provided all exterior wall textures and materials are consistent with the overall architectural style of the building;
      - Variation in roof planes or roof forms, including dormer or gables.
  - B. **Bulk Plane Standards:**
    - i) Bulk plane encroachments shall be allowed up to 1,000 cubic feet per 10,000 square feet of lot area.
    - ii) Bulk plane encroachments shall be allowed for livable/ habitable spaces.
  - C. **Roof Standards:**
    - i. **Intent:** to ensure that roof elements are compatible with or complementary to existing historic or contributing buildings in the area and to encourage visibly pitched roofs or roof elements and the use of dormers and breaks in ridgelines.
    - ii. **Roof Pitch:**
      - pitched roofs, or flat roofs augmented with pitched roof elements, are required
      - A minimum roof pitch of 6/12 is encouraged
      - Mansard roofs are prohibited.
  - D. **Roof Design:** Roof lines shall be designed in a manner where they do not substantially deposit snow onto required parking areas, sidewalks, trash storage areas, stairways, decks, balconies or entryways.

- E. Roof Materials:
- i. If metal roofs are used they shall be surfaced with a low-gloss finish, matte finish, or other finish proven to fade and not be reflective
  - ii. Metal roofs, asphalt and fiberglass shingles are permitted provided that they provide substantial relief and shadow, and the design and color are compatible with the building
  - iii. Bright colored roofs that exceed a chroma of four on the Munsell Color chart are prohibited
- F. Building Material Standards:
- i. Intent: to ensure that building materials are compatible and complementary to existing historic and contributing buildings in the area, using a combination of mainly natural materials
  - ii. Primary Materials:
    - Building materials shall be predominantly natural, including, but not limited to, wood siding, wood shakes, logs, stone, brick, or other similar materials
    - Other materials that imitate natural materials are also acceptable provided their texture, shape, and size are substantially similar to the natural materials they are imitating, and are not obviously artificial materials
    - Stucco or steel are acceptable materials when used in combination with other acceptable materials
  - iii. Specific Material Standards
    - Concrete Block: concrete block shall not be allowed as the primary or extensive exterior finish. When used as an accent, concrete block shall be a split block, or other similarly shaped, textured, and colored materials that are found to be compatible with the building and the purpose of this section
    - Metal: metal shall have a matte finish or a finish proven to fade and not be reflective
    - Glass: the use of mirrored or reflective glass is prohibited unless required for compliance with the voluntary green building program as administered by the Town's Building Official
    - A variety of materials shall be used on all building elevations
    - There shall be a variety of quality and type of exterior materials, and their application shall be generally in balance and proportional on all elevations of the building
    - Materials that wrap around the building, such as a durable material at the base of the structure, shall continue around projecting outside exterior corners and end at recessed inside exterior corners
- G. Accessory Structures: the same or similar building materials shall be used on main structures and any accessory structures located on the same site, unless an alternative material can be provided that will complement the project and which meets the other standards of this section.
- i. Additions: additions that are 50 percent or less of the existing building floor area, or exterior remodels or renovations, may be allowed to complement the

existing structure, even if the building does not currently meet the material standards of this section

H. Building Colors:

- i. Intent: to promote building colors compatible with the site and surrounding buildings
- ii. Maximum Color Chroma: no color may be used as the primary color of the building that exceeds a chroma of four on the Munsell Color chart. Pure white or black may not be utilized as the primary building color.
- iii. Exception for Building Accents: colors that exceed a chroma of four, but that do not exceed a chroma of eight on the Munsell Color chart may be used only sparingly as accents, such as on trim or railings. Luminescent, fluorescent, or reflective colors shall not be utilized on any exterior portion of the building.

10. Dedications and Other Agreements

Note: Changes to the recorded Operating Agreement shall be approved by the Town and re-recorded. No modification or amendment to this Designation shall be required.

Agreements	Description	Approval Timeline
<u>Public Walkway Easement</u>	- Provide access easement for future installation of trail, rec path or other improvement by Town of Frisco	- Record approved easement prior to certificate of occupancy
On-Street Parking Plan	- Provide specified parking spaces within Town Right of Way - Provide operating agreement	- On-street parking spaces finalized during Site Plan Approval - Operating Agreement to be recorded prior to certificate of occupancy
Car Share Management	- Provide operating agreement. - Acknowledge conditions of enforcement	- Operating Agreement to be recorded prior to certificate of occupancy.

C. **Workforce Housing Specific Regulations.**

- 1. This PUD Designation mandates the development of a minimum of 45 Affordable Workforce Housing Units.
- 2. Affordable Workforce Housing, as such term is specifically contemplated in this Designation, and for development purposes addressed herein, shall assume the meaning of the term Affordable Housing set forth in the Unified Development Code with the income restrictions as described in Section A.1. of this Designation.
- 3. The Units are planned as multi-family units as shown conceptually in the associated Major Site Plan. Nevertheless, the actual design and size of such units are subject to the physical conditions of the Property and market demands, and modifications to the design and nature of the Units may

be proposed provided that the level of Affordable Workforce Housing contemplated herein is reasonably maintained, in terms of the number of units, occupancy, amenities, et cetera. A final determination of the number of bedrooms and bathrooms for each unit will be made at Final Major Site Plan approval. Town approval shall be obtained for any deviation from the anticipated unit design or configuration, but no modification to this PUD Designation shall be required if the modifications are within 10% of the proposed unit mix.

4. Units shall be offered for rent, in accordance with the standards for such Units articulated in the attached Covenant (Exhibit B).
5. Renters approved for residence in this Planned Unit Development shall meet the income qualification requirements as enumerated in the Covenant (Exhibit B). The rental price of Units shall not exceed affordability limits established in the Covenant.
6. Owner/Developer may install the infrastructure for the entire Property at one time, or in phases, as is most advisable based on demand, site conditions, and prevailing construction conditions, provided that any such installation is conducted in accordance with this Designation and the Frisco Town Code.

#### **D. Utilities and Improvements.**

1. Road and Driveway Access:
  - A. The Property shall be accessed from Creekside Drive.
  - B. Road and driveway access is depicted on the Site Plan (Exhibit A). All road and driveway construction on the Property shall be in general accordance with such Plan, and the standards of the Frisco Town Code.
2. Right-of-Way and Easement Dedication:
  - A. All additional required easements for access, snow storage, utilities and maintenance will also be dedicated on the Plat, to the satisfaction of the Town of Frisco Public Works and Community Development Departments.
3. Sidewalks:
  - A. Concrete sidewalk to be constructed along Creekside Drive.
  - B. Main Street sidewalk to be realigned for new parking.
4. Water Systems: the water supply shall be provided by the Town of Frisco for all development on the Property.
5. Wastewater Disposal: wastewater disposal for all development shall be provided by the Frisco Sanitation District.
6. Fire Protection and Wildfire Hazard Mitigation: the Property is located within the jurisdiction of the Summit Fire & EMS. All development on the Property shall meet all fire protection requirements of the District.

#### **E. Implementation**



1. **Development Phasing.** The Owner/Developer intends to build the project in one phase. Although, the Owner/Developer may develop the project in phases, if advisable based on demand, site conditions, and prevailing construction conditions; provided that a proposed phasing plan is reviewed and approved by the Town in conjunction with the Site Plan Review.

## **E. General Provisions**

1. **Enforcement:** the provisions of this Designation and the dedication relating to the use of land shall run in favor of the Town of Frisco and shall be enforceable at law or in equity by the Town of Frisco without limitations on any power or regulation otherwise granted by law. Other provisions of this Designation and the Plan shall run in favor of the residents, occupants, or landowners of the Property, but only to the extent expressly provided in, and in accordance with the terms of this Designation and the Plan. Provisions not expressly stated as running in favor of the residents, occupants or owners of the Property shall run in favor of the Town of Frisco.
2. **Breach of Provisions:** if at any time, any provision or requirements stated in this Designation have been breached by the Owner/Developer, the Town may withhold approval of any or all subdivision plats, or the issuance of any or all grading or building permits or occupancy permits applied for on the Property, until such breach has been remedied; provided, however, that the Town shall not take affirmative action on the account of such breach until it shall have first notified the Owner/Developer in writing and afforded the Owner/Developer a reasonable opportunity to remedy the same.
3. **Binding Effect:** this Designation shall run with the land and be binding upon the Owner/Developer, their respective successors, representatives and assigns, and all persons who may hereafter acquire an interest in the Property or any part thereof, with the exception that provisions of this Designation may be modified through an amendment in accordance with the procedure stated in the Development Code. This Designation shall be recorded in order to put prospective purchasers or other interested persons on notice as to the terms contained herein.
4. **Amendments:** the Unified Development Code includes procedures and requirements for review of all Planned Unit Developments. The Owner/Developer shall be on notice of these requirements and their potential impact should modifications to this Designation be desired. Amendments to the provisions of this Designation shall be reviewed and acted upon as a rezoning application, subject to the Town's procedures for zoning amendments and the requirements for findings under the Planned Unit Development Act of 1972 at CRS section 24-67-106(3)(b), unless such amendment is determined to be minor in nature in accordance with the provisions outlined in the Unified Development Code.
5. **Notices:** all notices required by this Designation shall be in writing and shall be either hand-delivered or sent by certified mail, return receipt requested, postage pre-paid, as follows:

Notice to Town:

Town of Frisco  
Post Office Box 4100  
Frisco, Colorado 80443

Notice to /Owner/Developer:

NHP Foundation  
Attn: Neal Drobenare  
1090 Vermont Avenue, NW, Suite 400  
Washington, DC 20005

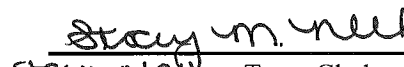
6. Entire Designation: this Designation contains all provisions and requirements incumbent upon the Owner/Developer relative to the 101 W Main Planned Unit Development, except as modified by subsequent action of the Town Council in accordance with the procedures set forth in the Unified Development Code and the Colorado Planned Unit Development Act (CRS Section 24-67-106) for amending planned unit developments, and except that nothing contained herein shall be construed as waiving any requirements of the Unified Development Code or other regulations otherwise applicable to the development of the Property.
7. Effective Date: to be legally effective and binding, this Designation must be recorded by the Summit County Clerk and Recorder. The date of such recording is referred to herein as the "Effective Date."
8. Legality of Provisions: in the case one or more of the provisions contained in this Designation, or any application thereof, shall be invalid, illegal, or unenforceable in any respect, the validity, legality, and enforceability of the remaining provisions contained in this Designation and the application thereof shall not in any way be affect or impaired thereby.

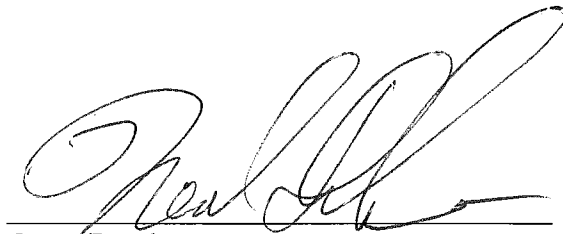
IN WITNESS WHEREOF, the Town and the Owner/Developers have executed this Designation as of the date first written above.

TOWN OF FRISCO, COLORADO

  
\_\_\_\_\_, Mayor  
Town of Frisco

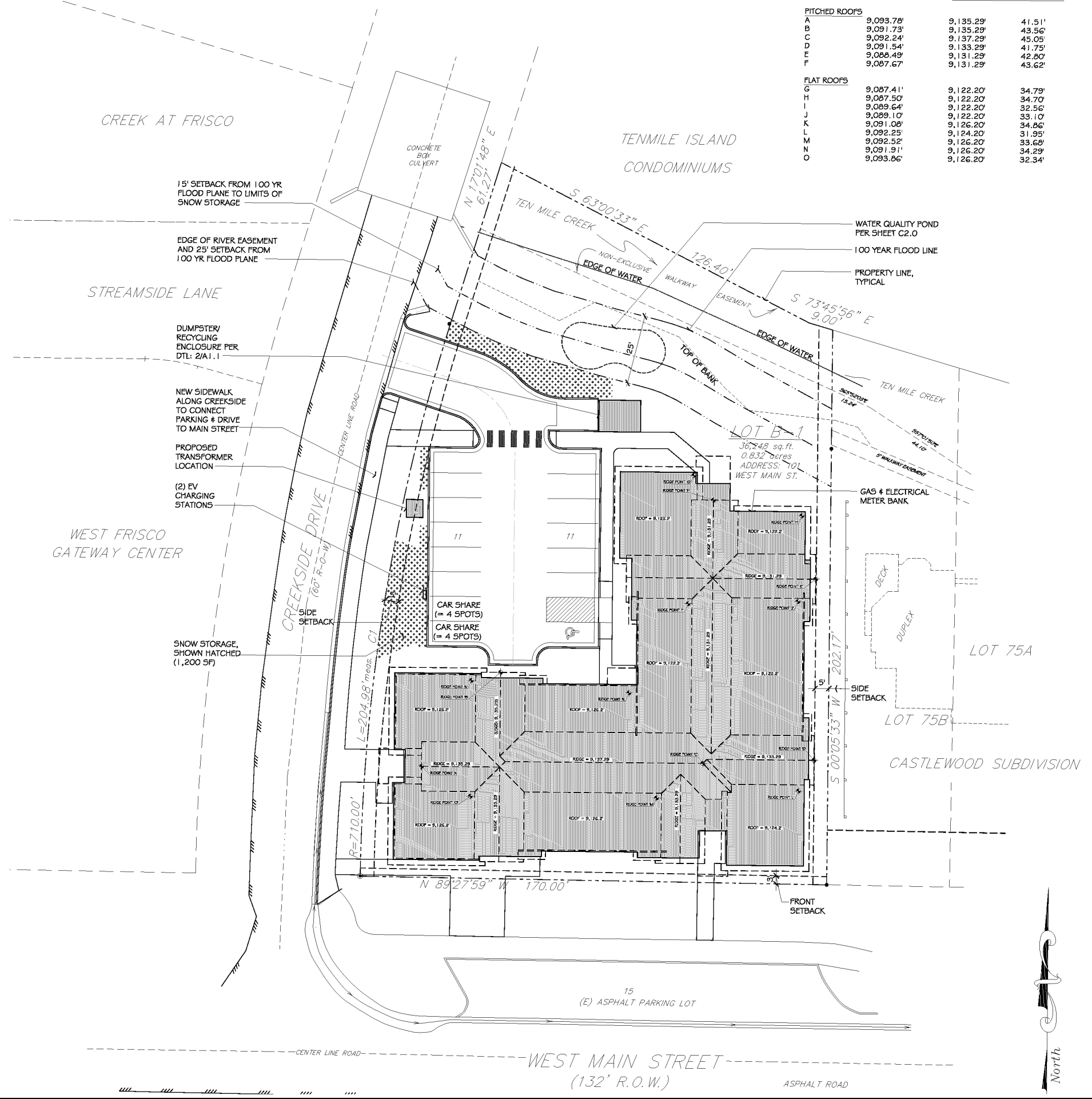
ATTEST:

  
\_\_\_\_\_, Town Clerk

  
\_\_\_\_\_  
Owner/Developer

**PROJECT SITE**  
 LOT B-1, AMENDED WEST  
 FRISCO 70 . FILING #2  
 101 WEST MAIN ST

PITCHED ROOFS			
A	9,093.78'	9,135.29'	41.51'
B	9,091.73'	9,135.29'	43.56'
C	9,092.24'	9,137.29'	45.05'
D	9,091.54'	9,133.29'	41.75'
E	9,088.49'	9,131.29'	42.80'
F	9,087.67'	9,131.29'	43.62'
FLAT ROOFS			
G	9,087.41'	9,122.20'	34.79'
H	9,087.50'	9,122.20'	34.70'
I	9,089.64'	9,122.20'	32.56'
J	9,089.10'	9,122.20'	33.10'
K	9,091.08'	9,126.20'	34.86'
L	9,092.25'	9,124.20'	31.95'
M	9,092.52'	9,126.20'	33.68'
N	9,091.91'	9,126.20'	34.29'
O	9,093.86'	9,126.20'	32.34'



**101 W MAIN | EXHIBIT A - SITE PLAN**

DECEMBER 2023

Notes:  
 1. Graphic provided for illustrative purposes only  
 and subject to change.

*Insert Project Name*

Record and Return to:

Colorado Housing and Finance Authority  
 1981 Blake Street  
 Denver, CO 80202  
 Attention: Legal Operations

**LOW-INCOME HOUSING TAX CREDIT  
 LAND USE RESTRICTION AGREEMENT**

THIS LAND USE RESTRICTION AGREEMENT (this "Agreement"), dated as of \_\_\_\_\_, 2023, is by and between \_\_\_\_\_, a Colorado \_\_\_\_, and its successors and assigns (the "Owner"), and the Colorado Housing and Finance Authority, a body corporate and political subdivision of the State of Colorado (the "Authority").

RECITALS:

A. The Authority was designated by the Governor of the State of Colorado (the "State") as the housing credit agency for the State for the allocation of low-income housing tax credits under Section 42 of the Internal Revenue Code of 1986, as amended (the "Code"), and the Treasury Regulations thereunder (the "Regulations").

B. The Owner applied to the Authority for an allocation of [4%][9%] federal low-income housing tax credits to the Project (the "Federal Credits").

C. The Owner is the owner of a ( ) unit rental housing development located on lands in the City of \_\_\_\_\_, County of \_\_\_\_\_, State of Colorado, as legally described in Exhibit A hereto commonly known as \_\_\_\_\_ (the "Project").

D. In connection with its Application (defined below) the Owner made certain representations to the Authority about the Project, including representations as to the number of Tax Credit Units (defined below) and the term of occupancy restrictions, upon which representations the Authority relied on in considering the Application for a reservation and allocation of Federal Credits.

E. The Code requires the Owner to execute and deliver this Agreement and that it be recorded in the official land records of the county in which the Project is located to create covenants running with the land for the purpose of enforcing the Code and Regulations; and certain additional undertakings of the Owner in connection with its Application by regulating and restricting the use and occupancy of the Project as set forth herein.

F. The Authority is unwilling to allocate Federal Credits to the Project unless the Owner enters into this Agreement, and consents to being regulated by the Authority so that the Authority may enforce the occupancy restrictions and other covenants, and terms and conditions of this Agreement in accordance with the Code and the Regulations.

G. The Owner, under this Agreement, intends, declares and covenants that the regulatory and restrictive covenants set forth herein governing the use, occupancy and transfer of the Project shall be and are covenants running with the Project land for the Term (defined below) and binding upon all subsequent owners of the Project for such Term.

NOW, THEREFORE, in consideration of the mutual promises and covenants hereinafter set forth, and of other valuable consideration, the Owner and the Authority agree as follows:

1. Incorporation of Recitals. The recitals above are incorporated in and made a part of this Agreement.
2. Definitions. Unless otherwise expressly provided herein or unless the context clearly requires otherwise, the terms defined above shall have the meanings set forth above and the following terms shall have the respective meanings set forth below for the purposes hereof, and all words and phrases defined in Section 42 of the Code shall have the same meanings in this Agreement:
  - (a) "AMI" means Area Median Gross Income, as defined below.
  - (b) "Applicable Fraction" means the applicable fraction as defined in Section 42(c)(1)(B) of the Code.
  - (c) "Application" means the application submitted to the Authority by the Owner for a reservation, initial determination, or allocation of Federal Credits for the Project. The Application includes all materials provided to the Authority, as required by the applicable checklist, and any additional materials provided to the Authority that are reviewed as part of the application process.
  - (d) "Area Median Gross Income" means the median gross income of the area in which the Project is located as determined by the Secretary for purposes of Section 42 of the Code, including adjustments for family size.
  - (e) "Compliance Manual" means the Authority's Multifamily Program Compliance Manual, as amended from time to time.
  - (f) "Compliance Period" has the same meaning as defined in Section 42(i)(1) of the Code.
  - (g) "Credit Period" means, with respect to the Federal Credit, the "credit period" as defined in Section 42(f) of the Code.
  - (h) "Eligible Basis" means the eligible basis as defined in Section 42(d) of the Code.
  - (i) "Extended Use Period" means for each building that is a part of the Project a period of [30/40] consecutive taxable years beginning on the first day in the Compliance Period on which the building is part of a qualified low-income housing project and ending on the date that is [15/25 years] after the close of the Compliance Period; provided that in no event shall the Extended Use Period be shorter than the Extended Use Period as defined in Section 42(d) of the Code.
  - (j) "IRS" means the Internal Revenue Service.
  - (k) "Materially Participate" has the same meaning set forth in Section 469(h) of the Code.
  - (l) "Qualified Low-Income Building" means a "qualified low-income building" as defined in Section 42(c)(2) of the Code.

- (m) **"Qualified Low-Income Housing Project"** means a "qualified low-income housing project" as defined in Section 42(g)(1) of the Code.
- (n) **"Qualified Nonprofit Organization"** means a qualified nonprofit organization as defined in Section 42(h)(5)(C) of the Code.
- (o) **"Qualifying Tenant"** means a tenant that meets the income requirements of Paragraph 7(b).
- (p) **"Rent Restricted"** a unit is "rent restricted" if the gross rent with respect to such unit does not exceed thirty percent (30%) of the imputed income limitation applicable to such unit as set forth in Paragraph 7(b), all as determined in accordance with Section 42(g)(2) of the Code.
- (q) **"Secretary"** means the Secretary of the United States Department of the Treasury.
- (r) **"Tax Credit Units"** means the rent-restricted units identified in Sections 7(a) and (b).
- (s) **"Term"** means the Term of this Agreement, as defined in Section 5(a).
- (t) **"Tenant Income Certification"** means the certification regarding resident eligibility to live in the Tax Credit Unit; and any successor certification, as required by the Authority from time to time.

3. Recording and Filing; Covenants to Run with the Land.

- (a) This Agreement will be recorded in the real property records where the Project is located, and the covenants contained herein shall run with the land and shall bind, and the benefits shall inure to the Owner and its successors and assigns, the Authority and its successors and assigns, and all subsequent owners of the Project or any interest therein, for the Term.
- (b) The Owner agrees that any and all requirements of the laws of the State that must be satisfied in order for the provisions of this Agreement to constitute restrictive covenants running with the land shall be deemed to be satisfied in full, and any requirements of privity of estate are intended to be satisfied, or in the alternate, that an equitable servitude has been created to insure that these restrictions run with the land.
- (c) During the Term of this Agreement, each and every contract, deed or other instrument hereafter executed conveying the Project or portion thereof shall expressly provide that such conveyance is subject to this Agreement; provided, however, the covenants contained herein shall survive and be effective as to successors and/or assigns of all or any portion of the Project, regardless of whether such contract, deed or other instrument conveying the Project or portion thereof provides that such conveyance is subject to this Agreement.

4. Representations, Covenants and Warranties of the Owner. The Owner covenants, represents and warrants as follows:

- (a) The Owner:

(i) is duly organized under the laws of the state of \_\_\_\_\_, and is qualified to transact business under the laws of the State;

(ii) has the power and authority to own its properties and assets and to carry on its business as now being conducted;

(iii) has the full legal right, power and authority to execute and deliver this Agreement; and

(iv) has good and marketable title to the Project.

(b) Each building that is the subject of an allocation of Federal Credits is, or by not later than the last day of the first year of the Credit Period, will be a Qualified Low-Income Building and the Project constitutes or will constitute a Qualified Low-Income Housing Project.

(c) The Owner shall not discriminate on the basis of race, creed, color, sex, age, marital status, national origin, disability or familial status or any other applicable protected class, in the lease, use or occupancy of the Project or in connection with the employment or application for employment of persons for the operation and management of the Project, and shall not refuse to lease a unit in the Project to the holder of a voucher or certificate for federal housing assistance payments pursuant to Section 8 of the United States Housing Act of 1937, or a successor federal program, on account of the status of the prospective tenant as such holder.

(d) The Owner shall not demolish any part of the Project or substantially subtract from any real or personal property of the Project; or permit the use of any residential rental unit for any purpose other than rental housing.

(e) The Owner has not and will not execute any other agreement with provisions contradictory to, or in opposition to, the provisions hereof, and in any event, the requirements of this Agreement are paramount and controlling as to the rights and obligations herein set forth and supersede any other provisions in conflict herewith.

(f) If the Owner becomes aware of any situation, event or condition that would result in non-compliance of the Project or the Owner with Section 42 of the Code, the Owner shall promptly give written notice thereof to the Authority.

(g) The Owner shall ensure that the Tax Credit Units shall be of comparable quality to other units, if any, in the Project.

(h) If the Project, or any part thereof, shall be damaged or destroyed or shall be condemned or acquired for public use, the Owner will use its best efforts to repair and restore the Project to substantially the same condition as existed prior to the event causing such damage or destruction, or to relieve the condemnation, and thereafter to operate the Project in accordance with the terms of this Agreement.

(i) The Owner has obtained or will obtain from any prior recorded lienholder on the Project its consent and partial subordination to this Agreement in a form satisfactory to the Authority.

- (j) The Owner shall not evict or terminate the tenancy of an existing tenant of any Tax Credit Unit other than for good cause and shall not increase the gross rent above the maximum allowed under the Code with respect to such Tax Credit Unit.
- (k) The Owner shall establish and maintain an operating reserve fund in an amount that is equal to, or greater than, four (4) months of projected annual operating expenses and four (4) months of debt service payments. The operating reserve fund must remain with the Project for a minimum of three (3) years from the time the Project is placed in service. These requirements, as well as provisions for reserve account reductions over time as Project benchmarks are achieved, must be contained in the entity partnership agreement. These requirements may not be modified without the prior written consent of the Authority.
- (l) **A Qualified Nonprofit Organization shall at all times own an interest in the Project (directly or through a partnership) and shall Materially Participate in the development and operation of the Project throughout the Compliance Period.**

5. Term of this Agreement.

- (a) This Agreement shall be in effect for each building that is part of the Project until the end of the Extended Use Period.
- (b) The Owner hereby waives any rights under Section 42(h)(6)(E)(i)(II) of the Code to terminate the Extended Use Period.
- (c) Code Requirements that Survive Foreclosure:
  - (i) This Agreement shall not be terminated with respect to any portion of the Project that is acquired by foreclosure or deed in lieu of foreclosure if the Secretary determines that such acquisition is part of an arrangement with the Owner a purpose of which is such termination.
  - (ii) If this Agreement terminates due to foreclosure or deed in lieu of foreclosure, the Owner shall not evict or terminate the tenancy of an existing tenant of any Tax Credit Unit other than for good cause and shall not increase the gross rent above the maximum allowed under the Code with respect to such Tax Credit Unit for a period of three (3) years following the termination of this Agreement.
  - (iii) This subsection (c) and the rights granted to the Authority and past, present and future tenants of the Project to enforce this Agreement shall survive any such termination.
  - (iv) In the event foreclosure proceedings are initiated, the Authority shall receive notice of such foreclosure no less than 15 days prior to such foreclosure.

6. Qualified Low-Income Housing Project. From no later than the last day of the first year of the Credit Period through the end of the Extended Use Period, the Owner shall maintain the Project as a Qualified Low-Income Housing Project at all times. To this end, and without limitation, the Owner shall assure that all of the residential units in the Project are available for use by the general public, suitable for occupancy and used on other than a

transient basis unless the Project qualifies as a single room occupancy project or as transitional housing for the homeless pursuant to Section 42(i)(3) of the Code).

7. Occupancy Restrictions.

- (a) For the purpose of Section 42(g)(1) of the Code, the Owner elects the following (select one):
  - \_\_\_ At least twenty percent (20%) of the residential rental units in the Project shall be maintained as both rent-restricted and occupied by individuals or families whose income is fifty percent (50%) or less of AMI.
  - \_\_\_ At least forty percent (40%) of the residential rental units in the Project shall be maintained as both rent-restricted and occupied by individuals or families whose income is sixty percent (60%) or less of AMI.
  - \_\_\_ At least forty percent (40%) of the residential rental units in the Project shall be maintained as both rent-restricted and occupied by individuals or families whose average income does not exceed the imputed income limitation designated by the Owner with respect to the Tax Credit Units provided that: (i) the Owner shall designate the imputed income limitation of each Tax Credit Unit taking into account; (ii) the average of the imputed income limitations shall not exceed sixty percent (60%) of AMI, and (iii) the designated imputed income limitations shall be either twenty percent (20%), 30 percent (30%), 40 percent (40%), 50 percent (50%), 60 percent (60%), seventy percent (70%) or 80 percent (80%) of AMI. Any changes to these initial designations may be made only with the Authority's expressed written consent and in accordance with the Authority's [Income Averaging Policy] in place at the time of the request.
- (b) Additionally, the Owner covenants and agrees that, during the Extended Use Period, the Tax Credit Units shall be maintained as both rent restricted and occupied by individuals or families whose income level does not exceed the applicable AMI as follows:

No. of Units	AMI Threshold
( ) units	20% or less of AMI
( ) units	30% or less of AMI
( ) units	40% or less of AMI
( ) units	50% or less of AMI
( ) units	60% or less of AMI
( ) units	70% or less of AMI
( ) units	80% or less of AMI
( ) units	employee

The Owner further agrees that additional residential units in the Project shall be maintained as both rent-restricted and occupied by low-income individuals or families whose incomes meet the requirements of this subsection (b) to the extent necessary to maintain the Applicable Fraction, at not less than percentage(s) shown on Exhibit B hereto for each taxable year of the Extended Use Period.

- (c) The determination of whether an individual or family is a Qualifying Tenant shall be made at least annually on the basis of the income of such Qualifying Tenant(s). Any Tax Credit Unit occupied by an individual or family who is a Qualifying Tenant at the commencement of occupancy shall continue to be treated as a Tax Credit Unit notwithstanding an increase in the income of such individual or family above the income limitation applicable under subsection (b) of this Section 7 provided that, if such Qualifying Tenant's income subsequently exceeds one hundred forty percent (140%) of the applicable income limit, such residential unit shall no longer be a Tax Credit Unit if after the determination of such increase, but prior to the next determination, any residential unit of comparable or smaller size is rented to a tenant who is not a Qualifying Tenant.
  - (d) As a condition to occupancy, each individual or family who is intended to be a Qualifying Tenant shall be required to sign a fully completed Tenant Income Certification prepared by the Owner, and the income and assets of such individual or family must be verified in the manner prescribed by the Authority.
  - (e) The form of lease to be utilized by the Owner in renting any residential unit in the Project to any person who is intended to be a Qualifying Tenant shall provide for termination of the lease and consent by such person to immediate eviction for failure to qualify as a Qualifying Tenant as a result of any material misrepresentation made by such person with respect to the Tenant Income Certification or the failure by such tenant to execute a certification annually.
8. Additional Agreements. [The Owner agrees that the Authority's award and allocation of Federal Credits is based on the Owner's additional obligations and agreements, as set forth on Exhibit C, attached hereto and incorporated herein by this reference. ][[Because the Owner is obtaining a loan that will be insured by the United States Department of Housing and Urban Development ("HUD"), the parties agree to the additional provisions set forth on Exhibit [D], attached hereto and incorporated herein by this reference.] [Intentionally deleted if N/A]
9. Compliance Monitoring; Fees.
- (a) The Owner acknowledges that Section 42 of the Code requires the Authority to monitor the compliance of the Owner and the Project with the requirements of Section 42 of the Code. The Owner agrees to strictly comply, at all times, with the Compliance Manual, the terms and provisions of which are by this reference incorporated in this Agreement and made a part hereof; and to take all actions required by the Authority pursuant to the Compliance Manual to assist or cooperate with the Authority in monitoring such compliance.
  - (b) The Owner agrees to pay to the Authority: (i) any non-compliance fees as required in the Compliance Manual as of the date of such non-compliance, and (ii) any fees in the amounts and at the times as the Authority shall, in its sole discretion, reasonably require the Owner to pay in order to reimburse the Authority for the costs of such monitoring.
10. Owner Certifications and Reports.
- (a) Within ninety (90) days of filing IRS Form 8609 with the IRS, the Owner shall provide to the Authority a copy of IRS Form 8609, for each building, as filed with the IRS for the first year of the Credit Period.

- (b) The Owner shall provide to the Authority, on the dates required by the Compliance Manual, a Certification of Continuing Program Compliance and an Occupancy Report, each in the form provided by the Authority.
  - (c) The Owner shall maintain in its records and provide to the Authority copies of any and all notices and correspondence from or with the IRS concerning the Project or the Owner upon request.
  - (d) In addition to the information provided for in Section 9 and in this Section 10, the Owner shall provide any other information, documents or certifications requested, from time to time, by the Authority with respect to the Project's physical, operational and financial condition and residents which the Authority reasonably deems necessary to substantiate the Owner's continuing compliance with the provisions of this Agreement and Section 42 of the Code, including a copy of the IRS Form 8609 Schedule A.
11. Transfer Restrictions.
- (a) The Owner shall not sell, assign, convey, transfer or otherwise dispose of the Project or any building in the Project without the prior written consent of the Authority. Such consent shall be given provided that: (i) the Owner is in compliance with the requirements of this Agreement and Section 42(j)(6) of the Code; (ii) the proposed transferee of the Project evidences, to the reasonable satisfaction of the Authority, by its performance with respect to other low-income housing tax credit or government-assisted housing projects and otherwise, its willingness and ability to comply with the terms of this Agreement; and (iii) the Authority shall be paid a transfer fee, as determined, from time to time, by the Authority. In no event shall the Owner dispose of any portion of any building in the Project to any person unless all of such building is disposed of to such person. For the purposes of this subsection, a transfer of any managing member or general partner of the Owner or a transfer of fifty percent (50%) or more of the ownership interests in Owner shall be deemed a transfer of the Project.
  - (b) The Owner shall include, verbatim or by incorporation by reference, all requirements and restrictions contained in this Agreement in any deed or other documents transferring any interest in the Project or in any building in the Project to any other person or entity to the end that such transferee has notice of and is bound by such restrictions, and shall obtain the express written assumption of this Agreement by any such transferee.
12. Physical Maintenance/Management/Books/Records/Inspections.
- (a) The Owner shall maintain each building in the Project such that all residential units are suitable for occupancy, taking into account applicable health, safety and building codes, and otherwise in a manner reasonably satisfactory to the Authority.
  - (b) The Owner shall provide for the management of the Project in a manner reasonably determined by the Authority to assure compliance with this Agreement. Any management contract entered into by the Owner involving the Project shall provide that it shall be subject to termination, without penalty and with or without cause, upon written request by the Authority addressed to the Owner. Upon such request the Owner shall immediately terminate the contract within a period of not more than thirty (30) days and shall make



arrangements reasonably satisfactory to the Authority for continuing proper management of the Project.

- (c) The books, contracts, records, computerized data, documents and other papers relating to compliance of the Owner and the Project with Section 42 of the Code and with this Agreement and to the eligibility of the Owner to claim the Federal Credits with respect to the Project shall at all times be maintained at the Project, or at the Owner's principal place of business in the State of Colorado, in reasonable condition for proper audit and shall be subject to examination and inspection and copying at any reasonable time by the Authority or its authorized agents. The Authority shall also have the right to enter and inspect the Project at any reasonable time.
- (d) Owners are required to keep records for each Qualified Low-Income Building in the Project showing the following:
  - (i) the total number of residential rental units in the building (including the number of bedrooms and the size in square feet of each unit);
  - (ii) the percentage of residential rental units in the building that are Tax Credit Units;
  - (iii) the gross rent charged on each residential rental unit in the building (including any utility allowance and any non-optional fees);
  - (iv) the number of occupants in each Tax Credit Unit;
  - (v) the Tax Credit Unit vacancies in the building and information that shows when, and to whom, the next available units were rented;
  - (vi) the annual income certification of each Qualifying Tenant;
  - (vii) documentation to support each Qualifying Tenant's income certification;
  - (viii) the Eligible Basis and qualified basis of the building at the end of the first year of the Credit Period; and
  - (ix) the character and use of the nonresidential portion of the building included in the building's Eligible Basis under Section 42(d) of the Code (e.g., tenant facilities that are available on a comparable basis to all tenants and for which no separate fee is charged for use of the facilities, or facilities reasonably required by the Project).

Owners are required to keep all records for each building for a minimum of six (6) years after the due date (with extensions) for filing the Owner's federal income tax return for any year; provided, that the records for the first year of the Credit Period must be retained for at least six (6) years beyond the due date (with extensions) for filing the federal income tax return for the last year of the Compliance Period of the building.

- (e) The Authority has the right to conduct physical inspections of the Project and to conduct a review of the Owner's files relating to the Project throughout the Extended Use Period.

13. Enforcement.

- (a) The Owner covenants that it will not knowingly take or permit any action that would result in a violation of the requirements of Section 42 of the Code or of this Agreement. Moreover, the Owner covenants to take any lawful action (including amendment of this Agreement) as may be necessary, in the opinion of the Authority, to comply fully with all applicable rules, rulings, policies, procedures, regulations or other official statements promulgated or proposed by the United States Department of the Treasury or the IRS from time to time pertaining to the Owner's obligations under Section 42 of the Code and affecting the Project.
- (b) The Owner shall promptly advise the Authority as to the date each building in the Project is a Qualified Low-Income Building.
- (c) In the event of any failure of the Owner to comply with the provisions of Section 42 of the Code or this Agreement, consistent with the procedures set forth in the Compliance Manual, the Authority shall: (i) inform the Owner by written notice of such failure; and (ii) provide the Owner with a period of time to correct the failure. If any such failure is not corrected to the satisfaction of the Authority within the period of time specified by the Authority, without further notice the Authority may declare a default under this Agreement effective on the date of such declaration of default, and the Authority may (i) apply to any court, state or federal, for specific performance of this Agreement or an injunction against any violation of this Agreement; (ii) secure the appointment of a receiver to operate the Project in compliance with this Agreement; or (iii) exercise any other remedies at law or in equity or any such other action as shall be necessary or desirable to correct non-compliance with this Agreement.
- (d) The Owner and the Authority each acknowledges that the primary purpose of requiring compliance by the Owner with the restrictions provided in this Agreement is to assure compliance of the Project and the Owner with Section 42 of the Code and the Regulations thereunder, **AND BY REASON THEREOF, THE OWNER IN CONSIDERATION OF RECEIVING AN ALLOCATION OF FEDERAL CREDITS FOR THE PROJECT HEREBY AGREES AND CONSENTS THAT THE AUTHORITY, ANY QUALIFYING TENANT AND ANY INDIVIDUAL WHO MEETS THE INCOME LIMITATION APPLICABLE TO THE BUILDING UNDER THE CODE (WHETHER PRESENT, PROSPECTIVE OR FORMER OCCUPANTS OF THE BUILDING) (ANY OR ALL OF THEM) SHALL BE ENTITLED, FOR ANY BREACH OF THE PROVISIONS HEREOF, AND IN ADDITION TO OTHER REMEDIES PROVIDED BY LAW OR IN EQUITY, TO ENFORCE SPECIFIC PERFORMANCE BY THE OWNER OF ITS OBLIGATIONS UNDER THIS AGREEMENT IN ANY COURT, STATE OR FEDERAL, OF COMPETENT JURISDICTION,** the Owner hereby further specifically acknowledging that the beneficiaries of the Owner's obligations hereunder cannot be adequately compensated by monetary damages in the event of any default hereunder.
- (e) In the event of the Owner's or Project's failure to comply fully with the Code, the covenants and agreements contained herein or with all applicable rules, rulings, policies, procedures, regulations or other official statements promulgated or proposed by the United States Department of the Treasury or the IRS or the Authority from time to time pertaining to the

obligations of the Owner as set forth therein or herein, the Authority, in addition to all of the remedies provided by law or in equity, shall notify the IRS of such noncompliance.

14. Issuance of Form 8609. The Authority shall prepare and file with IRS Form 8609 with respect to each building in the Project, evidencing the Authority's allocation of Federal Credits with respect to the Project. The Authority shall issue Form 8609(s) to the Owner when the following conditions have been met:

- (a) Each building in the Project for which a Form 8609 is issued is a Qualified Low-Income Building.
- (b) The Owner and the Project are in compliance with the terms of this Agreement.
- (c) The Owner shall have provided, on form(s) approved by the Authority, a certification of each building's Eligible Basis and the Authority shall have made its final determination of the Federal Credit amount and its final determination pursuant to Section 42(m)(2) of the Code.
- (d) The Owner shall have provided a copy of the executed partnership or operating agreement.
- (e) The Owner shall have provided to the Authority the partial subordination of any prior recorded lien on the Project to this Agreement.
- (f) The Owner and its management agent and any management staff involved in the certification of residents shall have completed compliance training provided or approved by the Authority.
- (g) The Owner shall have paid the compliance monitoring fee.

15. Return of Unused Federal Credit. Pursuant to Section 42(h)(3)(C) of the Code and Regulation §1.42-14(d), the Federal Credit amount allocated to the Owner with respect to the Project shall be canceled and returned to the Authority, in whole or in part, if (i) any building in the Project is not a Qualified Low-Income Building within the time period required by Section 42 of the Code, or (ii) the "Qualified Basis" of any building in the Project is less than the qualified basis on which the Federal Credit was allocated by the Authority.

16. Release and Indemnification. The Owner acknowledges that, in issuing IRS Form 8609(s) with respect to the Project, the Authority is relying or will rely upon information and representations given by or on behalf of the Owner and has made or will make no independent investigation and does not and will not have independent knowledge of the basis for such information and representations. Accordingly, to induce the Authority to issue IRS Form 8609(s), the Owner agrees as follows:

- (a) The Owner agrees to release and forever discharge the Authority, its members, employees, agents, officers, successors and assigns of and from any and all claims, demands, causes of actions, judgments and executions which Owner has or may hereafter have against the Authority, whether in law or in equity, arising or resulting from, or on account of or pertaining to, whether directly or indirectly, the issuance of a Form 8609 with respect to the Project by the Authority.

- (b) The Owner hereby agrees to indemnify, save harmless and defend the Authority, and its members officers, agents, employees, successors and assigns from any obligation, claim, loss, demand, cost, expense (including the costs of the investigation and settlement of any claim, and including reasonable attorney's fees) or judgment against the Authority arising or resulting from, or on account of or pertaining to, whether directly or indirectly, the Authority's issuance of a Form 8609 with respect to the Project. If any such claim is asserted, any indemnified party hereunder will give prompt notice to the Owner and will cooperate in the investigation and defense of any such claim. The Owner will assume the defense of any such asserted claim by engaging counsel approved by the indemnified party (which approval shall not be unreasonably withheld), it being understood that the indemnified party shall have the right to employ its own separate counsel and participate in such proceedings at its own cost and expense.

- (c) If the indemnification provided in subsection (b) is, for any reason, either unavailable to the Authority or any of the other persons intended to be indemnified thereby or insufficient to hold it or any of them harmless, then the Owner hereby agrees to contribute to all amounts paid or payable by the Authority and such other persons as a result of any such obligation, claim, loss, demand, cost, expense, or judgment. The amount to be contributed by the Owner shall be the amount that is appropriate to reflect both the relative benefits received by the Owner, on the one hand, and by the Authority and such other persons, on the other hand, and the relative degrees of fault of the Owner, on the one hand, and of the Authority and such other persons, on the other hand.

17. Miscellaneous.

- (a) The invalidity of any clause, part or provision of this Agreement shall not affect the validity of the remaining portions thereof.
- (b) All notices to be given pursuant to this Agreement shall be in writing and shall be deemed given when mailed by certified or registered mail, return receipt requested, to the parties hereto at the addresses set forth below, or to such other place as a party may from time to time designate in writing.

To the Authority: Colorado Housing and Finance Authority  
1981 Blake Street  
Denver, Colorado 80202-1272  
Attention: Low-Income Housing Tax Credit Program

To the Owner: [INSERT INFO]  
\_\_\_\_\_  
\_\_\_\_\_  
Attention: \_\_\_\_\_  
Email: \_\_\_\_\_

The Authority and the Owner may, by notice given hereunder, designate any further or different addresses to which subsequent notices, certificates or other communications shall be sent.

(c) This Agreement shall be governed by the laws of the State of Colorado and, where applicable, the laws of the United States of America.

This Agreement may be amended from time to time by any written instruments signed by both the Authority and the Owner. The signing of any such instrument by the Authority shall be deemed for all purposes to be on behalf of, and shall be legally binding on, the Authority, any Qualifying Tenant and any individual who meets the income limitation applicable to the Project under the Code (whether present, prospective or former occupants of the Project).

18. Counterparts. This Agreement may be executed in one or more counterparts, each of which will be deemed an original, but all of which together will constitute one and the same instrument. Delivery of an executed counterpart of a signature page of this Agreement by electronic image scan transmission will be effective as delivery of a manually executed counterpart of the Agreement.

**[REMAINDER OF PAGE INTENTIONALLY BLANK. SIGNATURE PAGES FOLLOW]**

IN WITNESS WHEREOF, the parties have caused this agreement to be signed by their respective duly authorized representatives, as of the day and year first written above.

**[Owner]**  
**[NOTE: SIGNATURE BLOCK TO BE CONFIRMED BY OWNER AND ENTITY DOCUMENTS]**

By: \_\_\_\_\_

Its: \_\_\_\_\_

STATE OF \_\_\_\_\_ )  
 ) ss.  
\_\_\_\_\_ COUNTY OF \_\_\_\_\_ )

Acknowledged before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_,  
by \_\_\_\_\_ as \_\_\_\_\_  
of \_\_\_\_\_  
\_\_\_\_\_.

My Commission expires: \_\_\_\_\_.

\_\_\_\_\_  
Notary Public

COLORADO HOUSING AND FINANCE  
AUTHORITY

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Title: \_\_\_\_\_

STATE OF COLORADO )  
CITY AND COUNTY OF DENVER ) ss.  
)

Acknowledged before me this \_\_\_\_ day of \_\_\_\_\_, 20\_\_, by  
\_\_\_\_\_ as \_\_\_\_\_ of Colorado Housing  
and Finance Authority.

My Commission expires: \_\_\_\_\_.

\_\_\_\_\_  
Notary Public

**EXHIBIT A  
LEGAL DESCRIPTION**

[to be inserted from title commitment or policy]

Also known as: [INSERT STREET ADDRESS] For informational Purposes only

**EXHIBIT B**

**Minimum Applicable Fraction by Building**

Building Identification Number	Minimum Applicable Fraction

**EXHIBIT C**

**Additional Owner Agreements**

**I. State Credits [insert if state credits]**

**RECITALS**

A. The Authority is authorized by Part 21 of Article 22 of Title 39, Colorado Revised Statutes, as may be amended from time to time (the "**State Affordable Housing Credit Act**") to allocate affordable housing tax credits under the State Tax Credit Act.

B. Under the State Affordable Housing Credit Act, the Authority determines the eligibility for and allocates credits in accordance with the standards and requirements set forth in the State Affordable Housing Credit Act and Section 42 of the Code.

C. The Owner applied to the Authority for an allocation of affordable housing credits under the State Affordable Housing Credit Act (the "**State Credits**").

D. In connection with its Application (defined below) the Owner made certain representations to the Authority about the Project, including representations as to the number of Tax Credit Units and the term of occupancy restrictions, upon which representations the Authority relied in considering the Application for a reservation and allocation of State Credits.

E. The Authority is unwilling to allocate State Credits to the Project unless the Owner enters into this Agreement, and consents to be regulated by the Authority in order that the Authority may enforce the occupancy restrictions and other covenants, terms and conditions of this Agreement in accordance with the State Affordable Housing Credit Act.

NOW, THEREFORE, in consideration of the mutual promises and covenants hereinafter set forth, and of other valuable consideration, the Owner and the Authority agree as follows:

1. Incorporation of Recitals. The recitals above are incorporated in and made a part of this Agreement.

2. Definitions. All references in the Agreement to the following capitalized terms, shall have the following meaning:

- a. "**Allocation Certificate**" means the Colorado Affordable Housing Tax Credit Allocation Certificate.
- b. "**Application**" means the application submitted to the Authority for a reservation, initial determination, or allocation of Federal Credits and/or State Credits. The Application includes all materials provided to the Authority, as required by the applicable checklist, and any additional materials provided to the Authority that are reviewed as part of the tax application process.

- c. "Credit Period" means, with respect to the State Tax Credit, the "credit period" as defined in Section 39-22-2101 of the State Tax Credit Act, and with respect to the Federal Credit, the "credit period" as defined in Section 42(f) of the Code.
  - d. "Department" means the Colorado Department of Revenue.
  - e. "Qualified Development" means a "qualified development" under Section 39-22-2101 (10) of the State Tax Credit Act.
3. Additional Representations, Covenants and Warranties of the Owner. The Owner covenants, represents and warrants as follows:
- a. In compliance with the State Affordable Housing Credit Act:
    - i. Each building that is the subject of an allocation of State Credits is or by not later than the last day of the Credit Period will be a Qualified Low-Income Building and the Project constitutes or will constitute a Qualified Development.
    - ii. The Project will be operated and maintained as a Qualified Development.
    - iii. The Owner shall operate the Project in accordance with the accessibility and adaptability requirements of the Federal Credits and Title VIII of the Civil Rights Act of 1968, as amended by the Fair Housing Amendments Act of 1988, as amended.
    - iv. The Owner will not knowingly take or permit any action that would result in a violation of the State Affordable Housing Act.
  - b. The Owner covenants that it will not knowingly take or permit any action that would result in a violation of the requirements of the State Affordable Housing Credit Act.
  - c. If the Owner becomes aware of any situation, event or condition that would result in non-compliance of the Project or the Owner with the State Affordable Housing Credit Act, the Owner shall promptly give written notice thereof to the Authority.
  - d. The Owner covenants to take any lawful action (including amendment of this Agreement) as may be necessary, in the opinion of the Authority, to comply fully with all applicable rules, rulings, policies, procedures, regulations or other official statements promulgated or proposed by the Department or the Authority from time to time pertaining to the Owner's obligations under the State Affordable Housing Credit Act and affecting the Project.
4. Compliance Monitoring. The Owner agrees that the Authority must monitor the Owner and the Project for compliance with the State Affordable Housing Credit Act in addition to the Code, and may report any non-compliance to the Department in addition to the IRS, and may provide the Department all filings provided to the IRS. As such, the Owner agrees to be bound by all State Credit compliance requirements found in the Compliance Manual and to take all actions required by the Authority pursuant to the Compliance Manual to assist or cooperate with the Authority in monitoring such compliance.
5. Owner Certifications and Reports.

- a. Within ninety (90) days of filing the Allocation Certificate with the Department, the Owner shall provide to the Authority a copy, for each building, of the Allocation Certificate, as filed by with the Department for the first year of the Credit Period.
  - b. The Owner shall provide the Department with any documentation, certifications, and/or reports required by the State Affordable Housing Credit Act and/or as required by the Department for its monitoring of the State Credits.
  - c. The Owner shall maintain its records and provide the Authority with all notices and correspondence from or with the Department concerning the Project or the Owner, and provide the Authority with any other information, documents or certifications requested, from time to time, by the Authority which the Authority reasonably deems necessary to substantiate the Owner's continuing compliance with the State Affordable Housing Credit Act.
6. Enforcement. All the enforcements rights of the Authority included in this Agreement include the Authority's rights to ensure compliance with the State Credits and the State Affordable Housing Credit Act.
7. Termination in the Event of Foreclosure. This Agreement shall not terminate, with respect to the State Credits, on the date the Project is acquired by foreclosure or instrument in lieu of foreclosure if the Authority determines that such acquisition is part of an arrangement, a purpose of which is to cause such termination.
8. Restrictions on Transfers. In addition to the requirements in Section 11 of the Agreement, the Authority's consent to a transfer of the Project will be conditioned on the Owner's and the Project's compliance with the State Affordable Housing Credit Act.
9. Allocation Certificate. The Authority's issuance of the Allocation Certificate is conditioned upon the satisfaction of the Owner of all requirements in the Qualified Allocation Plan for the issuance of such certificate.
- II. Special Populations Served
- 1. The Owner further covenants and agrees that not later than the last day of the first year of the Credit Period, at least \_\_\_\_\_ (\_\_\_\_\_) of the residential rental units in the Project shall be constructed, equipped, set aside and occupied (or held vacant and available for immediate occupancy) by \_\_\_\_\_ [describe special housing needs resident] at all times during the term of this Agreement, and the Owner shall provide evidence to the Authority of any license, permit or other governmental approval required for such occupancy. **[USE IF SPECIAL NEEDS HOUSING]**
  - 2. [USE IF REQUIREMENT TO ADD LANGUAGE RE SENIOR – PREFERENCE NOT TO INCLUDE, AND IF INCLUDE, NOT TO SPECIFY AGE]
  - 3. The Owner will provide the Authority with notification of any substantial changes in population served.

EXHIBIT D

1. [HUD Financing Provisions] Insert if applicable.

(a) The following terms shall have the following definitions:

“Code” means the Internal Revenue Code of 1986, as amended.

“HUD” means the United States Department of Housing and Urban Development.

“HUD Regulatory Agreement” means the Regulatory Agreement between Borrower and HUD with respect to the Project, as the same may be supplemented, amended or modified from time to time.

“Lender” means \_\_\_\_\_, its successors and assigns.

“Mortgage Loan” means the mortgage loan made by Lender to the Borrower pursuant to the Mortgage Loan Documents with respect to the Project.

“Mortgage Loan Documents” means the Security Instrument, the HUD Regulatory Agreement and all other documents required by HUD or Lender in connection with the Mortgage Loan.

“National Housing Act” means the National Housing Act of 1934, as amended.

“Program Obligations” has the meaning set forth in the Security Instrument.

“Security Instrument” means the mortgage or deed of trust from Borrower in favor of Lender, as the same may be supplemented, amended or modified.

“Surplus Cash” has the meaning specified in the HUD Regulatory Agreement.

(b) Notwithstanding anything herein to the contrary, except the requirements in 26 U.S.C. 42(h)(6)(E)(ii), the provisions of hereof are expressly subordinate to (i) the Mortgage Loan Documents, including without limitation, the Security Instrument, and (ii) Program Obligations (the Mortgage Loan Documents and Program Obligations are collectively referred to herein as the “HUD Requirements”). Borrower covenants that it will not take or permit any action that would result in a violation of the Code, HUD Requirements or the requirements hereof. In the event of any conflict between the provisions hereof and the provisions of the HUD Requirements, HUD shall be and remains entitled to enforce the HUD Requirements. Notwithstanding the foregoing, nothing herein limits the Agency’s ability to enforce the terms of this LURA, provided such terms do not conflict with statutory provisions of the National Housing Act or the regulations related thereto. The Borrower represents and warrants that to the best of Borrower’s knowledge the LURA imposes no terms or requirements that conflict with statutory provisions of the National Housing Act and related regulations.

(c) In the event of foreclosure, this LURA (including without limitation, any and all land use covenants and/or restrictions contained herein) shall automatically terminate, with the

exception of the requirements of 26 U.S.C. 42(h)(6)(E)(ii) above, or to the extent applicable, as otherwise approved by HUD.

(d) Borrower and the Authority acknowledge that Borrower’s failure to comply with the covenants provided in the LURA does not and shall not serve as a basis for default under the HUD Requirements, unless a default also arises under the HUD Requirements.

(e) Except for the Authority’s reporting requirement, in enforcing the LURA, the Authority will not file any claim against the Project, the Mortgage Loan proceeds, any reserve or deposit required by HUD in connection with the Security Instrument or HUD Regulatory Agreement, or the rents or other income from the property other than a claim against:

- i. Available surplus cash, if the Borrower is a for-profit entity;
- ii. Available distributions of surplus cash and residual receipts authorized for release by HUD, if the Borrower is a limited distribution entity;
- iii. Available residual receipts authorized by HUD, if the Borrower is a non-profit entity; or
- iv. A HUD-approved collateral assignment of any HAP contract.

(f) For so long as the Mortgage Loan is outstanding, Borrower and Authority shall not further amend the LURA, with the exception of clerical errors or administrative correction of non-substantive matters, without HUD’s prior written consent.

(g) Subject to the HUD Regulatory Agreement, the Authority may require the Borrower to indemnify and hold the Authority harmless from all loss, cost, damage and expense arising from any claim or proceeding instituted against Authority relating to the subordination and covenants set forth in the LURA, provided, however, that Borrower’s obligation to indemnify and hold the Authority harmless shall be limited to available surplus cash and/or residual receipts of the Borrower.

(h) Notwithstanding anything to the contrary contained herein, it is not the intent of any of the parties hereto to cause a recapture of the Low-Income Housing Tax Credits or any portion thereof related to any potential conflicts between the HUD Requirements and the LURA. Borrower represents and warrants that to the best of Borrower’s knowledge the HUD Requirements impose no requirements which may be inconsistent with full compliance with the LURA. The acknowledged purpose of the HUD Requirements is to articulate requirements imposed by HUD, consistent with its governing statutes, and the acknowledged purpose of the Restrictive Covenants is to articulate requirements imposed by Section 42 of the Code. In the event an apparent conflict between the HUD Requirements and the LURA arises, the parties and HUD will work in good faith to determine which federally imposed requirement is controlling. It is the primary responsibility of the Borrower, with advice of counsel, to determine that it will be able to comply with the HUD Requirements and its obligations under the LURA.]

[2. HUD Public Housing Provisions.

(a) As used herein, the following terms shall have the following meanings:

- (i) “HUD” means the U.S. Department of Housing and Urban Development.

- (ii) "Restriction Period" means the period in which the Declaration is in effect.
- (iii) "ACC" means the Consolidated Annual Contributions Contract between HUD and the Authority dated as of \_\_\_\_\_, as amended by the Mixed Finance ACC Amendment, dated as of \_\_\_\_\_, as the same may be further amended from time to time.
- (iv) "Act" means the United States Housing Act of 1937 (42 U.S.C. § 1437, *et seq.*), as amended from time to time, any successor legislation, and all implementing regulations issued thereunder or in furtherance thereof.
- (v) "Applicable Public Housing Requirements" means all requirements applicable to public housing, including, but not limited to, the Act, HUD regulations thereunder (and, to the extent applicable, any HUD-approved waivers of regulatory requirements), the ACC, the HOPE VI grant agreement (if applicable), HUD notices (including any notice of fund availability under which Landlord received an award of HOPE VI funds for use in connection with the Project), the HUD-approved Declaration of Restrictive Covenants in favor of HUD, the Owner's admissions and occupancy policies applicable to the Project, as set forth in its PHA Plan, and all applicable Federal statutory, executive order and regulatory requirements, as those requirements may be amended from time to time.
- (vi) "Declaration" means the declaration of restrictive covenants required by HUD to be recorded against the Project prior to any mortgage(s) or other encumbrance(s) against the Property.

(b) Notwithstanding anything in this Agreement to the contrary, except for the requirements of 26 U.S.C. 42(h)(6)(E)(ii) of the Code:

(i) The provisions hereof are expressly subordinate to Applicable Public Housing Requirements. Owner covenants that it will not take or permit any action that would result in violation of Section 42 of the Code, Applicable Public Housing Requirements or Restrictive Covenants. In the event of any conflict between the provisions of the Restrictive Covenants and the provisions of the Applicable Public Housing Requirements, HUD shall be and remains entitled to enforce the Applicable Public Housing Requirements. Notwithstanding the foregoing, nothing herein limits the Authority's ability to enforce the terms of the Restrictive Covenants as required by the Code, provided such terms do not conflict with statutory provisions of the Act or the regulations related thereto. The Owner represents and warrants that to the best of the Owner's knowledge the Restrictive Covenants impose no terms or requirements that conflict the Act and related regulations.

(ii) In accordance with 26 U.S.C. 42(h)(6)(E)(i)(1), in the event of foreclosure of the Project, this Agreement (including without limitation, any and all land use covenants and/or restrictions contained herein) shall automatically terminate, with the exception of the requirements of 26 U.S.C. 42(h)(6)(E)(ii) of the Code.

(iii) The Authority expressly reserves the right to: seek specific performance of this Agreement and to report to the Internal Revenue Service, pursuant to Section 42(m)(1)(B)(iii) of the Code and applicable regulations thereunder, any non-compliance with any terms of this Agreement. In addition, pursuant to 26 U. S. C. Section 42(h) (6)(B)(ii), tenants have the right to enforce the Restrictive Covenants.

(iv) During the Restriction Period, no amendment to this Agreement shall be effective without the prior written approval of HUD.

(v) Notwithstanding anything to the contrary contained herein, it is not the intent of any of the parties hereto to cause a recapture of the Low-Income Housing Tax Credits or any portion thereof related to any potential conflicts between the Applicable Public Housing Requirements and this Agreement. Owner represents and warrants that to the best of Owner's knowledge the Applicable Public Housing Requirements impose no requirements which may be inconsistent with full compliance with this Agreement. The acknowledged purpose of the Applicable Public Housing Requirements is to articulate requirements imposed by HUD, consistent with its governing statutes, and the acknowledged purpose of this Agreement is to articulate requirements imposed by Section 42 of the Code. In the event an apparent conflict between the Applicable Public Housing Requirements and this Agreement arises, the parties and HUD will work in good faith to determine which federally imposed requirement is controlling. It is the primary responsibility of Owner, with advice of counsel, to determine that it will be able to comply with the Applicable Public Housing Requirements and its obligations under this Agreement.]



## Parking Management Plan - 602 Galena

This document proposes a framework to guide the creation of a Parking Management Plan for on-street parking stalls that will serve the proposed development at 602 Galena Street, Frisco, CO 80443. Any Parking Management Plan or regulations must be approved by the Town of Frisco.

A total of twenty (29) new on-street parking stalls are proposed to be constructed as part of the development of the 602 Galena project. These parking stalls are proposed to be constructed in the Galena Street and 6th Avenue right-of-ways.

The 602 Galena project proposes to use nine (9) of the on-street parking stalls for users of the Development, while the other twenty (20) on-street parking stalls shall be constructed as a community benefit providing new vehicular parking for the public.

The 602 Galena site is constrained and unable to accommodate all required parking on-site. This configuration will allow the 602 Galena project to meet the number of parking spaces required by code while also providing additional parking for the community at large.

### Construction

The Owner/Developer will construct twenty-nine (29) parking spaces within the Town of Frisco's right-of-way along Galena Street and 6th Avenue as part of the off-site infrastructure improvements for the 602 Galena project. Construction shall include all demolition, grading, drainage, pavement, signage, stripping, and other work required to construct twenty-nine parking spaces per the plans.

### Operation and Maintenance

**Signage:** Signage will be installed as part of the construction of the 602 Galena project's off-site improvements. The signage will display designated hours for parking and which parking stalls require parking permits. Signage shall state "Permit Parking Only. No Parking Monday and Thursday 7 am - 10 am. November 1<sup>st</sup> through May 1<sup>st</sup>" Signage will also display towing information for any vehicles improperly parked reference Exhibit C, page 17. Any replacement, upgrade, or maintenance of the signage after initial installation will be the exclusive right and responsibility of the Town of Frisco.

**Permit parking and towing :** To park in the 9 spaces dedicated to the users of 602 Galena, a vehicle must display a valid parking permit at all times. Parking permits will be issued by property management and may be subject to per-unit limits. Property management will also oversee the towing of vehicles that are improperly parked or parked without a permit. The Town of Frisco may tow or ticket improperly parked vehicles in those spaces as well.

Visitors to 602 Galena who choose to park in permitting spaces will also be required to obtain a parking permit from property management if they choose to park overnight. Vehicles improperly parked are subject to towing.

Parking procedures and requirements, including permit requirements, will be included in all lease agreements. Repeated parking violations will be considered lease violations subject to standard remedies including fines and/or eviction.

Lease agreements will stipulate what vehicles are allowed to be parked in the parking spaces. Only registered and operational passenger vehicles are allowed in the parking spaces. Recreational vehicles, trailers, boats, oversized vehicles or vehicles longer than 18 feet, or other equipment are not allowed. Car washing and car repair are not allowed in the parking spaces. Motorcycles must be parked in regular parking spaces. Abandoned or inoperable vehicles are not allowed to be parked in the parking spaces. Vehicles shall not overhang onto the sidewalk.

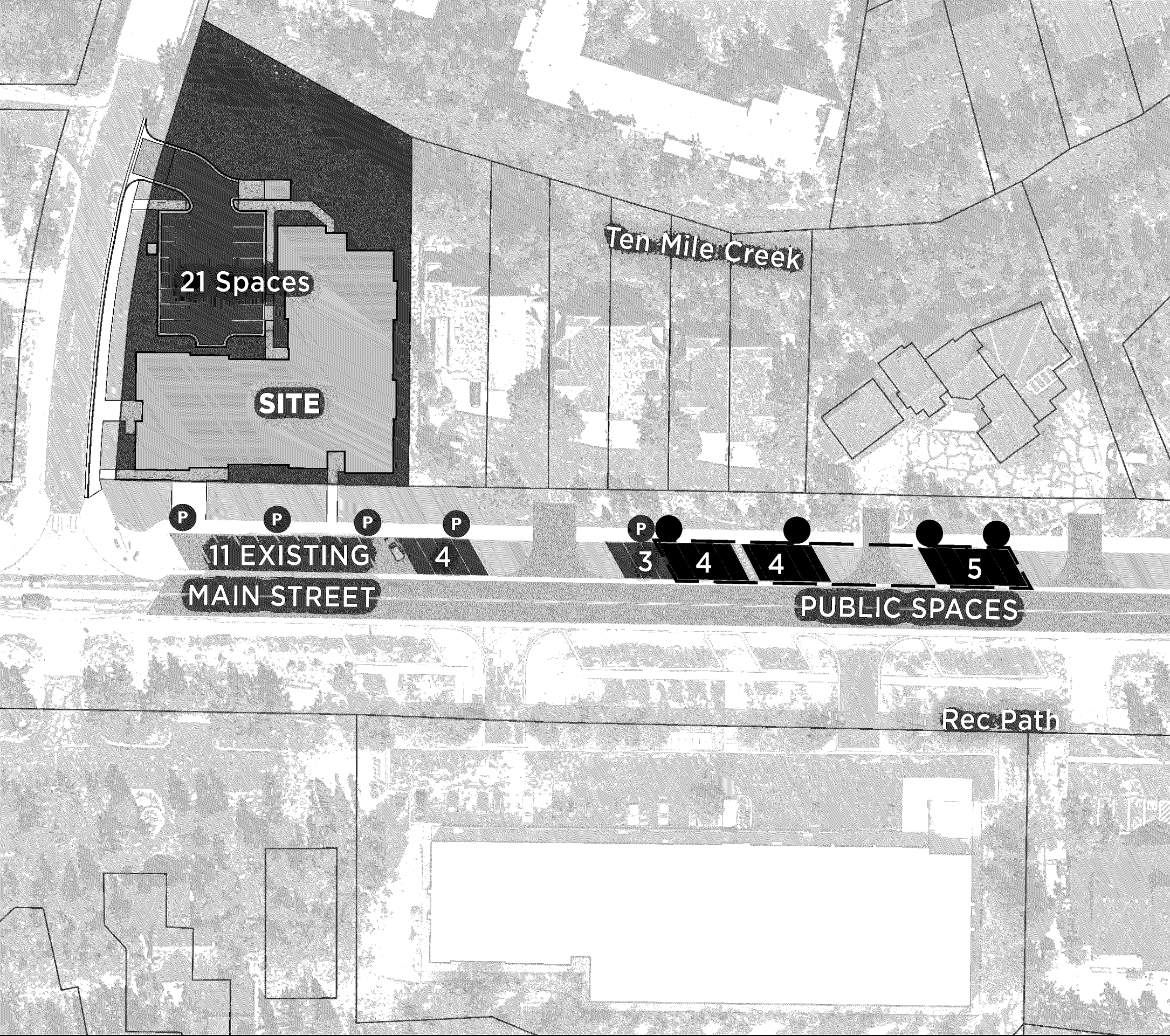
**Overnight parking:** Overnight parking will be allowed in the 9 spaces dedicated to the users of 602 Galena. However, vehicles that do not display the proper parking permits may be towed (see above). Overnight parking will not be allowed in the 20 on-street parking stalls open to the public, except at the discretion of the Town of Frisco.

**Snow Removal:** The removal of snow from the public right-of-way will continue to be the sole duty and responsibility of the Town of Frisco. Parking restrictions to allow for the removal of snow are required.

The Town may restrict parking in the right-of-way at any time to allow for regular maintenance of the right-of-way, including snow removal. The Town shall sign parking restrictions times. On-street overnight parking spaces for permit spaces shall be signed for no parking 7 am to 10 am on Mondays and Thursdays November 1<sup>st</sup> through May 1<sup>st</sup>. Property management shall tow any vehicles not removed from the permit parking spaces during these times. To allow for snow removal, all vehicles must be moved from the on-street parking spaces during posted times.

**Maintenance :** Maintenance of the right-of-way, including parking stalls constructed in association with the 602 Galena Development, shall be the sole right and responsibility of the Town of Frisco. Maintenance may include, but is not limited to resurfacing, snow removal, and signage maintenance.

The Town of Frisco may enforce parking regulations for all the on-street spaces constructed through the 602 Galena project through any appropriate legal means, including the ticketing of vehicles, the towing of vehicles, and the disabling of vehicles pursuant to Colorado state law.



### SITE PARKING

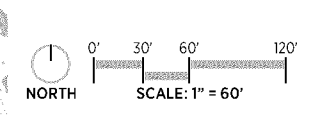
ON-SITE PARKING	21
ADA SPACES	2
CAR SHARE ALLOWANCE	8 (2 SPACES)
PARKING SPACES	11
EXISTING ON-STREET PARKING	11
PROPOSED ON-STREET PARKING	20
TOTAL PARKING	52
TOTAL REQUIRED PARKING	43
NET PARKING	+9

### NET PUBLIC BENEFIT

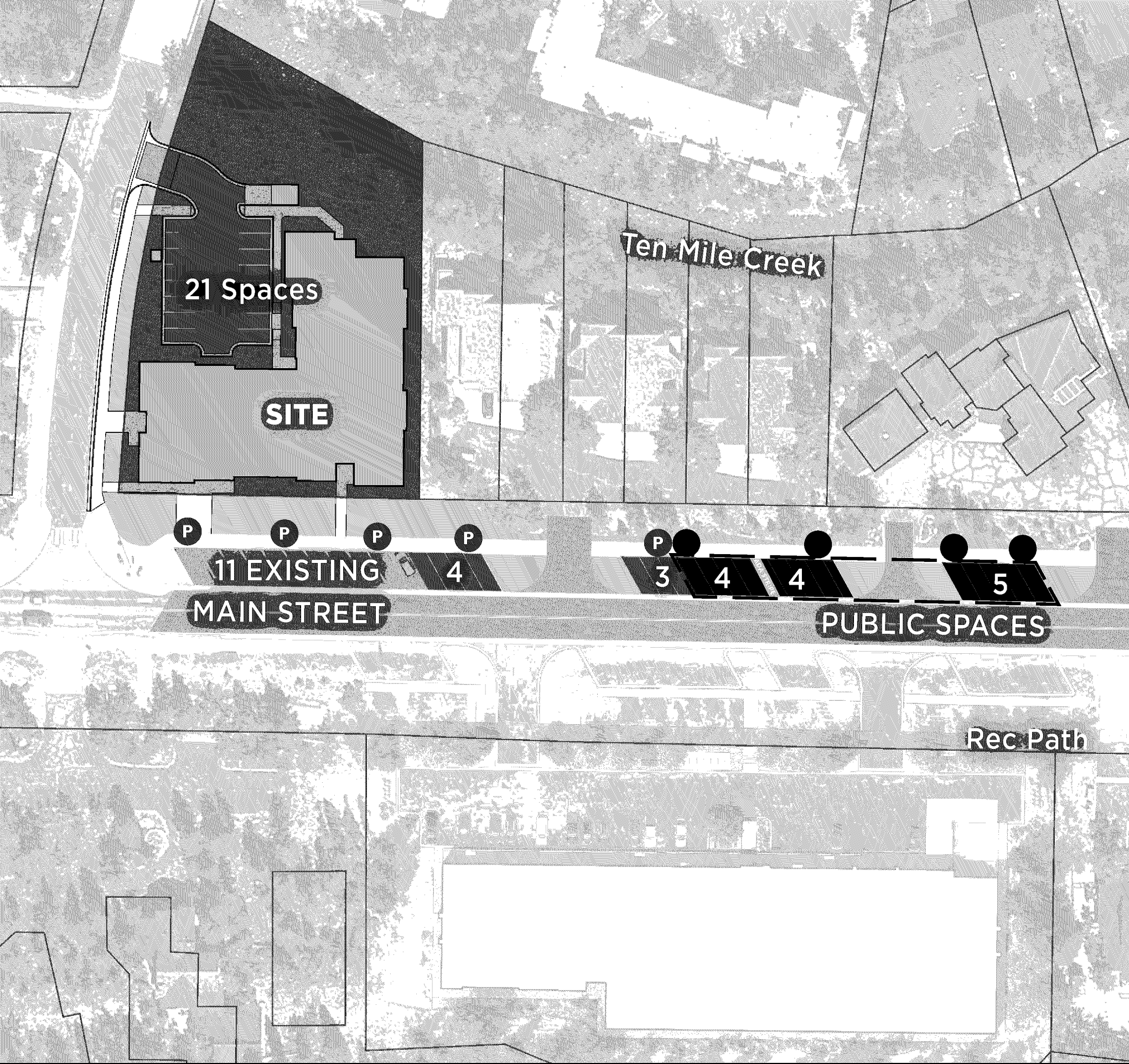
11	EXISTING PUBLIC SPACES
20	PROPOSED SPACES
-18	RESIDENTIAL PERMIT
13	REMAINING PUBLIC SPACES

### LEGEND

- RESIDENTIAL
  - COMMERCIAL
  - UNALLOCATED
- PUBLIC SPACES**
- P PERMIT SIGNAGE (SEE PG 17)
  - TOWN SIGNAGE



Notes:  
1. Graphic provided for illustrative purposes only and subject to change.  
2. Proposed parking subject to Parking Management Plan and permitting.



## LEGEND

- RESIDENTIAL
  - COMMERCIAL
  - UNALLOCATED
- PUBLIC SPACES**
- PERMIT SIGNAGE (SEE PG 17)
  - TOWN SIGNAGE

## SITE PARKING

**9 UNALLOCATED ON STREET PARKING SPACES ARE PROPOSED TO BE BUILT FOR PUBLIC USE.**

101 West Main St. Parking Calcs (PUD)		
Location	Number of Units	Required per PUD
<i>Residential Parking</i>		
Studios	24	12.0
1BR	17	8.5
2BR	11	11.0
<b>Total</b>	<b>52</b>	<b>31.5</b>
<i>Visitor</i>		
Visitor	1/7 # of units	7.4
<b>Residential Parking</b>		<b>38.9</b>
Office Space	3.4	3.4
<b>Total</b>		<b>42.3 (round to 42)</b>
		<b>43 spaces provided</b>
<i>Other</i>		
Onsite (Residential)		21
Offsite (Residential) plus Office Space		22



Notes:  
 1. Graphic provided for illustrative purposes only and subject to change.  
 2. Proposed parking subject to Parking Management Plan and permitting.

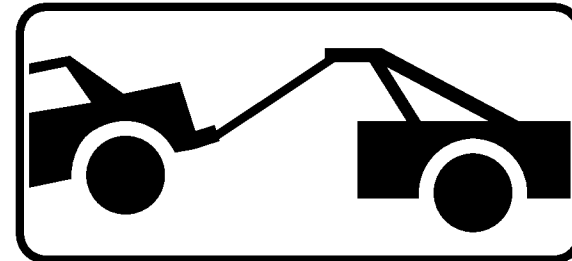
**EXAMPLES OF PERMITTED AND UNPERMITTED PARKING SIGNAGE**

**PROPOSED SIGNAGE**

PROPOSED SIGNAGE TO HAVE WARNINGS DENOTING TOWING, FINES, AND TIME WHEN PARKING IS NOT ALLOWED.

ADDITIONAL SIGNAGE IN THIS LOCATION WILL BE USED TO DENOTE PERMIT REQUIREMENTS FOR RESERVED RESIDENTIAL SPACES.

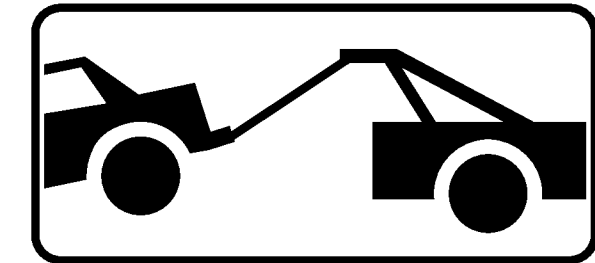
SIGNAGE SHOULD MAINTAIN CONSISTENCY WITH TOWN SIGNAGE THROUGHOUT THE TOWN. SIGNAGE SHOULD BE UPDATED AS NEEDED TO MATCH CURRENT TOWN STANDARDS.



**PERMIT SIGNAGE  
(SEE PG 15)  
RESIDENTIAL  
OVERNIGHT  
PARKING FOR  
DEVELOPMENT**



**TOWN SIGNAGE  
(SEE PG 15)  
PUBLIC PARKING,  
NO OVERNIGHT  
PARKING ALLOWED**



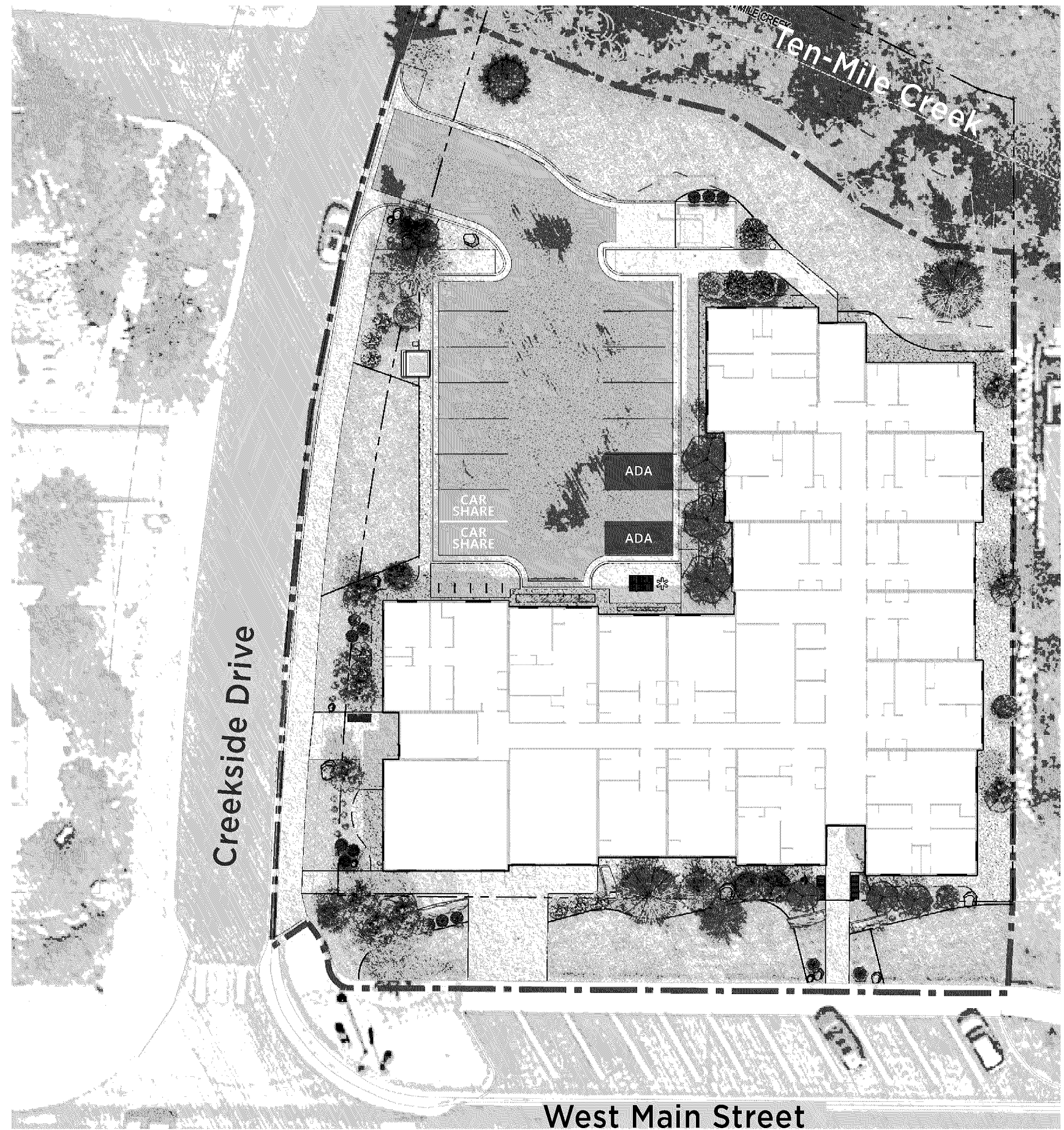
## Car Sharing Proposal for 101 W Main Housing Planned Unit Development

This document proposes a framework to guide the establishment of a car-sharing program that will serve the proposed development at 101 W Main Street, Frisco, CO 80443. Any parking reductions based on a car-sharing program or regulations governing parking management must be approved by the Town of Frisco.

The development at 101 W Main proposes to allocate two (2) on-site parking spaces for car-sharing services for residents of the development. Under The 101 W Main Housing Planned Unit Development, provision of a parking space dedicated to a car-sharing service shall count as 4 parking spaces when calculating the required minimum parking spaces for a development qualifying for the Town's Affordable Housing Development Incentive Program.

The 101 W Main Housing project will be a deed-restricted rental development under single ownership and operated by a professional property management company. The property owner will enter into an agreement with a car-sharing company such as ZipCar and Car2Go; the agreement will be enforced by the property management company.

If at any time following lease-up of the development, the 101 W Main Housing project fails to provide on-site car-sharing as required in the approved Major Site Plan, the property owner shall have 60 days to cure the issue. If a car-sharing service is not offered as required at the end of 60 days, the property shall be in violation of the Town code and shall be subject to penalties as described in Section 1-14 of the Code of Ordinances of the Town of Frisco.



# 101 WEST MAIN STREET

## MAJOR SITE PLAN APPLICATION

May 13, 2024

Katie Kent  
Community Development Director  
Town of Frisco  
P.O. Box 3600  
Frisco, CO 80443

### Introduction

The proposed site plan for the Planned Unit Development at 101 West Main will comprise a mixed-use development in the Town of Frisco that seeks to bring online 52 deed restricted residential dwelling units and 1,546 square feet of leasable nonresidential space. Located at 101 West Main Street, we propose redeveloping an under-utilized parcel of land to provide fully deed-restricted workforce housing and commercial space focused on meeting the needs of our community.

### Overall Site Conditions and Existing Land Uses

Currently, this 0.832-acre lot, Lot B-1 of the West Frisco 70 Subdivision Filing 2 comprises the Westmain Professional Building, a 1979 building with approximately 20 business tenants. In February of 2024, a Planned Unit Development application was approved for this property by the Town Council; this site plan application seeks to fulfill the vision of the PUD by maximizing the utility of the property with modernized non-residential space and deed-restricted, affordable residential housing units.

This proposed development will bring the property into greater conformance with the Comprehensive Plan goals for West Main Street, activating the streetscape, engaging with Tenmile Creek, and importantly developing attainable and attractive housing for Frisco's workforce.



Existing Site - Facing Southwest



Existing Site - Facing Northeast

### Surrounding Neighborhood

While the West Main corridor includes a range of uses, residential is the primary nearby land use. The Tenmile Creek defines the northern boundary of the site with Ten Mile Island Condominiums located further to the north. A variety of commercial/office and residential uses exist to the west of the project including medical offices, townhome and condominium developments, retail/office space, the Summit Daily newspaper, and Tavern West. To the east are paired homes and townhomes, with the Post Office located at the end of the block. The southern edge of the property is bounded by West Main Street with residential, institutional, and commercial uses across the street, including townhome and multi-family development, a preschool, and The Peak School. This site directly connects to the Summit County Rec. Path, providing direct access to this public amenity for pedestrian and recreational users alike. Bus stops along Summit Stage's Copper Mountain route are located adjacent to the site along Main, while stops for the Breckenridge route are approximately a half-mile east on Main Street. Dozens of businesses are located within a half-mile radius of the property along Main, Galena, and Granite streets.

### Vision

The vision for this site is to redevelop 101 W. Main Street by activating a crucial property of Frisco's main street corridor with 100% workforce housing units and new commercial space. The goal of the project is to provide deed-restricted, rental workforce housing in perpetuity to support the fabric of the community. By embracing Frisco's multi-modal transportation network, tenants will have direct access to nearby bus stations and the walkability of the town. Providing desirable and affordable housing resources to the local workforce will activate the western end of Main Street and meet local needs to maintain healthy and affordable living.



Photo Credit: Town of Frisco

The site is being developed by the NHP Foundation in partnership with the Town of Frisco. Active since 1989, the NHP Foundation is a nonprofit affordable housing developer, owner, and operator with a current portfolio of approximately 10,000 units across 16 states. NHPF's mission is to create sustainable, service-enriched housing that is affordable to low- and moderate-income families and seniors, and beneficial to their communities. We seek to keep our housing affordable in perpetuity and partner with local governments, churches, nonprofits, and service providers to build local capacity and amplify our impact.

Like many mountain resort communities, Frisco is struggling to retain its workforce as housing prices soar. This impacts local businesses, who are unable to keep their doors open without staff. Residents of all income levels are in turn negatively affected as they lose access to important services. A mix of studios, one-bedrooms, and two-bedrooms will be provided at rents affordable to households earning from 30 – 80% AMI, making them affordable to hospitality/resort workers, retail employees, school staff, public servants, and healthcare workers.

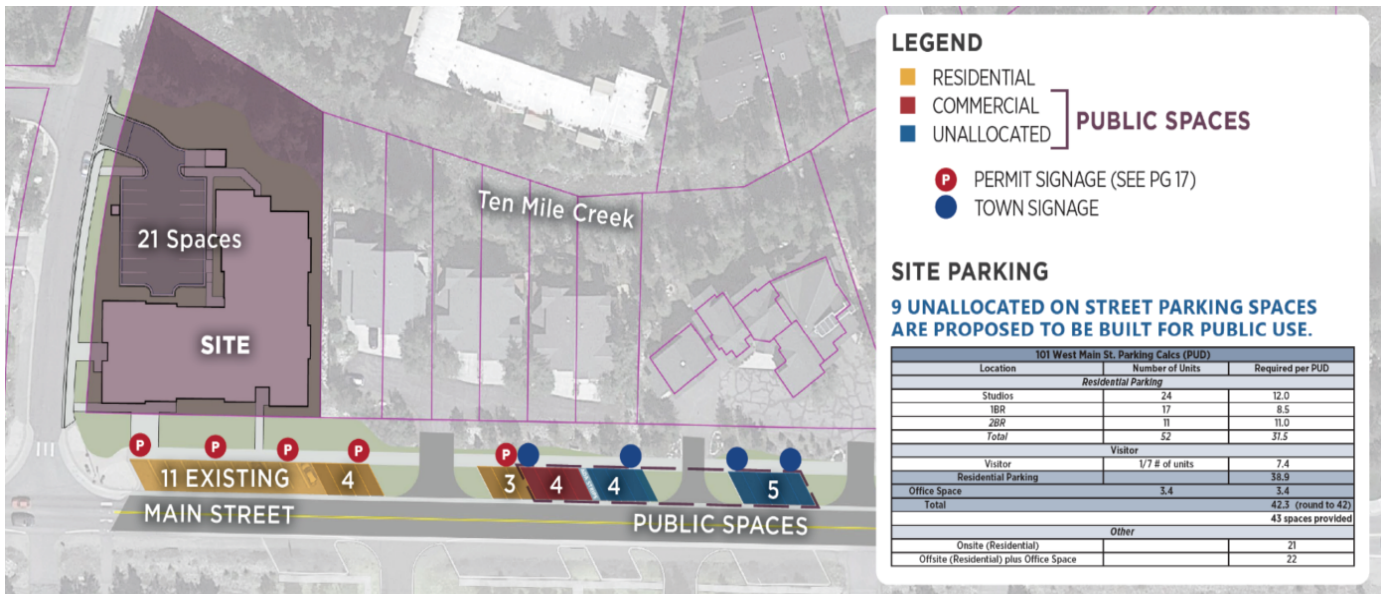
### Proposed Site Plan

Mixed uses are proposed on the site, including leasable non-residential space and multi-family residential uses/space. Multi-family residential is the primary proposed land use with 52 deed restricted residential dwelling units ranging from 481 to 882 square feet, averaging 626 square feet. Within the same building, a secondary use of commercial space shall comprise approximately 1,546 square feet. The 52 deed restricted units and the minimum of 1,500 sq. ft. of commercial space meet the requirements established in the PUD.

The proposed building will be brought closer to W. Main Street. Situated nearer to the street, the structure will add critical mass to W. Main Street, embracing the streetscape to contribute to a pedestrian-friendly urban corridor. In addition to creating a more engaging, welcoming pedestrian experience along West Main Street, this site plan will provide public access through the site to the Tenmile Creek through a trail easement. In pulling the built form toward Main Street, this proposed site plan will also create an increased buffer from the Tenmile Creek, some of which will be dedicated as public easement at a future date. This will allow for further construction of the Tenmile Creek Trail, as proposed by the Frisco Master Trail Plan. The mix of uses, site layout, and future access to Tenmile Creek will activate this end of Main Street, fostering the Town's stated goals of expanding the pedestrian-friendly urban corridor, as well as expanding access to natural and recreational resources.

The architecture proposed on this site fits the mountain character of Frisco. The proposed building will front Main Street and hold the edge of the street in a historic grid pattern. The gabled roofs create a rhythm to the building that draws from historic architecture within the town. Natural building materials and glazing reinforce the architectural language of the Main Street corridor. The building meets the development standards outlined in the PUD, as well as the goals of the Community Plan.

Previous development has left a depression in the middle of the site, creating drainage issues. We are proposing to fill the site back to natural grade to attain proper surface drainage. In following the Unified Development Code, our team is using the exception to the building height calculations which states that when fill is required for drainage purposes, "in the case where a building is proposed adjacent to two or more rights-of-way, then the building height will be calculated using the average grade of each right-of-way as measured at the edge of asphalt".



101 West Main St. Parking Calcs (PUD)		
Location	Number of Units	Required per PUD
<b>Residential Parking</b>		
Studios	24	12.0
1BR	17	8.5
2BR	11	11.0
<b>Total</b>	<b>52</b>	<b>31.5</b>
<b>Visitor</b>		
Visitor	1/7 # of units	7.4
<b>Residential Parking</b>		<b>38.9</b>
<b>Office Space</b>	<b>3.4</b>	<b>3.4</b>
<b>Total</b>		<b>42.3 (round to 42)</b>
		<b>43 spaces provided</b>
<b>Provided Parking</b>		
On-site (Residential)		13.0
On-site (Car Share)	4x per actual space	8.0
<b>Provided On-Site Parking</b>		<b>21.0</b>
<b>Off-site (Residential and Office)</b>		<b>22.0</b>
<b>Provided Parking</b>		<b>43.0</b>
<b>Off-site (Community Benefit)</b>		<b>9.0</b>
<b>Total</b>		<b>52.0</b>

Enlarged for legibility

We propose to use the grade at the edge of asphalt at Creekside Drive and at Main Street to create an average grade plane, from which we have taken our building height and bulk plane measurements. The exception requires that we have our solution verified by a licensed engineer and approved by the Frisco Town Engineer; both conditions will be satisfied at the time of our final submittal.

The site plan proposes twenty-one (21) parking spaces on-site, and thirty-one (31) parking spaces located adjacent to the site in the right-of-way along Main Street. This development will utilize existing parking along Main Street and further develop twenty (20) parking spaces in collaboration with the Town of

Frisco's Public Works Department. Of the fifty-two (52) spaces provided, thirty-two (32) spaces are provided for residents, seven (7) are provided for visitors, and four (4) are provided for office space non-residential uses. The remaining nine (9) spaces will be developed as a community benefit. All parking requirements set forth in the PUD are met either on site or in the right-of-way per collaboration with the Town of Frisco Public Works Department.

See the table and exhibit on page 4 for a graphic breakdown of the parking provided for this project. A full parking plan is included as a supplement to this site plan application.



## **Frisco Community Plan Fulfillment**

### **Guiding Principle 1 - Inclusive Community**

The primary goal of this project is to provide 100% workforce housing, significantly supporting the Inclusive Community principle. By bringing this project to fruition, an example will be set for collaboration between “community partners and the private sector to expand the reach of Frisco’s workforce housing efforts.” This redevelopment will contribute to a positive balance of full-time residents. The proposed quality architecture and landscape design will strengthen Main Street and the neighborhood character; thus, supporting “the creation of an attractive community entrance along W. Main Street to create a gateway that is inviting to visitors.” This project will enrich the character and scale of the town core. By placing the proposed building strategically closer to W. Main Street, the proposed site plan will restore Frisco’s historic street grid to “retain the character and walkability of the town core.” With a publicly accessible internal circulation and future trail access along Main Street and Tenmile Creek, the project will contribute desirable public space and create a more engaging, attractive streetscape.

### **Guiding Principle 2 - Thriving Economy**

The workforce housing proposed by this redevelopment will support local businesses, employees, residents, and visitors alike. A common community goal will be met by providing much needed housing for local workers. This project will support local business by providing affordable housing resources in proximity to Main Street, encouraging local workers to remain employed in Frisco or pursue employment in Frisco. This project will set another precedent for “supporting efficient regulations and processes that foster a supportive business environment.”

### **Guiding Principle 3 - Quality Core Services**

This project will deliver a product that meets the mission of the Town of Frisco, as stated by the Community Plan:

“Frisco is a place for people to live and work...core services help businesses recruit workers, contribute to a tight knit community, and ensure the year-round vitality of our economy.”

As the developer of the proposed project and a publicly supported not-for-profit real estate organization, NHPF will deliver a product that helps develop the quality core services which support the Frisco community. Enabling workers to live in Frisco strengthens the community and makes employment in Frisco more attractive and accessible. Providing housing resources on W. Main Street will increase the year-round vitality of the Frisco economy by supporting workers and consumers who will live in the community and engage with the economy year-round.

### **Guiding Principle 4 - Mobility**

This redevelopment encourages the use of Frisco’s multi-modal transportation network with the proximity to bus stations, proposed bike racks on-site and the walkability of Main Street. With the improvements to the site, this project will contribute to the “Central Core for the construction of more developed, urban streetscape.” By providing an easement for the future Tenmile Creek Trail, the proposed site plan helps to improve neighborhood and commercial area connections, as well as the recreational pathway system.

### **Guiding Principle 5 - Vibrant Recreation**

This project proposes a publicly accessible pedestrian connectivity, as well as bike racks and future trail connections to build upon the Town of Frisco’s recreation amenities. The proposed site plan will also lay the groundwork for the future Tenmile Creek Trail. With the project’s new construction, the service and network reliability of the site will be improved in relation to existing and future public infrastructure. This site already offers residents access to Frisco’s recreational resources such as the Kayak Park (0.3mi), Walter Byron Park (0.6mi), and Frisco Bay Marina (0.9mi). This site also offers direct access to the Summit County Recreation Path and lies in close proximity to ‘Frisco’s Backyard’ trail network including the North Tenmile Creek Trailhead (0.4mi) and Rainbow Lake Trailhead (0.7mi). To ensure appropriate connections to these resources, the project team will work with town staff to deliver exceptional access and quality trail signage.

### **Guiding Principle 6 - Sustainable Environment**

The new construction will provide updated infrastructure, replacing an outdated building built in 1979. As part of this process, the development team plans to design the building to the Zero Energy Ready Home standard for multifamily structures and design an all-electric building. The site plan proposes a generous setback from Tenmile Creek to properly protect ground water and sensitive riparian areas. Also, the proposed landscape plan identifies native plants and species to enhance the local environment and minimize irrigation needs. Located adjacent to a bus stop serving several bus routes and within walking distance of Main Street, the site offers an excellent opportunity for local employees to use alternative transportation modes. This site enables residents to be an active member of the Frisco community without the need to travel by car.



Photo Credit: Town of Frisco

### **Frisco Strategic Plan Contributions**

This project will support the majority of the Strategic Plan’s high-priority goals. Providing 100% workforce housing will increase full-time residents. Redevelopment of the site will include on-site and off-site infrastructure improvements that will strengthen infrastructure resiliency, including fiber/telecom system upgrades. Placing long-term housing units on this site will further develop the Main Street economy, supporting local business. Deed-restricting the rental units to households between 30% and 80% AMI will provide housing options for workers, further supporting local business and meeting the goal of building an inclusive community by ensuring there is a balance of housing options for residents of all income levels.

### **Workforce Housing Needs Assessment**

Per the 2020 assessment released by the Summit Combined Housing Authority (SCHA), year-round business growth and retirement of resident workforce have increased the need for permanent residential housing. Additional rental units are in high demand at 80% area median income (AMI) and below. Summit County had a housing gap of approximately 1,200 units in 2020, projected to swell to over 2,700 units by 2023. This deficit results in rising housing costs and increasing housing poverty for Summit County workers. The Ten Mile Basin area has the second largest gap of the four major basin areas identified in the assessment with a net gap of 600 housing units, a gap which is projected to grow in 2023. This project aims to address the growing issue of housing affordability and access in the Ten Mile Basin.

### **Conclusion**

The proposed development at 101 W. Main Street offers the Town of Frisco a unique opportunity to make meaningful progress on several community goals through the redevelopment of a single site. This project will create an engaging streetscape for pedestrians, attracting greater foot traffic further west down Main. The opportunity to bring 100% deed-restricted affordable homes to Main Street will be a transformative development in the Frisco housing market. This project will provide desirable and affordable housing resources to the local workforce strengthening the fabric of the Frisco community.



Photo Credit: Town of Frisco



## TREE IMPACT WITH MAIN STREET IMPROVEMENTS

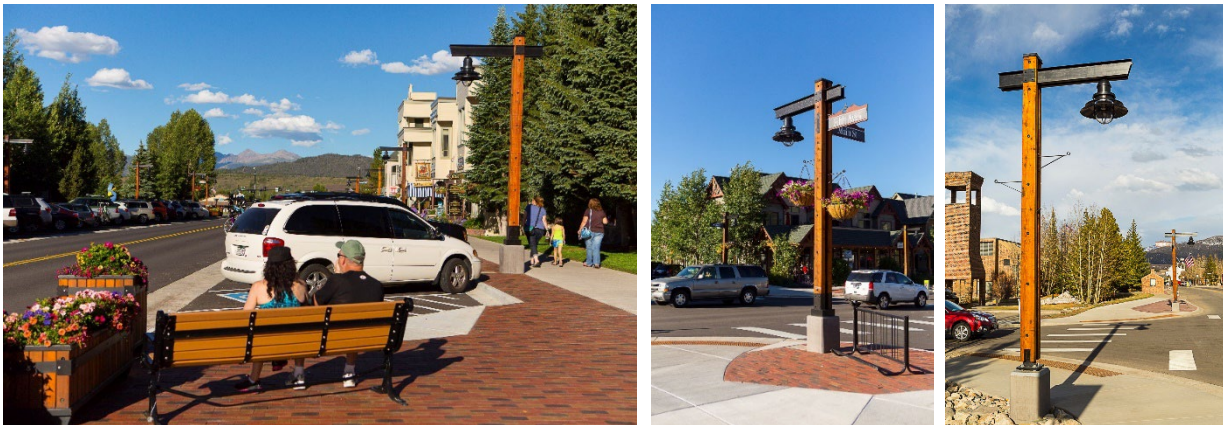
Date: 05/29/2024

To: Emily Weber, Principal Planner

From: Megan Testin, Principal Norris Design; Andrew Rostek, Associate

**Re: MAJ-23-0011 (101 W Main Street) Staff require for narrative to explain tree impacts associated with the proposed Main Street improvements.**

In association with the site improvements for MAJ-23-0011 (101 W Main Street), the project has proposed improvements along the north side of West Main Street between the Post Office and Creekside Drive. These improvements mirror the improved streetscape design found between Madison Avenue and Summit Boulevard (shown below). Updating the design will help to improve the continuity of entry experience from the Western entrance into town and will improve accessibility for visitors and residents.



These improvements will necessitate the removal of the existing trees that are located within the right of way or Main Street to accommodate the parking and right of way improvements. Based on a conservative approach toward tree projection, we are expecting to lose 37 existing trees along the 500' stretch of roadway. We are anticipating the ability to preserve 33 of the existing trees in the right of way, and we are looking to avoid impacting trees on neighboring properties.

Many of the trees that are being removed are located between the street and detached sidewalk and are in poor health or failing condition (photos below). We are looking to preserve the trees located behind the existing path as much as possible.





The design proposed looks to achieve a similar level of screening and density as found in front of the Frisco Town Hall with the addition of 37 trees. If significant trees must be removed during construction that were not anticipated in the design documents, the Landscape architect will work with ownership to provide additional tree locations to maintain the desired design intent.



Overall the project will look to improve the cohesive design language that the town has established along Main Street and provide parking for both the site and for the public while respecting the need to buffer adjacent residential uses.

**101 W Main Housing Development****Unit Mix and Affordability Levels**

101 W Main will comply with the requirements of Section 180-5.5.1 of the Town Code of Ordinances by renting all units on the property at rates affordable to households making at or below 120% of the Area Median Income (AMI). The table below shows the unit mix and proposed AMI levels.

<b>Unit Type</b>	<b>Total Units</b>	<b>Units up to 30% AMI</b>	<b>Units up to 50% AMI</b>	<b>Units up to 60% AMI</b>	<b>Units up to 80% AMI</b>
Studio	24	2	11	3	8
1 Bedroom	17	2	8	1	6
2 Bedroom	11	1	5	1	4
Total	52 units	5	24	5	18

Average AMI served = 59.4%



## MEMORANDUM

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**P.O. Box 4100 ♦ FRISCO, COLORADO 80443**

**TO:** EMILY WEBER, PRINCIPAL PLANNER  
**FROM:** CHRISTOPHER MCGINNIS, PUBLIC WORKS DIRECTOR/TOWN ENGINEER  
**RE:** MAJ-23-0011(101 W MAIN STREET) TOWN ENGINEER REVIEW – 5/13/24 SUBMITTALS  
**DATE:** MAY 21, 2024

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The MAJ-23-0011 submittals dated 12/12/23 were reviewed for general conformance with Town Code, standards, and general engineering principles and comments were sent in January 2024. Submittals reviewed included the civil plans, geotechnical report, traffic impact study, and wetland disturbance permit/report. Revised documents were then submitted on 5/13/24 and reviewed. New comments were added to the plans and also summarized below:

1. The proposed easement adjacent to Ten Mile Creek is labeled as a walkway easement and located almost entirely within edge of water boundary. As shown, the easement would likely be unusable for a walkway. For this reason, it is recommended to change to an “open space and walkway easement” and/or modify the easement to the south.
2. The Frisco Trails Master Plan identifies Creekside Drive as a roadway recommended for a shared use path. Standards require a path to be 10’ wide to be classified as a shared use path, requiring the Creekside sidewalk width to be increased to 10’.
3. Existing bollard lights shall be removed.
4. Curb ramps to be modified to meet code at Creekside Lane. Ramp to be added at perpendicular ADA space on Main Street.
5. Pan needs to be removed and replaced with curb at Creekside/Main Street intersection bulbout per ADA requirements.
6. 10’ separation between water and sewer required.
7. Existing hydrant to be reconnected to new line or new hydrant installed.
8. Summit Fire comment response states that a hydrant is being added to the SE corner of site, but no new hydrant is shown.
9. Sewer manhole to be relocated outside sidewalk.

Final engineering reviews will also be required prior to issuance of building permits and right-of-way permits.

The project contains many unique site conditions and exemptions. These conditions/exemptions are recommended for approval and discussed below.

### ***Building Height***

The project calculated building heights using proposed finished grade at the foundation rather than using existing grades. Section 180-9.3 states that to use proposed grade for height calculations, the following shall be met: *“For the purposes of calculating building height, any placement of fill on a lot must be determined as necessary, showing there are no other viable alternatives. Said determination and showing must be verified by a professional engineer licensed in the State of Colorado and approved by the Frisco Town Engineer.”* The project was determined to meet the exemption above since the existing site grades are lower than the adjacent roads, properties, and riverbank. These existing grades would have resulted in a site without positive drainage, ponding of water, and increased flooding risk. The proposed plan to raise the site grades to adjacent elevations results in improved stormwater management and decreased flood risk. The applicant provided a letter from a licensed engineer, describing the need for fill and the absence of a viable alternative, which was approved by the Town Engineer.

### **Water Quality Protection**

Section 180-6.7 creates limitations of disturbance within 25 feet of a water body, defined by the ordinary high-water mark. The project proposes to complete building demolition, construct a detention pond, and add a sidewalk within the setback. These proposed improvements in the setback were determined to be acceptable for the following reasons:

- The setback was measured from the 100-year floodplain. This was a conservative approach as the ordinary high-water mark would be less restrictive and if used, the encroachment into the setback would be further reduced.
- The sidewalk only encroaches approximately 1 foot into the setback and meets exemption 4 of Section 180-6.7 (public benefit is greater than the impact to wetlands).
- The detention pond/rain garden will improve water quality prior to discharging to Ten Mile Creek by increasing groundwater recharge, reducing flood events, and reducing contaminants from runoff (exemption 7 of Section 180-6.7).
- The existing building is currently located in the setback. The proposed development would create a net reduction of encroachments in the setback and improves the natural quality of the area.
- A wetlands delineation was completed in May 2024 and wetlands were identified on the site. However, no disturbance is proposed within the wetlands, and the only disturbance shown in the wetlands is the rain garden. The rain garden will improve water quality and increase groundwater recharge to the wetlands, so the disturbance in the setback will be permitted.

### **Parking Management Plan**

A parking management plan was submitted to offset a portion of the off-street parking requirement by adding twenty on-street parking spaces and utilizing eighteen of the on-streets parking spaces for residents. The applicant submitted a parking management plan that was reviewed and approved by Public Works. However, the Town is currently developing new on-street and overnight parking management policies and still in the testing phase of a pilot program in the downtown area. The Town may require the applicant to revise the parking management plan prior to issuing certificate of occupancy to be consistent with the Town’s pilot program. The parking changes could include a change to non-permitted parking with alternating sides of Main Street in winter, similar to the parking management being tested on 2<sup>nd</sup> Ave and 3<sup>rd</sup> Ave currently.

### **Utilities**

There are utility conflicts on the site requiring relocation. A sanitary sewer line is located within an easement on the site which conflicts with the proposed building. The applicant proposes to extinguish this easement and relocate the sewer line into the Town right-of-way. Public Works has approved the sewer relocation into the right-of-way. Additionally, there is an existing Town water main line located on the parcel (without an easement), that would be closer to the building than allowed by Town code. The revised submittals show the water main being relocated into Creekside Drive. The water main relocation will be constructed by the applicant.





## MEMORANDUM

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P.O. Box 4100 ♦ FRISCO, COLORADO 80443

**TO:** EMILY WEBER, PRINCIPAL PLANNER  
**FROM:** CHRISTOPHER MCGINNIS, TOWN ENGINEER  
**RE:** MAJ-23-0011 (101 W MAIN STREET) TOWN ENGINEER REVIEW  
**DATE:** JANUARY 10, 2024

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The MAJ-23-0011 submittals dated 12/12/23 were reviewed for general conformance with Town Code, standards, and general engineering principles. Submittals reviewed included the civil plans, geotechnical report, traffic impact study, and wetland disturbance permit/report. Comments were added to the plans and sent to the applicant on 1/10/24; these comments shall be addressed by the applicant and plans resubmitted prior to final approval. Final engineering reviews will also be required prior to issuance of building permits, wetlands disturbance permit, and right-of-way permits.

The project contains many unique site conditions and exemptions. These conditions/exemptions are recommended for approval and discussed below.

### ***Building Height***

The project calculated building heights using proposed finished grade at the foundation rather than using existing grades. Section 180-9.3 states that to use proposed grade for height calculations, the following shall be met: *“For the purposes of calculating building height, any placement of fill on a lot must be determined as necessary, showing there are no other viable alternatives. Said determination and showing must be verified by a professional engineer licensed in the State of Colorado and approved by the Frisco Town Engineer.”* The project was determined to meet the exemption above since the existing site grades are lower than the adjacent roads, properties, and riverbank. These existing grades would have resulted in a site without positive drainage, ponding of water, and increased flooding risk. The proposed plan to raise the site grades to adjacent elevations results in improved stormwater management and decreased flood risk. The applicant provided a letter from a licensed engineer, describing the need for fill and the absence of a viable alternative, which was approved by the Town Engineer.

### **Water Quality Protection**

Section 180-6.7 creates limitations of disturbance within 25 feet of a water body, defined by the ordinary high-water mark. The project proposes to complete building demolition, construct a detention pond, and add a sidewalk within the setback. These proposed improvements in the setback were determined to be acceptable for the following reasons:

- The setback was measured from the 100-year floodplain. This was a conservative approach as the ordinary high-water mark would be less restrictive and if used, the encroachment into the setback would be further reduced.
- The sidewalk only encroaches approximately 1 foot into the setback and meets exemption 4 of Section 180-6.7 (public benefit is greater than the impact to wetlands).

- The detention pond/rain garden will improve water quality prior to discharging to Ten Mile Creek by increasing groundwater recharge, reducing flood events, and reducing contaminants from runoff (exemption 7 of Section 180-6.7).
- The existing building is currently located in the setback. The proposed development would create a net reduction of encroachments in the setback and improves the natural quality of the area.
- The project has not completed a wetlands delineation report due to winter conditions which would result in an inaccurate delineation. The area was inspected and wetlands are not expected to be found on the site. However, a wetlands delineation will still be required to confirm absence of wetlands in the area. If wetlands were discovered, the drawings would be updated and the setbacks reexamined to determine if the exemptions were still met. The wetlands delineation report shall be submitted prior to building permit.

### **Parking Management Plan**

A parking management plan was submitted to offset a portion of the off-street parking requirement by adding twenty on-street parking spaces and utilizing eighteen of the on-streets parking spaces for residents. The applicant submitted a parking management plan that was reviewed and approved by Public Works. However, the Town is currently developing new on-street and overnight parking management policies. Based on the development of these policies, the Applicant may be required to revise the parking management plan prior to completion of the project.

### **Utilities**

There are utility conflicts on the site requiring relocation. A sanitary sewer line is located within an easement on the site which conflicts with the proposed building. The applicant proposes to extinguish this easement and relocate the sewer line into the Town right-of-way. Public Works has approved the sewer relocation into the right-of-way. Additionally, there is an existing Town water main line located on the parcel (without an easement), that would be closer to the building than allowed by Town code. The water main will likely need to be relocated into Creekside Drive right-of-way.



## INFORMATION SYSTEMS DEPARTMENT

970-668-4200

Post Office Box 5660  
County Commons  
0037 County Road 1005  
Frisco, Colorado, 80443

TO: Town of Frisco Planning Department  
FROM: Suzanne Kenney, GIS Services Lead  
SUBJECT: Project Review  
DATE: January 9, 2024

Property Location: 101 W. Main Street

Project Description: New mixed-use structure fifty-two workforce housing units and two offices.

### **Comments:**

The County requests that a new name be submitted for this proposed development. "101 West Main Street" will no longer make sense once this development receives a new address.

The redevelopment of this property will require a new address. The existing structure on the property is to be demolished to make room for the new construction. 101 W Main ST will be retired once the original structure is removed. This will ensure that there is no confusion between the new and old structure. The new proposed address for the new structure will be:

### **99 W Main ST Unit 101-118, 201-218, and 301-318**

These addresses will not be officially assigned until the site plan has been approved and a new subdivision/project name is approved. Please contact us with questions or concerns. The County GIS will await further feedback from the town and applicant.

Sincerely,

### **Suzanne Kenney, GISP**

GIS Services Lead  
Summit County Government  
Frisco, CO 80443  
(970) 668-4216  
Suzanne.Kenney@summitcountyco.gov



# SUMMIT FIRE & EMS

PO Box 4910  
Frisco, CO 80443  
(970) 262-5100  
www.summitfire.org

February 7, 2024

Ms. Emily Weber  
Principal Planner  
Town of Frisco  
PO Box 4100  
Frisco, CO 80443

**Re: 101 West Main Street Site Plan, Frisco, CO 80443**

Dear Ms. Weber

Thank you for the opportunity to review and comment on the above proposed site plan. The 2018 edition of the International Fire Code (IFC), as amended and adopted, is the fire code of record for this site plan and future permits. Summit Fire & EMS (SFE) has the following comments and concerns regarding the proposed project plans:

1. A Construction permit through Summit Fire & EMS is required for this this project. Please advise the developer/contractor to contact the fire department for details.
2. This project shall require the following fire protection systems: an approved fire sprinkler system, an approved fire alarm system, and an approved manual dry fire standpipe system.
3. RPZ backflow devices are required in the fire sprinkler system.
4. Please provide information on the location and size of the mechanical room that will house the fire sprinkler riser. Minimum spacing requirements per the IFC and the SFE Life Safety Policy are required.
5. Additional fire hydrants will be required for this project.
6. Bollard protection shall be required for all fire hydrants. A field inspection is required.
7. All Infrastructure as required by the adopted and amended International Fire Code (IFC) shall be in place, inspected and accepted by the Frisco Water Department prior to the start of any combustibile construction.
8. Snow storage areas and proposed landscaping throughout the site shall not visually or physically obstruct or hinder access to any fire hydrants and/or fire department appurtenances.
9. Please advise the developer/contractor to size the waterline to meet fire sprinkler and domestic water demand accordingly.
10. Overhead utilities, power lines and building exterior features shall not be located over or project into the fire apparatus access road width.
11. The fire department suggests a meeting with the developer and architect to discuss fire code requirements and fire protection system concepts for the buildings.
12. Based on the size of the building, type of construction and radio signal strength in the building, an emergency responder radio amplification system may be required. See SFE for details.

Kim J McDonald  
Division Chief/Fire Marshal  
Summit Fire & EMS

**From:** [msmith.fsd@gmail.com](mailto:msmith.fsd@gmail.com)  
**To:** [Weber, Emily](#)  
**Subject:** RE: Frisco Site Plan - Referral Comments for 101 W Main St  
**Date:** Thursday, January 25, 2024 8:44:08 AM  
**Attachments:** [image001.png](#)

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Emily,

I think I expressed all of my concerns during the DRC meeting. I don't have any further comments.

~Matt

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**From:** Weber, Emily <EmilyW@townoffrisco.com>  
**Sent:** Wednesday, January 24, 2024 9:59 AM  
**To:** Matt Smith <msmith.fsd@gmail.com>  
**Subject:** RE: Frisco Site Plan - Referral Comments for 101 W Main St

Hi Matt,

I'm following up on the application below. There was a slight delay with scheduling the meeting on our end but I'm hoping to have any comments by the end of the week. Thank you!

**Emily Weber** ([she/her](#)) | **Principal Planner | Community Development**



**Mailing PO Box 4100, Frisco, CO 80443**  
**Physical 1 Main Street, Frisco, CO 80443**  
**Email:** [emilyw@townoffrisco.com](mailto:emilyw@townoffrisco.com)  
**Office:** 970-668-9121  
[FriscoGov.com](http://FriscoGov.com)  
[TownofFrisco.com](http://TownofFrisco.com)

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**From:** Weber, Emily  
**Sent:** Friday, January 5, 2024 3:23 PM  
**To:** Matt Smith <[msmith.fsd@gmail.com](mailto:msmith.fsd@gmail.com)>  
**Subject:** Frisco Site Plan - Referral Comments for 101 W Main St

Hi Matt,

We have the final submittal for the workforce housing development located at 101 W Main Street. The site plan consists of one 52-unit building and on-site parking. Application materials can be found using this link: <https://www.frisco.gov/wp-content/uploads/2024/01/WestMain-Planning-121223.pdf> or can be accessed through Community Core.

Please review and provide any necessary revisions or approval by **Friday, January 19<sup>th</sup>**. Let me

**From:** [Lagace, Amy S](#)  
**To:** [Weber, Emily](#); [Frick, Jessica L](#); [Suzanne Allen-Sabo](#); [Andrew Stabile](#)  
**Subject:** RE: Frisco Site Plan Final Review - 101 W Main Street  
**Date:** Friday, February 16, 2024 4:18:17 PM  
**Attachments:** [image002.png](#)  
[image003.png](#)  
[image004.png](#)  
[image005.png](#)

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Hi Emily – sorry for the delay – I am adding [@Suzanne Allen-Sabo](#) and [@Andy Stabile](#).

Please let me know if you have any questions.

## Xcel Energy – Review Comments – Plat and/or Site Plan

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### Project Information:

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2/16/2024

Reviewing: 101 W Main Street – Civils & Architectural for Electric Distribution

Address: 101 W Main – Frisco CO

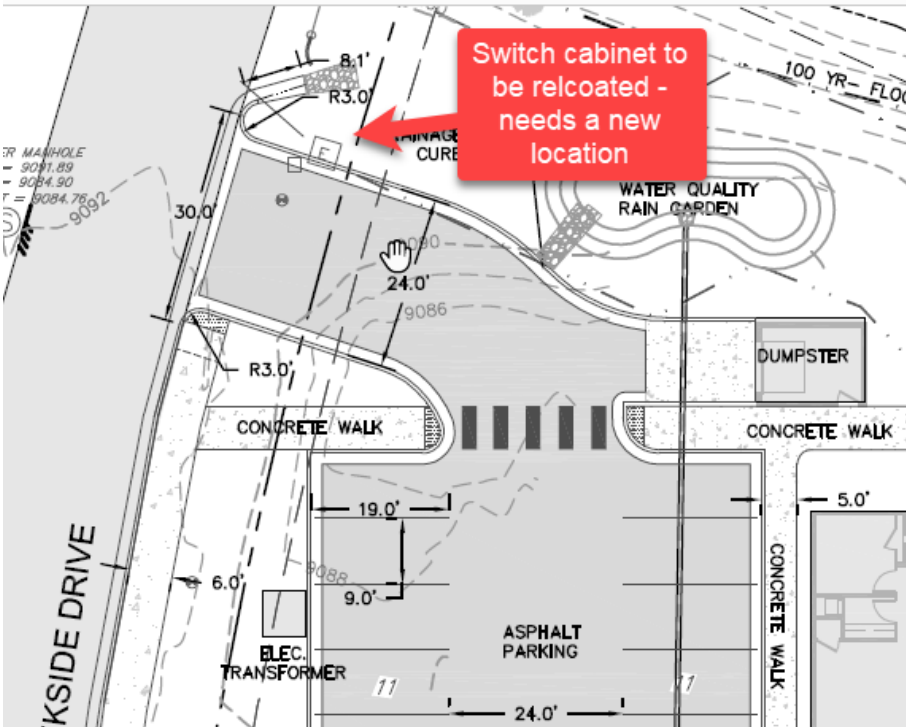
Customer did not indicate any proposed loads

### Existing Facilities

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- Known existing Xcel facilities onsite or near project
  - UG Primary and Gas Main is running North/South of west property line and East to west of south property line
  - Gas service to existing structure will need to be removed prior to start of construction
  - Transformer and switch cabinet on property will need to be removed and relocated prior to start of construction
  - Existing facilities are not sufficient to provide power/gas to new structures.
- If any grading or landscape work is planned over existing Xcel Facilities, Xcel will need the customer to have the lines surveyed and located to review for proper clearance and depth.
- Any encroachment/grade change +/-6" over the existing Xcel facilities in ROW UG and above ground will need to be reviewed to maintain proper clearances and depth.
- The existing switch cabinet will need a new location as it conflicts with the entrance. No proposed location on plans.
  - This will require a relocation application and the costs to relocate will be born by customer.
  - Cabinet will require 10' of clearance on both door sides (long sides) and 5' to the other side.

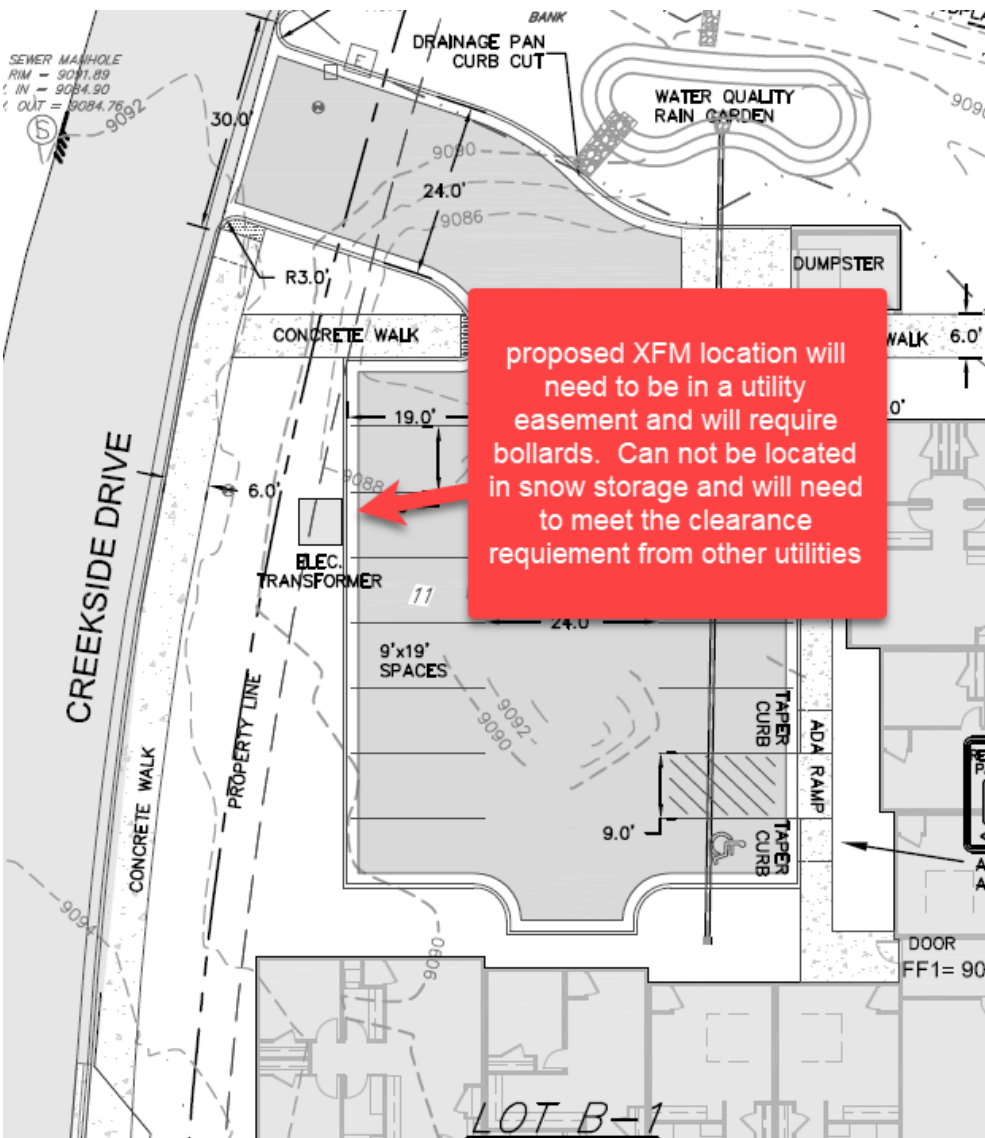
- Town will need to approve proposed location and confirm that it will not impede on driver visibility at the intersection.
- Please call 811 for locates before digging.



## Proposed Facilities

- Plan shows proposed transformer location on west side of property near parking area.
  - Note transformer CANNOT be place in dedicated snow storage area
  - Xcel's facilities will need to be installed on property in 10' wide dry utility easement.
  - Proposed location will require bollards
- Customer must ensure that all Company facilities meet all local setback and zoning requirements and remain accessible at all times for routine maintenance purposes.
- Customer to install sleeves at all culvert crossings if culverts are installed prior to Xcel's distribution.
  - 12" of separation required from bottom of culvert
  - Min of 36" from top of conduit to final grade
- Any GEO grading materials which are disturbed from the installation of Xcel's facilities will need to be repaired as needed by customer.





## Easements

Easements are required to accommodate Xcel facilities needed to serve. Size and location dependent on transformer size/location and trench routing.

## Gas and Electric Cable/Trench Clearances

Clearances required from buried Xcel facilities:

- 10' Min from Sewer and Water
- 1' Min Horizontal from Communications
- 1' vertical from gas and electric
- 5' Min from Gas pipe to any structure
- No private customer owned facilities allowed in Xcel's electric/gas trench

## Above Ground Equipment Locations and Clearances

All Above ground Xcel owned equipment:

- Must be located outside
- Cannot be located under any overhang (roof, balcony, stairs, etc)
- Requires safe access
  - Cannot be located under a drip edge
  - Must be accessible by a truck
- Cannot be placed in dedicated snow areas

Transformers require safety clearance from structures including:

- 10' from any combustible structures
  - 30" from non-combustible 2hr rated wall
- 1<sup>st</sup> story:
  - No exits within 20' (including garage doors when attached to structure)
  - No operable windows within 10'
- 2<sup>nd</sup> story and higher:
  - If the transformer is within 20' from building, there can be NO operable windows or doors on the 2<sup>nd</sup> story and higher in the 20' zone.

Further details relating specifically to transformers can be found the Xcel Energy Standard for Electric Installation and Use, Section 5. Refer to the Illustrations in the CR – Clearance requirement section, for typical pad-mounted transformer installation and clearance requirements.

## Meter Location and Required Clearances

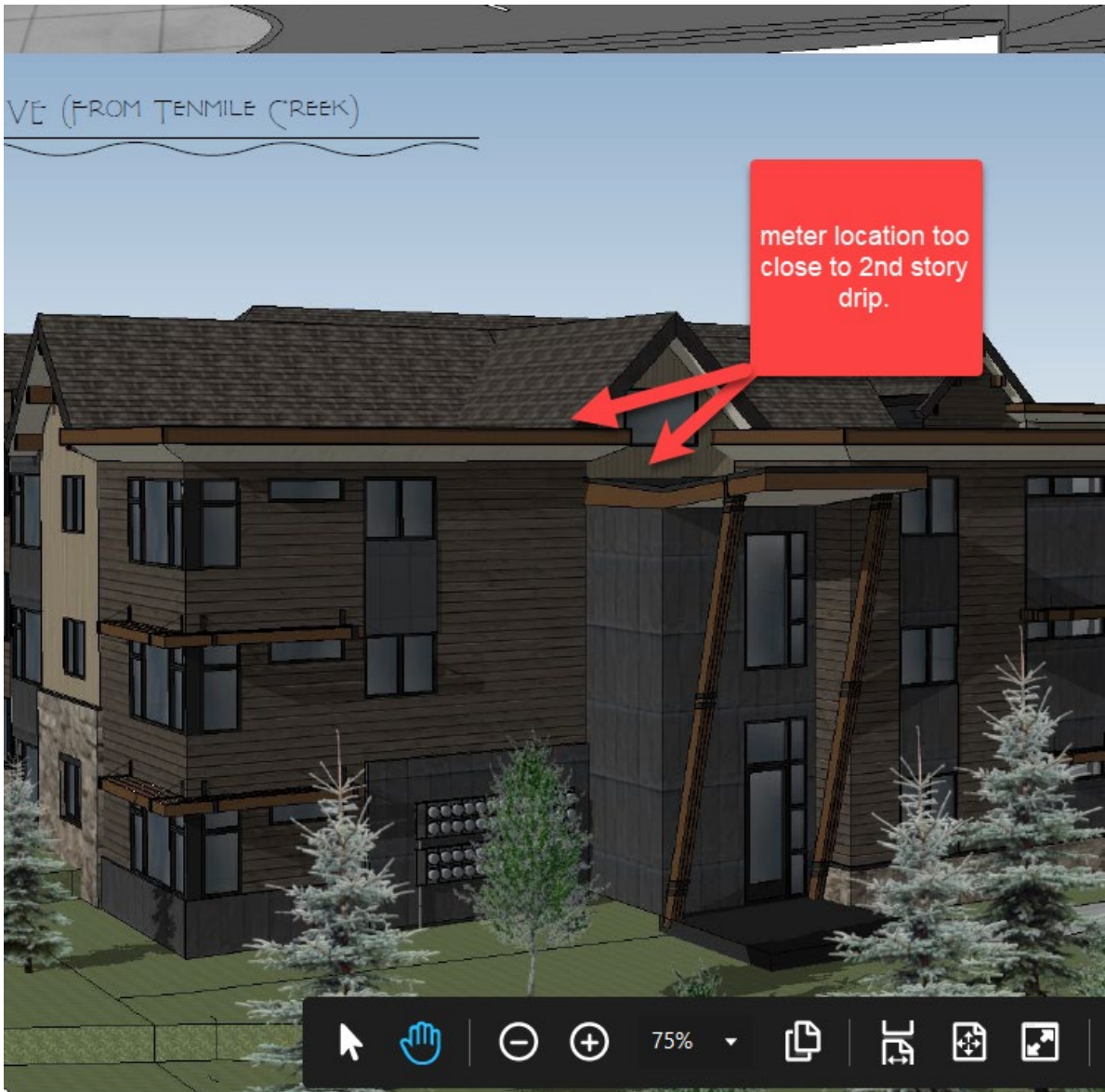
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All meter locations will need to be approved by Xcel. Proposed electric meter location indicated on elevation is not in an approved location.

- Meters shall be located outdoors, on the front 1/3<sup>rd</sup> of the structure with safe access under a non-drip edge(gable) wholly on the customers property.
  - Including CT enclosures
- Meters shall not be installed:
  - Where the meter will, in the Company's opinion, interfere with traffic on sidewalks, driveways, hallways or passageways.
  - Where the meter will, in the Company's opinion, obstruct the opening of doors or windows.
- In locations with heavy snowfall or ice loading and in locations above 6000 feet in elevation, all meters shall be installed on the gable or non-drip side of a building or other structure, and there shall be no adjacent rooflines, which will drip directly on or towards a neighboring meter installation.
- Xcel does not allow ice or snow shields in Summit County. Meters must be located under a non-drip edge and there shall be no adjacent rooflines, which will drip directly on or towards a neighboring meter installation.

- *Note: Due to excessive snowfall, ice and snow shields will not be permitted in the following Colorado counties: Eagle, Lake, Park and Summit. Meters shall be installed on the gable or non-drip side of a building or in an approved remote location from the building or structure in these counties.*

Further details relating specifically to meters and meter locations can be found the Xcel Energy Standard for Electric Installation and Use, Sections 4.14 thru 4.18 and the Illustrations on CR-10 and SC-20B



## Installation of Xcel Facilities

Xcel will install all Xcel owned facilities. Installation of Xcel facilities will start once a final design has been approved, paid for and the site is ready, equipment locations at

final grade and trench route at +/- 6" of final grade. The customer is responsible for staking the trench line and above ground equipment prior to start of construction. Also, all easements must be in place and staked prior to start of construction.

- Where existing slopes prohibit trenching, Customer must provide temporary grade for trenching equipment.
- Pouring/paving of roads, driveways, sidewalks, and landscaping must be delayed until after installation of facilities (services excluded).
- Water line, sewer lines septic systems, leach fields, and any other underground obstruction must be staked, flagged, and installed prior to Company gas and/or electric construction.
- Only shallow root vegetation allowed over any Xcel gas or electric lines. No trees or other tall vegetation in the front of the transformer doors.
- Transformers, switch cabinet locations, pedestals, gas regulator stations, meter installations, and other surface mounted equipment must be exact final grade. All other street/easements/service lateral routes must be within plus or minus six (6) inches of final grade.
- All roof drains must be directed away from Company equipment in a manner that prevents damage or settling of facilities, or both.
- If transformers, switch cabinets, or gas meters require bumper protection, Customer must install protection at Customer's sole cost. Customer must contact design engineer for bumper protection clearance requirements.
- When construction consists of five (5) sites or fewer, all sites must be ready. For projects with more than five (5) sites, approximately fifty (50) percent of the sites must be ready.
- As determined by Company, required property pins, necessary curve points, easements, proposed structures, and facility equipment locations must be staked and visible in the field.

If gas service is requested, the service line from the main to the structure will be installed/owned and maintained by Xcel.

- Please note no private customer owned facilities can join in gas trench.

If the electric services to the structures are anticipated to be commercial/customer owned – they will be installed/owned and maintained by the customer

- Please note –electric services require one point of service to structure, ie, transformer/Ped
- A transformer or Ped will be required to be placed on the property being served in utility easement and will be sized once final building loads have been received.

Additional information on the design and installation process can be found on our website here. [Planning Your Project \(xcelenergy.com\)](http://www.xcelenergy.com) & [Building & Remodeling \(xcelenergy.com\)](http://www.xcelenergy.com)

Design Layout and Estimate

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When the customer is ready to submit their application for a design, they will need to submit the following:

- Approved site plans (as required by local jurisdiction(s))
- Civil grading and utility plan with proposed transformer/PED locations
  - Please include deep and shallow utilities
- Final approved plat with required utility easements
- Landscape Plan
- Elevations of buildings – with proposed meter locations highlighted.
- Gas Loads
- Electric one line and panel schedule with loads –
  - Please indicate # of EVS and EV ready with kW per charges
  - Any electric heating needs
  - Please indicate if solar
    - If yes, please make sure to apply for review and approval through the Xcel Solar Rewards application process
      - [Solar\\*Rewards | Xcel Energy](#)
- Builders Call line application are needed for:
  - New Electric & Gas Distribution
  - DEMO for each service on property
  - Services – new applications for each residential service (as needed)

Online Application link - [Building and Remodeling | Partner Resources | Xcel Energy](#)

Also – please check out the free Xcel Energy programs for new building projects [New Building Programs \(xcelenergy.com\)](#)

Link to full version - [Xcel Energy Standards of Installation and Use Manual](#)

Please note – this is not a final assessment of what the request will entail. There may be additional things in the field I cannot see. Once an application has been submitted to XCEL we can start the full design process and identify the scope of work for this request.

Thank you,  
Amy Lagace  
Xcel Energy  
Designer

**Amy Lagace**  
**Xcel Energy**  
**Designer**

200 W. 6<sup>th</sup> St PO Box 1819, Silverthorne, CO 80498  
P: 970-262-4033  
E: [amy.lagace@xcelenergy.com](mailto:amy.lagace@xcelenergy.com)

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**XCELENERGY.COM**

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*My Office Hours: Tues - Fri 7:00am - 4:30pm*

#### Useful Links

**[Xcel Energy Standards of Installation and Use Manual](#) \*Updated Version – July 2022**

[Building and Remodeling](#) | [Partner Resources](#) | [Xcel Energy](#)

[Additional Xcel Customer Support](#)

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**From:** Weber, Emily <EmilyW@townoffrisco.com>  
**Sent:** Monday, January 29, 2024 1:48 PM  
**To:** Frick, Jessica L <Jessica.L.Frick@xcelenergy.com>  
**Cc:** Lagace, Amy S <Amy.Lagace@xcelenergy.com>  
**Subject:** RE: Frisco Site Plan Final Review - 101 W Main Street

Some people who received this message don't often get email from [emilyw@townoffrisco.com](mailto:emilyw@townoffrisco.com). [Learn why this is important](#)

**EXTERNAL - STOP & THINK** before opening links and attachments.

Hi Jess,

Checking in on the application for 101 W Main Street. We need any outstanding comments to be finalized before the application can go to Planning Commission. Your response and review are appreciated. Thanks!

**Emily Weber** ([she/her](#)) | **Principal Planner** | **Community Development**



Mailing PO Box 4100, Frisco, CO 80443  
Physical 1 Main Street, Frisco, CO 80443  
Email: [emilyw@townoffrisco.com](mailto:emilyw@townoffrisco.com)  
Office: 970-668-9121  
[FriscoGov.com](http://FriscoGov.com)  
[TownofFrisco.com](http://TownofFrisco.com)

---

**From:** Weber, Emily  
**Sent:** Wednesday, January 24, 2024 9:56 AM  
**To:** Frick, Jessica L <Jessica.L.Frick@xcelenergy.com>  
**Cc:** Amy Lagace <[amy.lagace@xcelenergy.com](mailto:amy.lagace@xcelenergy.com)>  
**Subject:** RE: Frisco Site Plan Final Review - 101 W Main Street

Hi Jess,

I'm following up on the application below. There was a slight delay with our meeting schedule for the application but I'm hoping to have comments by the end of the week. Thanks!

Emily Weber ([she/her](#)) | Principal Planner | Community Development



Mailing PO Box 4100, Frisco, CO 80443

Physical 1 Main Street, Frisco, CO 80443

Email: [emilyw@townoffrisco.com](mailto:emilyw@townoffrisco.com)

Office: 970-668-9121

[FriscoGov.com](http://FriscoGov.com)

[TownofFrisco.com](http://TownofFrisco.com)

---

**From:** Frick, Jessica L <[Jessica.L.Frick@xcelenergy.com](mailto:Jessica.L.Frick@xcelenergy.com)>

**Sent:** Monday, January 8, 2024 10:13 AM

**To:** Weber, Emily <[EmilyW@townoffrisco.com](mailto:EmilyW@townoffrisco.com)>

**Cc:** Amy Lagace <[amy.lagace@xcelenergy.com](mailto:amy.lagace@xcelenergy.com)>

**Subject:** RE: Frisco Site Plan Final Review - 101 W Main Street

I will get this to you by Friday!

**Jess Frick**

**Xcel Energy | Responsible By Nature**

Planner Thereafter; Mountain Design

Monday- Thursday 7:00a-5:30p MST

Email: [jessica.l.frick@xcelenergy.com](mailto:jessica.l.frick@xcelenergy.com)

Cell: 970-409-7257

Supervisor: Kyle Alsup; [Kyle.C.Alsup@xcelenergy.com](mailto:Kyle.C.Alsup@xcelenergy.com)

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**[WWW.XCELENERGY.COM](http://WWW.XCELENERGY.COM)**

BUILDERS CALL LINE: 800-628-2121 or [BCLCO@xcelenergy.com](mailto:BCLCO@xcelenergy.com)

CUSTOMER SERVICE: 800-895-4999

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Useful Links:

[CLICK HERE to Apply Online!](#)

[Call 811 Before You Dig!](#)

[Xcel Energy Standard for Installation- Click HERE!](#)

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**From:** Weber, Emily <[EmilyW@townoffrisco.com](mailto:EmilyW@townoffrisco.com)>

**Sent:** Friday, January 5, 2024 3:15 PM

**To:** Frick, Jessica L <[Jessica.L.Frick@xcelenergy.com](mailto:Jessica.L.Frick@xcelenergy.com)>

**Cc:** Lagace, Amy S <[Amy.Lagace@xcelenergy.com](mailto:Amy.Lagace@xcelenergy.com)>

**Subject:** Frisco Site Plan Final Review - 101 W Main Street

Some people who received this message don't often get email from [emilyw@townoffrisco.com](mailto:emilyw@townoffrisco.com). [Learn why this is important](#)

**EXTERNAL - STOP & THINK** before opening links and attachments.

Hi Jess,

We have another site plan application. This one is for the workforce housing development at 101 W Main Street in Frisco. The site plan consists of one 52-unit building and on-site parking. Application materials can be found using this link: <https://www.frisco.gov/wp-content/uploads/2024/01/WestMain-Planning-121223.pdf>

Can you please review and provide either revisions required or approval? This is the final stage of review so any changes to the transformer size/location needs to be completed before the site plan can be approved. I'm looking for a response by **Friday, January 19<sup>th</sup>**. Let me know if you have any questions. Thank you!

**Emily Weber** ([she/her](#)) | **Principal Planner | Community Development**



**Mailing PO Box 4100, Frisco, CO 80443**

**Physical 1 Main Street, Frisco, CO 80443**

**Email: [emilyw@townoffrisco.com](mailto:emilyw@townoffrisco.com)**

**Office: 970-668-9121**

**[FriscoGov.com](http://FriscoGov.com)**

**[TownofFrisco.com](http://TownofFrisco.com)**





## COMMENT RESPONSE LETTER

Date: 05/29/2024

To: Emily Weber, Principal Planner

From: Megan Testin, Principal Norris Design; Andrew Rostek, Associate

Re: MAJ-23-0011 (101 W Main Street) Town Engineer and Planning Review

### Town Engineer Review Comments:

1. The proposed easement adjacent to Ten Mile Creek is labeled as a walkway easement and located almost entirely within edge of water boundary. As shown, the easement would likely be unusable for a walkway. For this reason, it is recommended to change to an “open space and walkway easement” and/or modify the easement to the south.
  - a. Easement will be replated before permit as an “open space and walkway easement”.
2. The Frisco Trails Master Plan identifies Creekside Drive as a roadway recommended for a shared use path. Standards require a path to be 10’ wide to be classified as a shared use path, requiring the Creekside sidewalk width to be increased to 10’.
  - a. The Project Team is in conversation with town staff about the inclusion of a trail. Staff has recommended not including a trail at this time as it does not connect to existing trails.



3. Existing bollard lights shall be removed.
  - a. No existing lighting is to remain within the proposed improvement area. A note has been added to Civil plans.

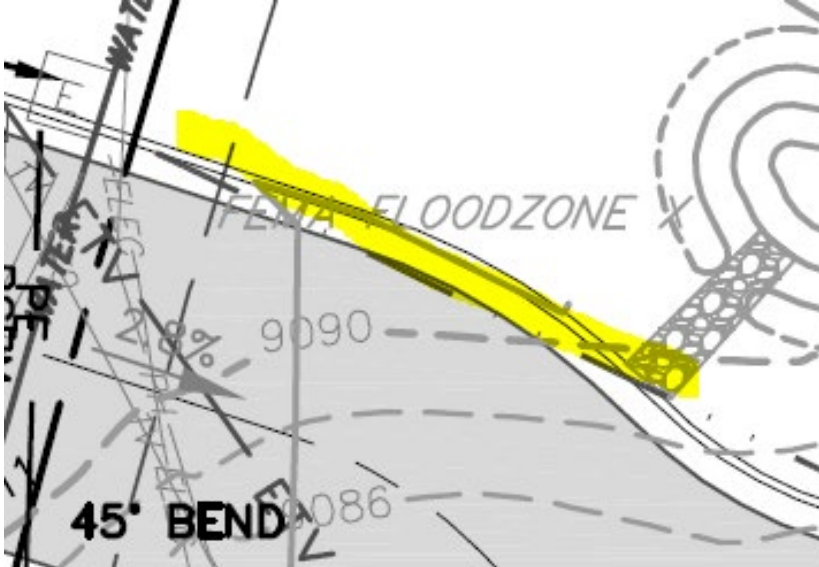


4. Curb ramps are modified to meet code at Creekside Lane. Ramp to be added at perpendicular ADA space on Main Street.
  - a. Curb ramp at Creekside Drive and Main Street updated to match updated code per town requirements.
  - b. A ramp has been added to access ADA spaces along Main Street.
5. Pan needs to be removed and replaced with curb at Creekside/Main Street intersection bulbout per ADA requirements.
  - a. Curb at Creekside Drive and Main Street updated to match updated code per town requirements. Existing pan to be removed.
6. 10' separation between water and sewer required.
  - a. Water and Sewer separation has been met within the Creekside Drive right of way.
7. Existing hydrant to be reconnected to new line or new hydrant installed.
  - a. Existing hydrant to be connected to proposed relocated water line. Hydrant to be installed at appropriate elevation as necessary to match proposed grade.
8. Summit Fire comment response states that a hydrant is being added to the SE corner of site, but no new hydrant is shown.
  - a. Hydrant has been removed from Architecture plans. Existing location to be maintained as it meets code.
9. Sewer manhole to be relocated outside sidewalk.
  - a. Top section of manhole and lid to be rotated outside of the sidewalk.



**Town Planning Review Comments:**

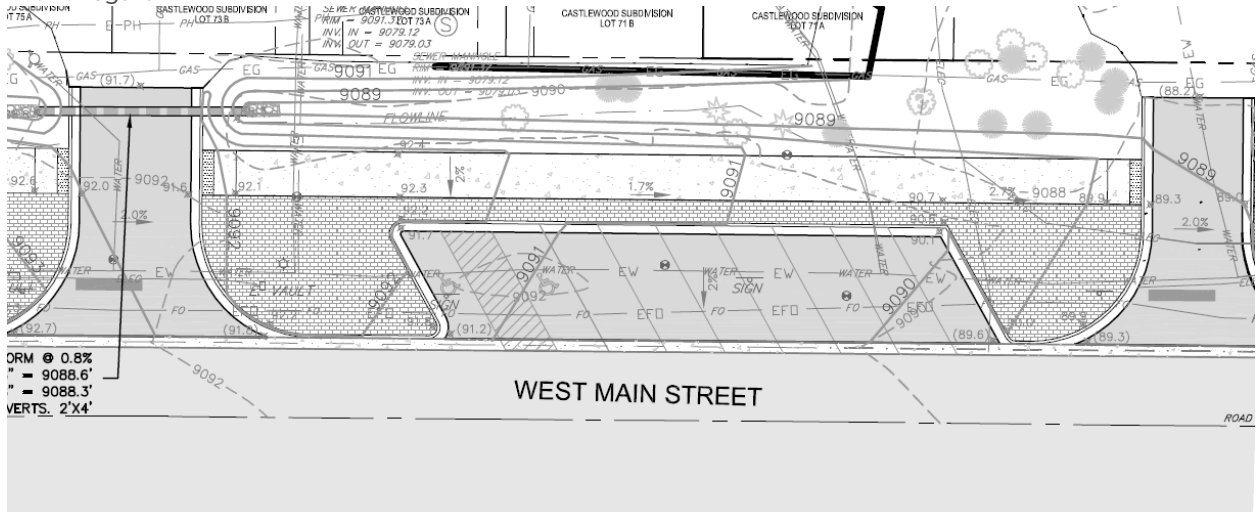
1. There is an encroachment into the wetland setback near the entrance into the site. This does not meet the exemption criteria and will have to be modified.



- a. The highlighted line is not the wetland setback. The line is 25' from the top of bank, and has been removed from the plans. No hardscape is proposed to encroach into the wetland setback.
2. The math for point K on the ridge height chart (sheet A1.1) is incorrect. This was noted in the sketch staff report.
    - a. Architecture has revised the elevation for point K.
  3. Are there two transformers for the site? The landscape plan shows one on the north side of the access into the site, but it is not shown on the civil plans.
    - a. Plans have been coordinated. Second transformer location is for downstream users, and is located outside of the 25' setback from the FEMA flood line.
  4. The snow storage calculation is incorrect and there is not enough shown on the site plan. This was noted in the sketch staff report.
    - a. Snow storage calculations have been corrected. Additional snow storage locations have been coordinated with staff and are reflected in the plans.
  5. The traffic study indicates that the proposed site consists of 48 units and 4,000 SF of commercial space. This is incorrect. Does this impact the number of vehicle trips? If it doesn't change the land use designation per the "Trip Generation Manual" this might be okay.
    - a. Traffic study has been updated to correct this typo. Change does not impact the number of trips calculated for the site.



6. The parking spaces located in the ROW are two less than what is shown in the PUD exhibit plan (the site still meets parking requirements but please make sure plans are consistent). We will modify the operating agreement when the development is closer to completion to reflect accurate numbers. Also, the accessible spaces located within the ROW may want to be relocated so the required residential spaces can be closer to the building. The civil plans and landscaping plans are inconsistent in this regard.



- Civil plans have been updated to match the Landscape Plans.
- ADA parking has been shown on the east side of the parking block (above) to place residential spaces closer to the building.
- The existing parking in front of the building is proposed to be restriped and expanded to include 17 spaces.
- Specific parking locations have been engineered to match the site conditions surveyed, which differed from the aerial imagery that the PUD exhibits are based on. The Project Team sought to prioritize total parking counts over locations, in keeping with the intent of the PUD.

TOF PW COMMENTS  
 COMMENTS BY CHRIS MCGINNIS  
 5/21/24

APPLICANT RESPONSE TO COMMENTS  
 5/30/24

# 101 WEST MAIN STREET



101 WEST MAIN STREET  
 LOT B-1, AMENDED WEST FRISCO 70, FILING #2  
 TOWN OF FRISCO, COLORADO

TITLE  
 COVER SHEET

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## PROJECT CODE STUDY

JURISDICTION	TOWN OF FRISCO
ZONE DISTRICT	101 W MAIN HOUSING PUD
ALLOWABLE DENSITY	45 UNITS MINIMUM
MAXIMUM LOT COVERAGE	80% MAX
FRONT YARD SETBACK	5'
SIDE YARD SETBACK	5'
REAR YARD SETBACK	10'
MAXIMUM BUILDING HEIGHT	49'-6"
CONSTRUCTION TYPE	V
FIRE SPRINKLER	YES

## LOCATION MAP



## PROJECT DIRECTORY

<b>OWNER</b> NHP FOUNDATION 122 EAST 42 STREET, SUITE 4900 NEW YORK, NEW YORK . 10168 T: 832.280.7554	<b>GENERAL CONTRACTOR</b> TBD
<b>ARCHITECT</b> ALLEN-GUERRA ARCHITECTURE 711 B GRANITE ST PO BOX 5540 FRISCO, COLORADO . 80443 T: 970.453.7002	<b>CIVIL ENGINEER</b> ALPINE ENGINEERING, INC 3451 O HWY 6 / UNIT A9 / PO BOX 97 EDWARDS, COLORADO . 81632 T: 970.926.3373
<b>LANDSCAPE ARCHITECT</b> NORRIS DESIGN 409 EAST MAIN STREET PO BOX 2320 FRISCO, COLORADO . 80443 T: 970.368.7068	<b>GEOTECHNICAL ENGINEER</b> KUMAR & ASSOCIATES, INC. 240 ANNIE ROAD PO DRAWER 1887 SILVERTHORNE, COLORADO . 80498 T: 970.468.1989
<b>SURVEYOR</b> RANGE WEST ENGINEERS & SURVEYORS, INC PO BOX 569 SILVERTHORNE, COLORADO . 80498 T: 970.468.6281	

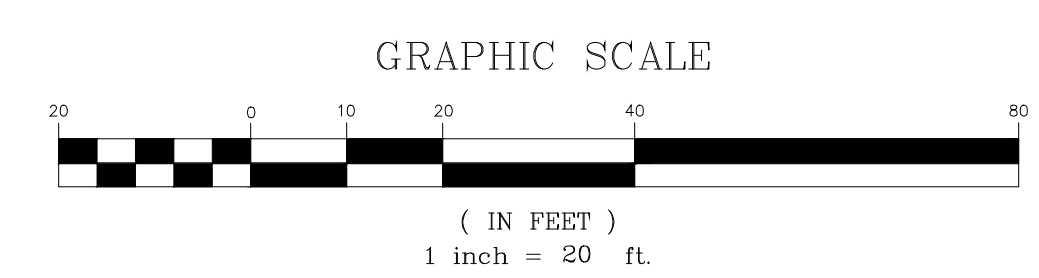
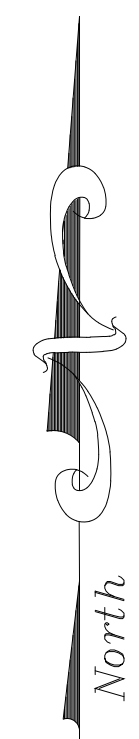
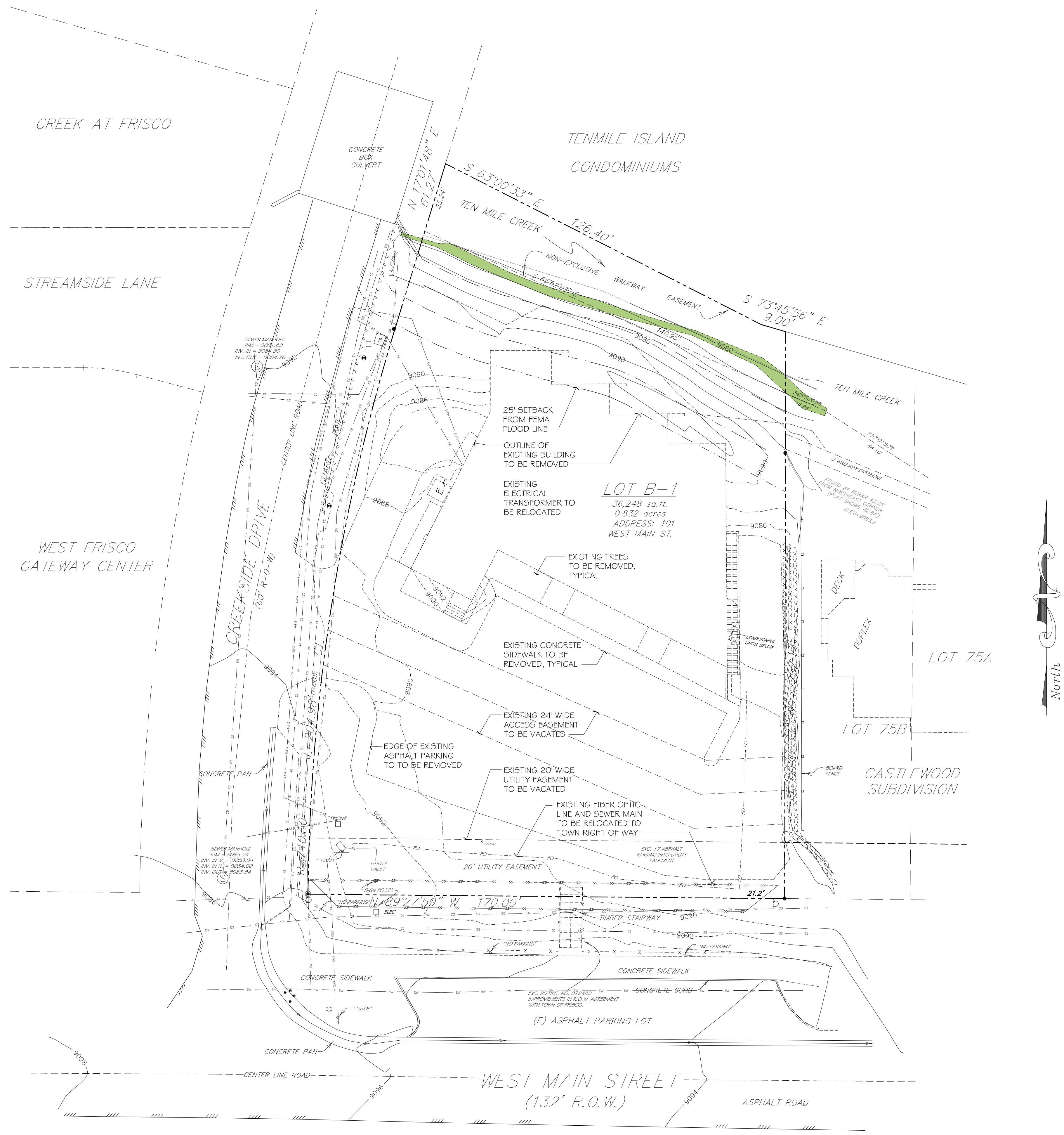
## SHEET INDEX

CS	COVER SHEET	A2.1	FIRST LEVEL FLOOR PLAN
A1.0	EXISTING SITE AND DEMO PLAN	A2.2	SECOND LEVEL FLOOR PLAN
A1.1	PROPOSED SITE PLAN	A2.3	THIRD LEVEL FLOOR PLAN
CM	CONSTRUCTION MANAGEMENT PLAN	A2.4	ROOF PLAN
C1.1	EXISTING CONDITIONS/DEMOLITION	A2.6	TYPICAL UNIT PLANS
C1.2	EXISTING CONDITIONS/DEMOLITION	A3.1	EXTERIOR ELEVATIONS
C1.3	SITE LAYOUT PLAN	A3.2	EXTERIOR ELEVATIONS
C2.0	GRADING AND DRAINAGE	A3.3	PERSPECTIVE RENDERINGS
C2.1	GRADING AND DRAINAGE	A3.4	BULK PLANE ENCROACHMENTS
C3.0	DRAINAGE AREA MAP	A3.5	EXTERIOR MATERIALS LEGEND
C3.1	STORMWATER CRITERIA	E01	LIGHTING SITE PLAN AND PHOTOMETRIC
C3.2	STORM SEWER PLAN	SURVEY	
C4.0	UTILITY PLAN		
C5.0	EROSION CONTROL PLAN		
C6.0	DETAILS		
C6.1	DETAILS		
C6.2	DETAILS		
L-001	LANDSCAPE NOTES		
L-002	LANDSCAPE SCHEDULE		
L-003	LANDSCAPE SCHEDULE		
L-101	LANDSCAPE PLAN		
L-501	LANDSCAPE DETAILS		
L-502	LANDSCAPE DETAILS		
L-503	LANDSCAPE DETAILS		
L-504	LANDSCAPE DETAILS		

ISSUE	DATE
DRC	7 DEC 2022
SKETCH	9 JAN 2023
SKETCH	3 MAY 2023
UPDATE	8 AUG 2023
DRC	26 OCT 2023
SKETCH	3 NOV 2023
PLANNING	13 MAY 2024

PROJECT # 22105

INFO



101 WEST MAIN STREET  
LOT B-1, AMENDED WEST FRISCO 70, FILING #2  
TOWN OF FRISCO, COLORADO

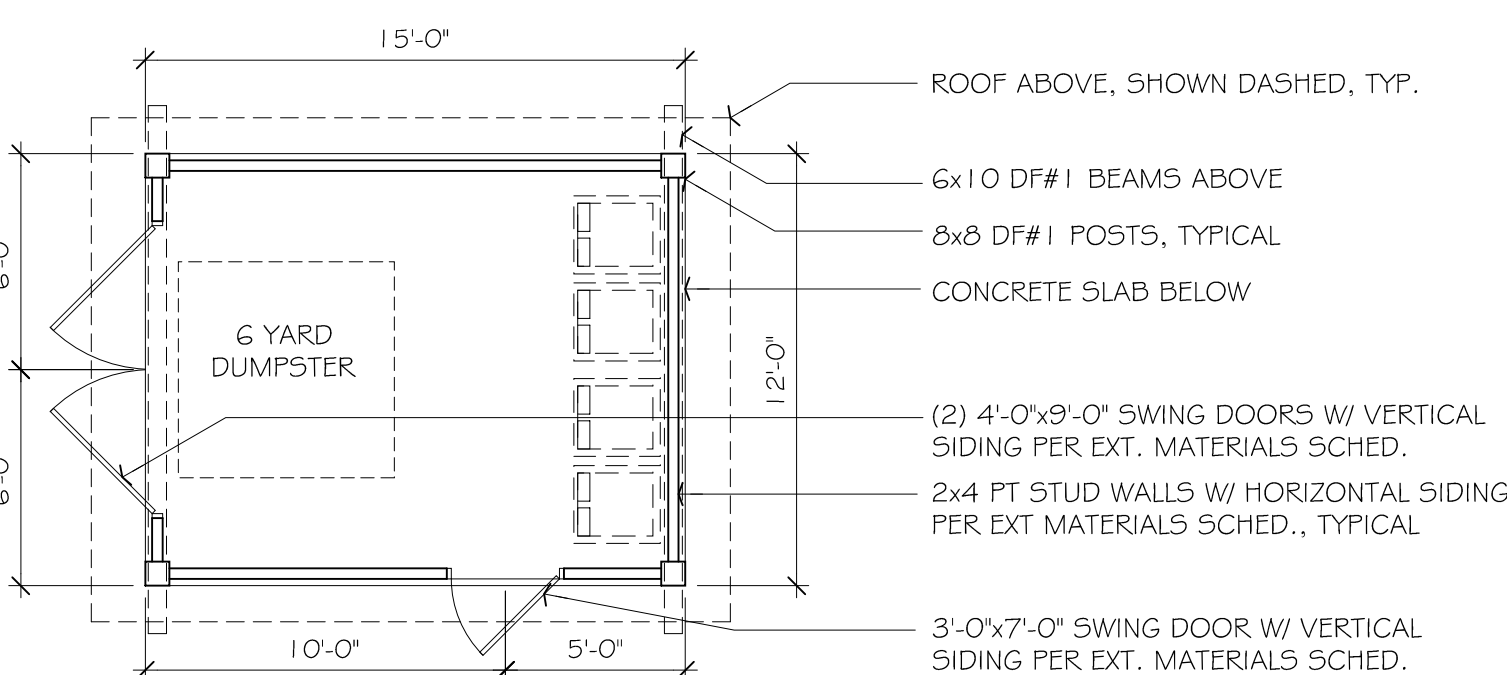
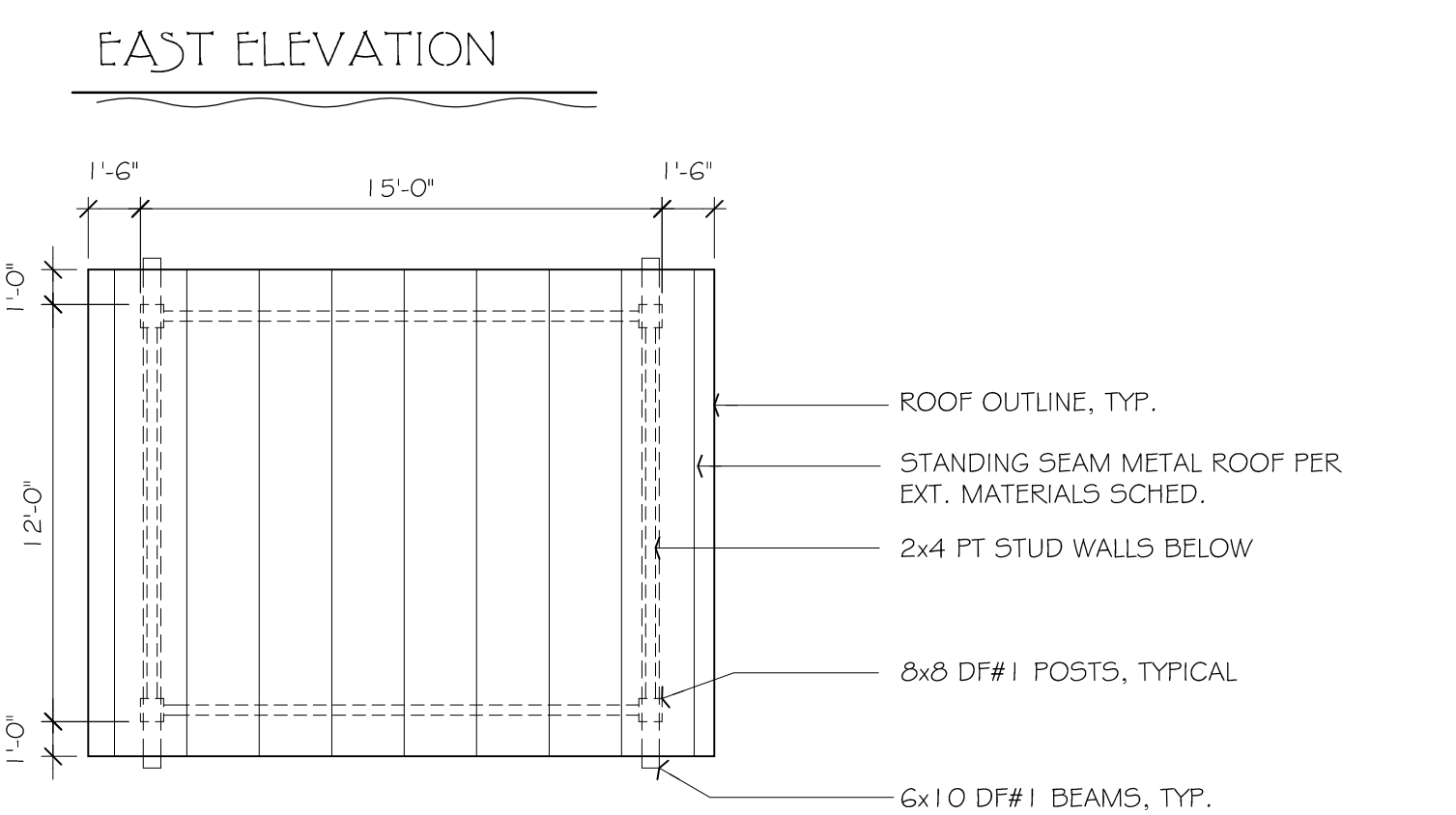
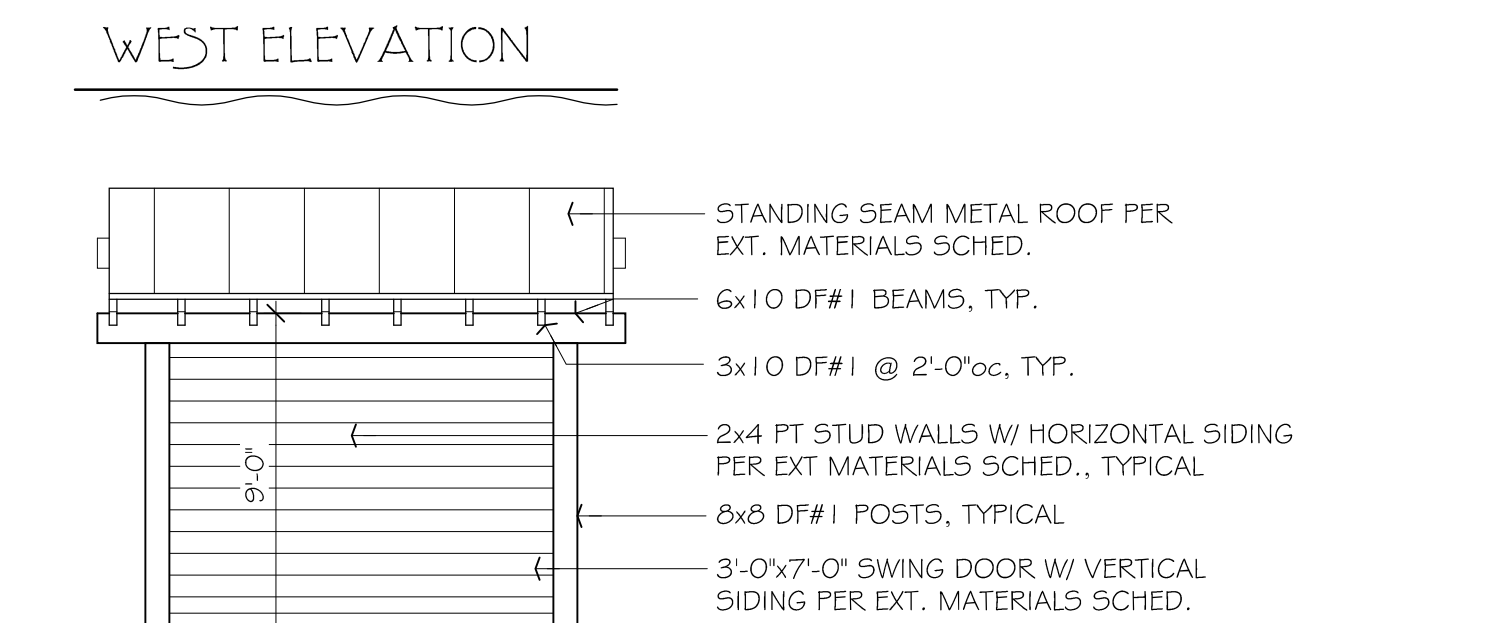
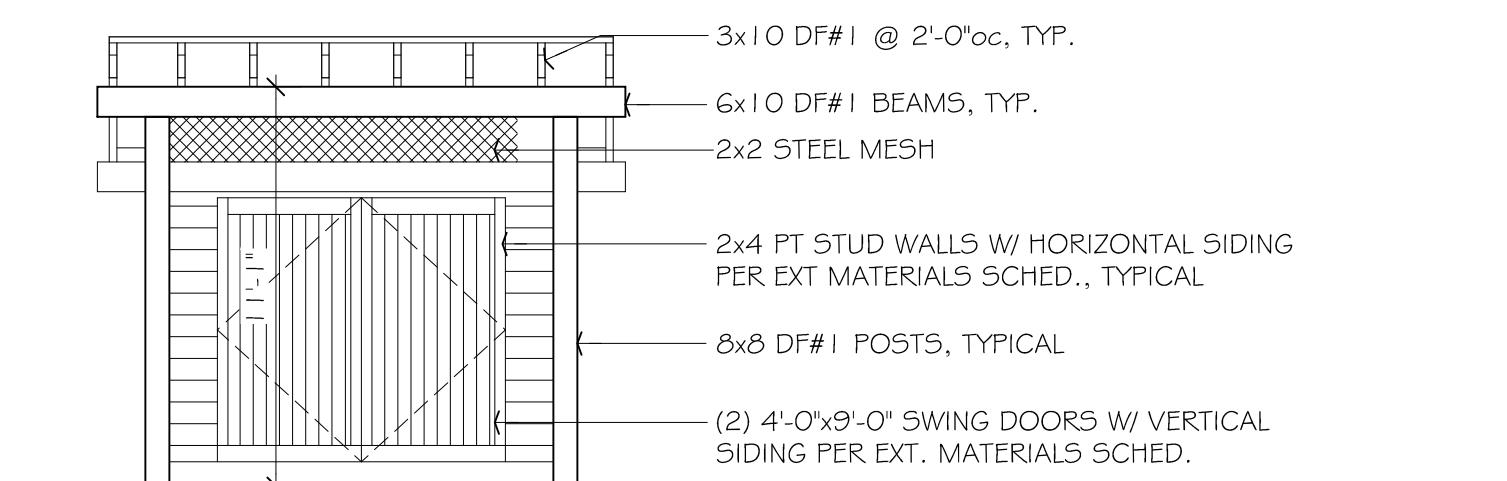
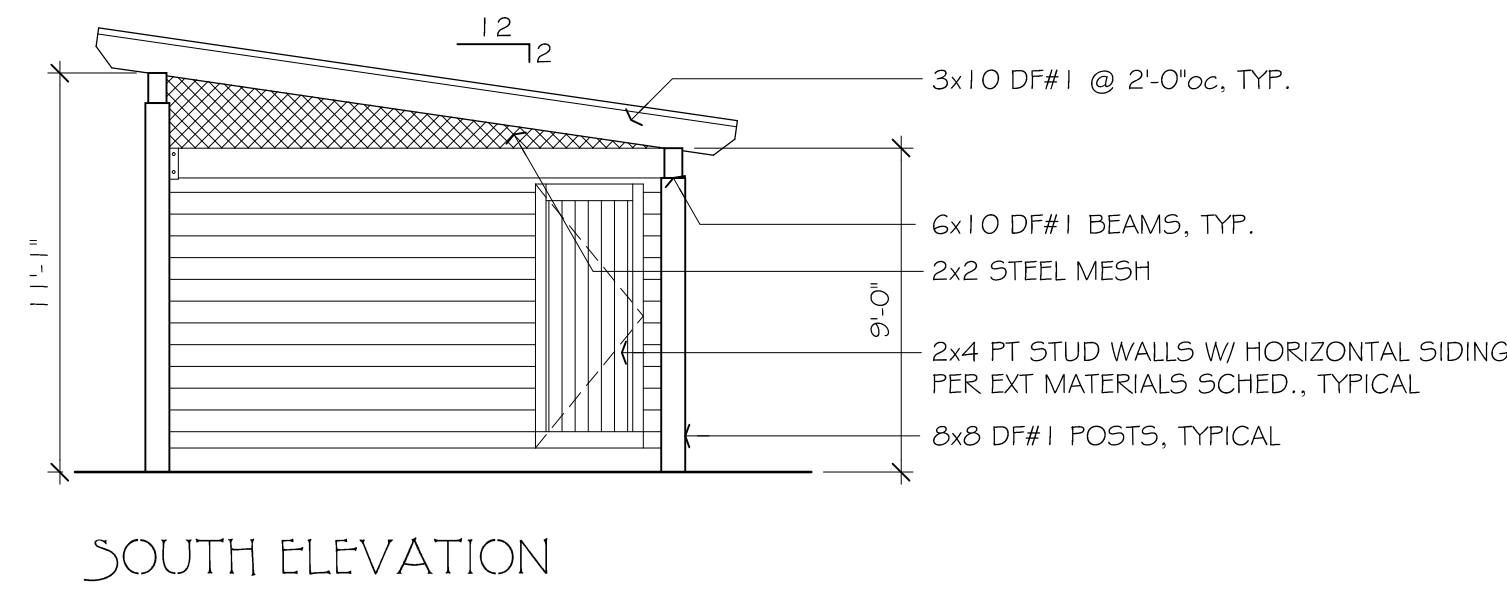
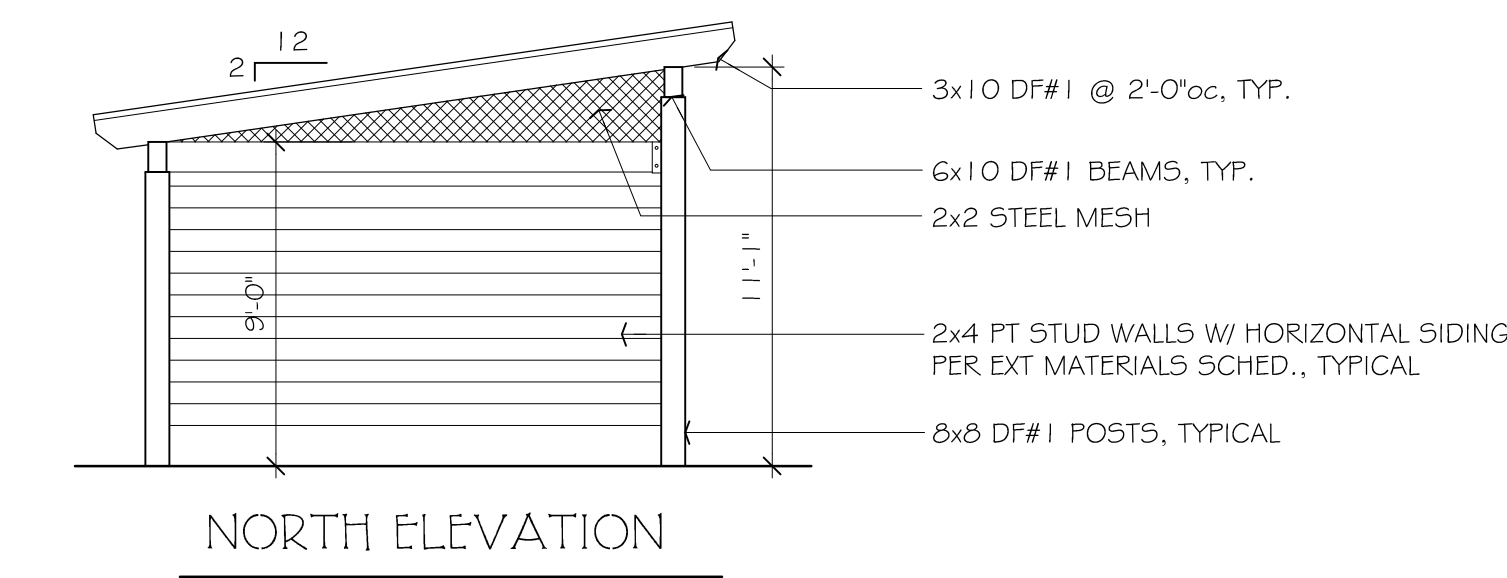
EXISTING SITE & DEMO PLAN

ISSUE:	DATE:
DRC	7 DEC 2022
SKETCH	9 JAN 2023
SKETCH	3 MAY 2023
UPDATE	8 AUG 2023
DRC	26 OCT 2023
SKETCH	3 NOV 2023
PLANNING	13 MAY 2024

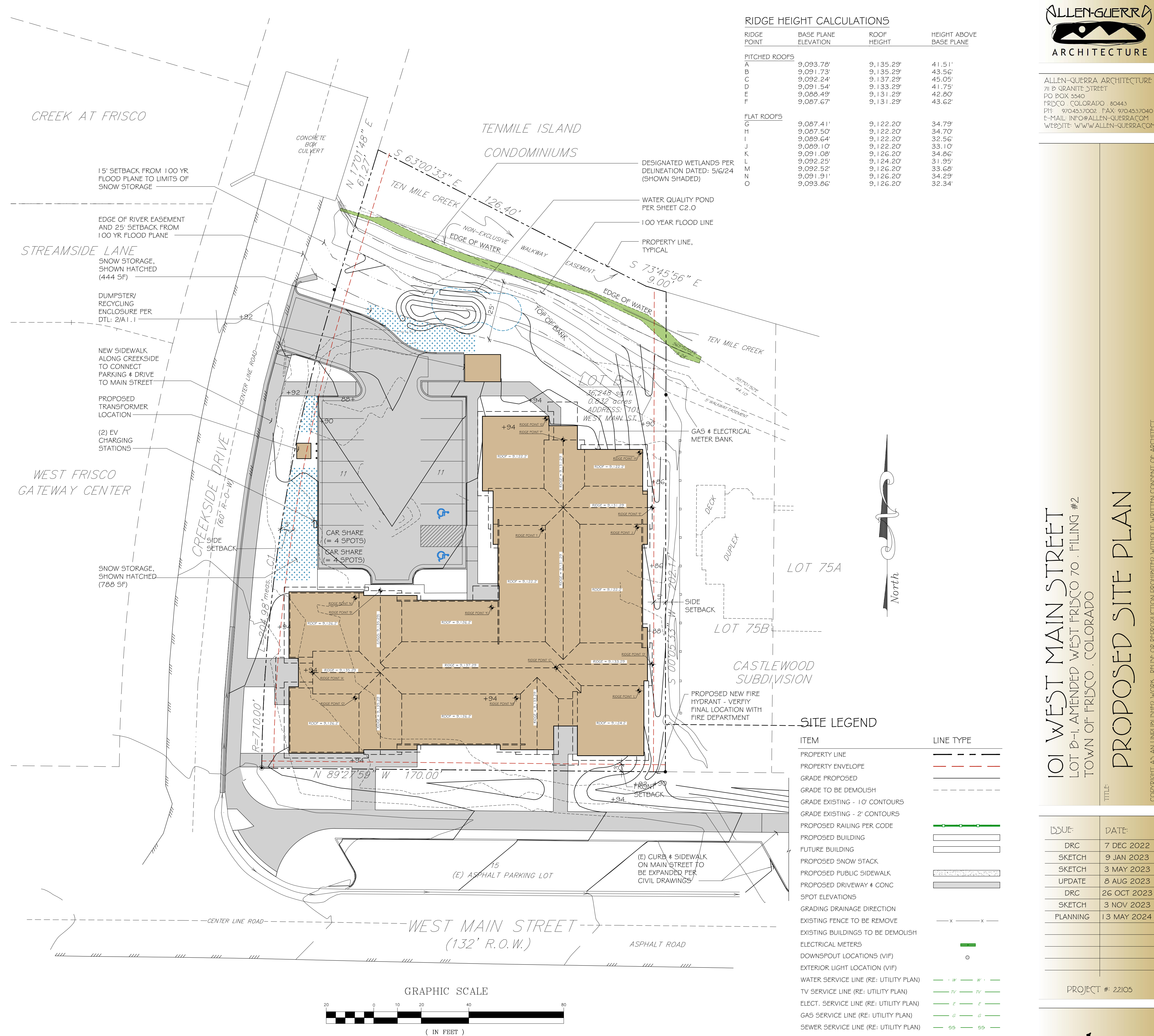
PROJECT #: 22105

RIDGE HEIGHT CALCULATIONS

RIDGE POINT	BASE PLANE ELEVATION	ROOF HEIGHT	HEIGHT ABOVE BASE PLANE
<b>FITCHED ROOFS</b>			
A	9,093.78'	9,135.29'	41.51'
B	9,091.73'	9,135.29'	43.56'
C	9,092.24'	9,137.29'	45.05'
D	9,091.54'	9,133.29'	41.75'
E	9,088.49'	9,131.29'	42.80'
F	9,087.67'	9,131.29'	43.62'
<b>FLAT ROOFS</b>			
G	9,087.41'	9,122.20'	34.79'
H	9,087.50'	9,122.20'	34.70'
I	9,089.64'	9,122.20'	32.56'
J	9,089.10'	9,122.20'	33.10'
K	9,091.08'	9,126.20'	34.86'
L	9,092.25'	9,124.20'	31.95'
M	9,092.52'	9,126.20'	33.68'
N	9,091.91'	9,126.20'	34.29'
O	9,093.86'	9,126.20'	32.34'



2 DUMPSTER ENCLOSURE DETAILS  
SCALE: 3/16" = 1'-0"



**SITE LEGEND**

ITEM	LINE TYPE
PROPERTY LINE	---
PROPERTY ENVELOPE	---
GRADE PROPOSED	---
GRADE TO BE DEMOLISH	---
GRADE EXISTING - 10' CONTOURS	---
GRADE EXISTING - 2' CONTOURS	---
PROPOSED RAILING PER CODE	---
PROPOSED BUILDING	---
FUTURE BUILDING	---
PROPOSED SNOW STACK	---
PROPOSED PUBLIC SIDEWALK	---
PROPOSED DRIVEWAY & CONC	---
SPOT ELEVATIONS	o
GRADING DRAINAGE DIRECTION	---
EXISTING FENCE TO BE REMOVE	---
EXISTING BUILDINGS TO BE DEMOLISH	---
ELECTRICAL METERS	o
DOWNSPOUT LOCATIONS (VIP)	o
EXTERIOR LIGHT LOCATION (VIP)	o
WATER SERVICE LINE (RE: UTILITY PLAN)	---
TV SERVICE LINE (RE: UTILITY PLAN)	---
ELECT. SERVICE LINE (RE: UTILITY PLAN)	---
GAS SERVICE LINE (RE: UTILITY PLAN)	---
SEWER SERVICE LINE (RE: UTILITY PLAN)	---

101 WEST MAIN STREET  
LOT D-1, AMENDED WEST FRISCO 70, FILING #2  
TOWN OF FRISCO, COLORADO

**PROPOSED SITE PLAN**

TITLE

**ISSUE:** **DATE:**

DRC	7 DEC 2022
SKETCH	9 JAN 2023
SKETCH	3 MAY 2023
UPDATE	8 AUG 2023
DRC	26 OCT 2023
SKETCH	3 NOV 2023
PLANNING	13 MAY 2024

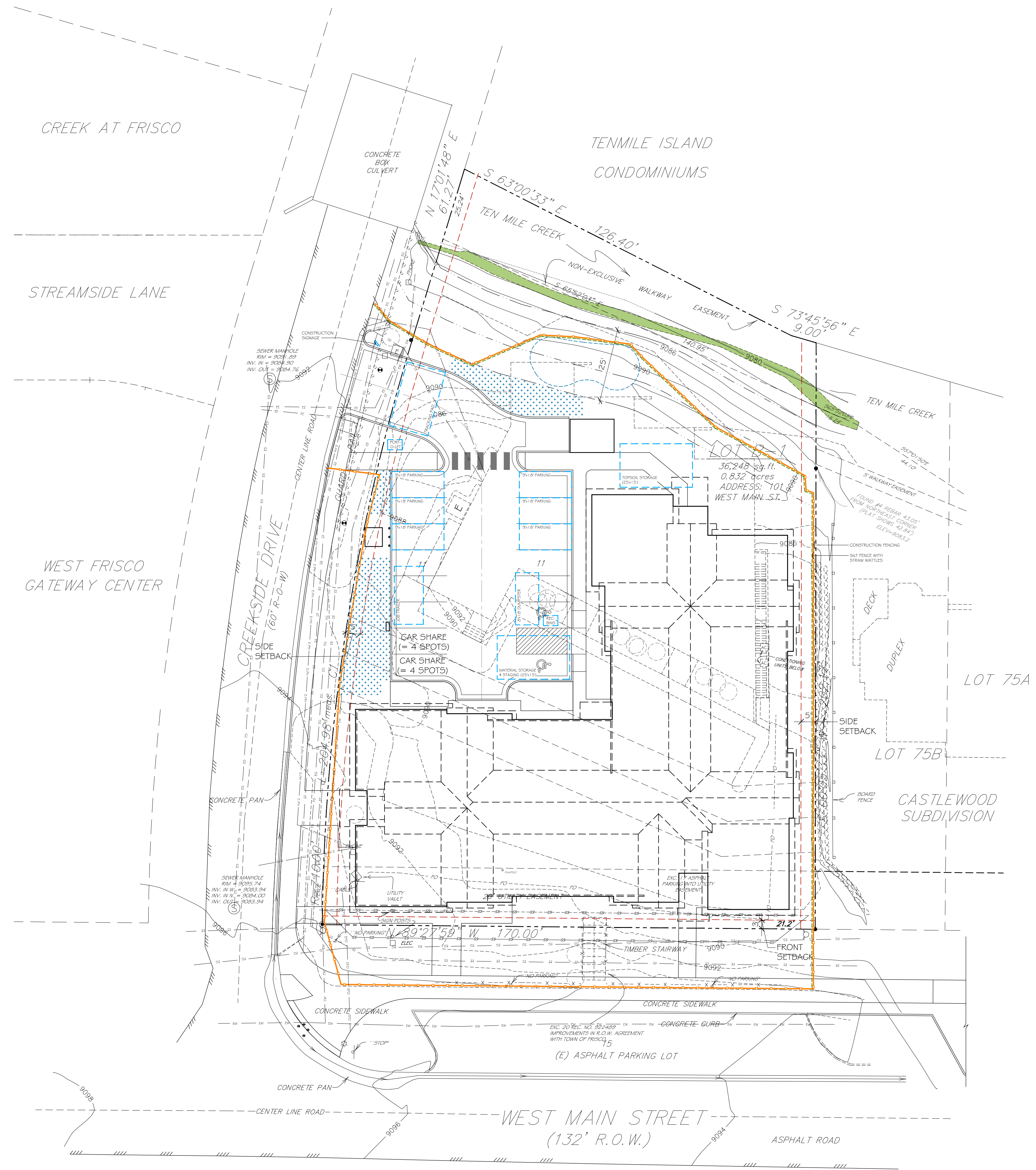
PROJECT #: 22105

A11

1 PROPOSED SITE PLAN  
SCALE: 1" = 20'-0"

CONST. MANAGEMENT LEGEND

ITEM	LINE TYPE
PROPERTY LINE	---
PROPERTY ENVELOPE	---
CONSTRUCTION FENCE	---
SILT FENCE (AS REQUIRED)	---
CONSTRUCTION PHASE ELEMENTS	---
PROPOSED BUILDING	---
FUTURE BUILDING	---
WATER SERVICE LINE (RE: UTILITY PLAN)	---
TV SERVICE LINE (RE: UTILITY PLAN)	---
ELECT. SERVICE LINE (RE: UTILITY PLAN)	---
GAS SERVICE LINE (RE: UTILITY PLAN)	---
SEWER SERVICE LINE (RE: UTILITY PLAN)	---
EXISTING FENCE TO BE REMOVED	---
ELECTRICAL METERS (RE: UTILITY PLAN)	---



101 WEST MAIN STREET  
LOT D-1, AMENDED WEST FRISCO 70, FILING #2  
TOWN OF FRISCO, COLORADO

CONSTRUCTION MANAGEMENT PLAN

ISSUE:	DATE:
DRC	7 DEC 2022
SKETCH	9 JAN 2023
SKETCH	3 MAY 2023
UPDATE	8 AUG 2023
DRC	26 OCT 2023
SKETCH	3 NOV 2023
PLANNING	13 MAY 2024

PROJECT #: 22105





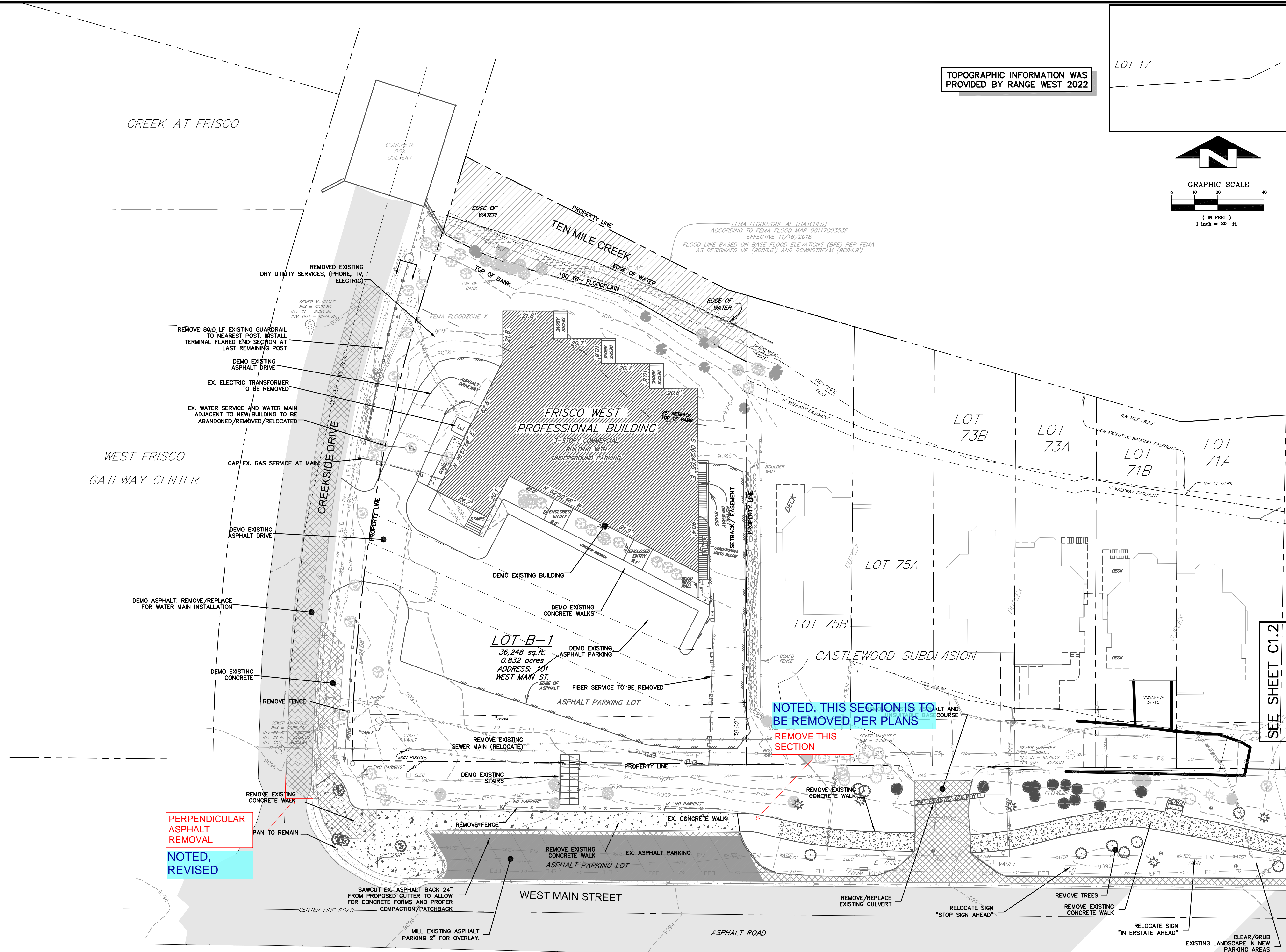
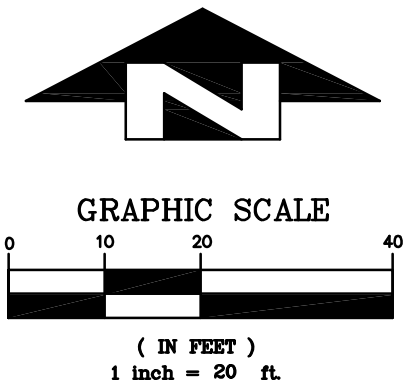


**101 WEST MAIN STREET**  
 FRISCO, CO  
 EXISTING CONDITIONS/DEMOLITION

NO.	DATE	REVISIONS	BY
1	05/12/2023	PRELIMINARY PLAN	MCW
2	12/12/2023	FINAL SITE PLAN	MCW
3	01/25/2024	RESPONSE TO COMMENTS	MCW
4	05/14/2024	FINAL SITE SUBMITTAL	MCW

DESIGNED	MCW	DATE	05/11/2023
DRAWN	MCW		
CHECKED	MCW_TSL		
JOB NO.			

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022



PERPENDICULAR ASPHALT REMOVAL  
 NOTED, REVISED

NOTED, THIS SECTION IS TO BE REMOVED PER PLANS

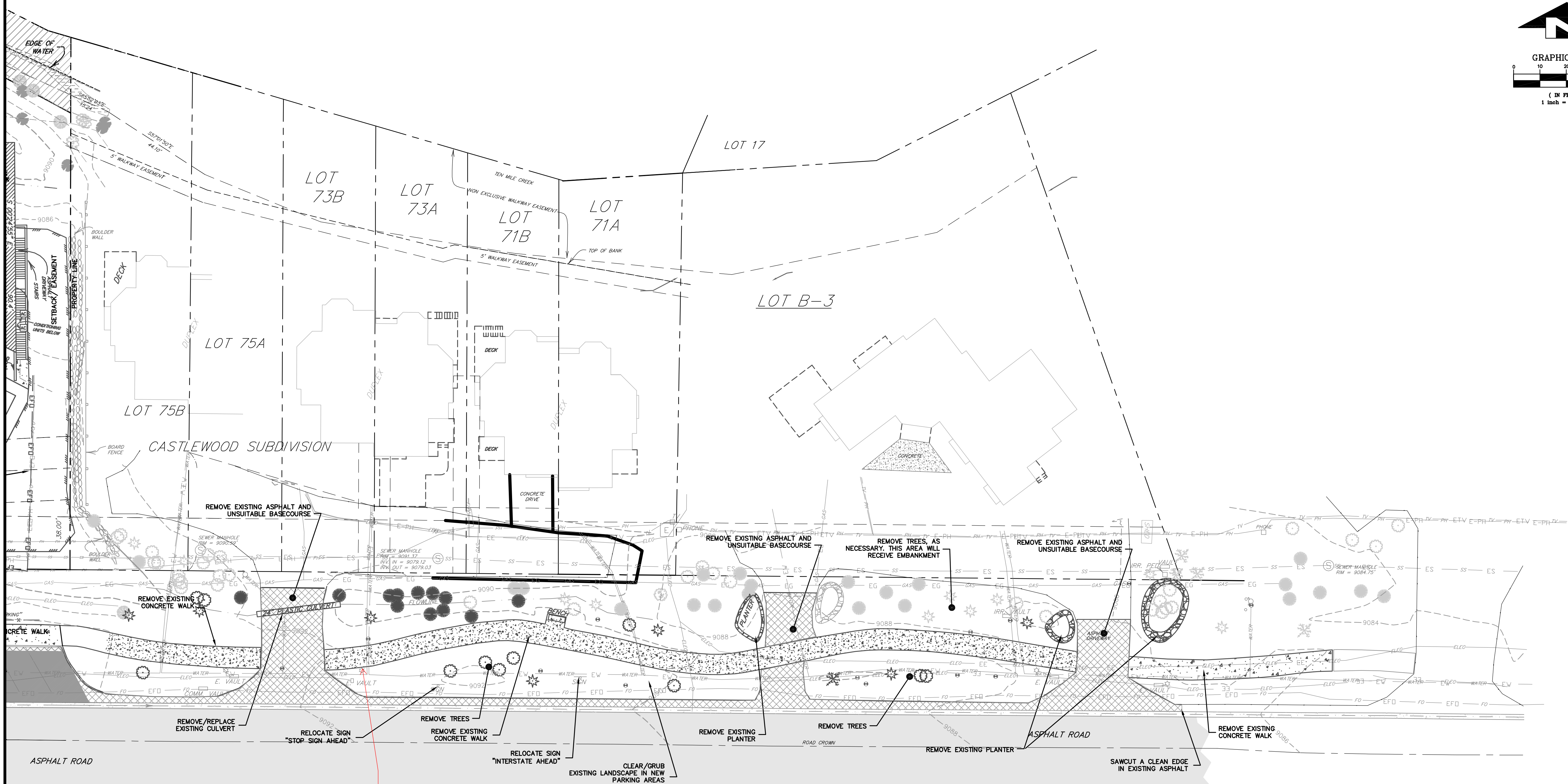
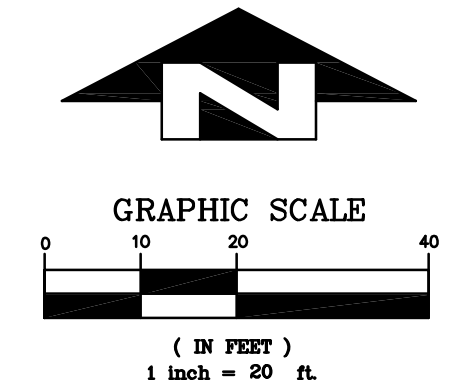
REMOVE THIS SECTION

SEE SHEET C1.2

FEMA FLOODZONE AE (HATCHED)  
 ACCORDING TO FEMA FLOOD MAP 08177C0353F  
 EFFECTIVE 11/16/2018  
 FLOOD LINE BASED ON BASE FLOOD ELEVATIONS (BFE) PER FEMA  
 AS DESIGNATED UP (9088.6') AND DOWNSTREAM (9084.9')

TOPOGRAPHIC INFORMATION WAS  
 PROVIDED BY RANGE WEST 2022

LOT 17



SEE SHEET C1.1

REMOVE EXISTING BOLLARD LIGHTS  
 NOTED, NOTE TO BE ADDED TO THE PLANS

**101 WEST MAIN STREET**  
**FRISCO, CO**  
 EXISTING CONDITIONS/DEMOLITION

DESIGNED	NO.	DATE	REVISIONS	BY
MCW		05/12/2023	PRELIMINARY PLAN	MCW
MCW		12/12/2023	FINAL SITE PLAN	MCW
MCW_TSL		01/25/2024	RESPONSE TO COMMENTS	
		05/14/2024	FINAL SITE SUBMITTAL	

SHEET  
 C1.2

NO.	DATE	REVISIONS
01	05/12/2023	PRELIMINARY PLAN
02	12/12/2023	FINAL SITE PLAN
03	01/25/2024	RESPONSE TO COMMENTS
04	05/14/2024	FINAL SITE SUBMITTAL

DESIGNED	BY	DATE
MCW	MCW	05/11/2023
DRAWN	BY	DATE
MCW	MCW	
CHECKED	BY	DATE
MCW, TSL	MCW	
JOB NO.	DATE	

**LEGEND**

- PROPERTY LINE
- EXISTING CONTOUR
- EASEMENT
- PROPOSED CONTOUR
- PROPOSED GRADING, SLOPE/SPOT
- EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT

**GRAPHIC SCALE**  
 ( IN FEET )  
 1 inch = 20 ft

GRADE TO DRAIN AWAY FROM BUILDING IN ALL AREAS

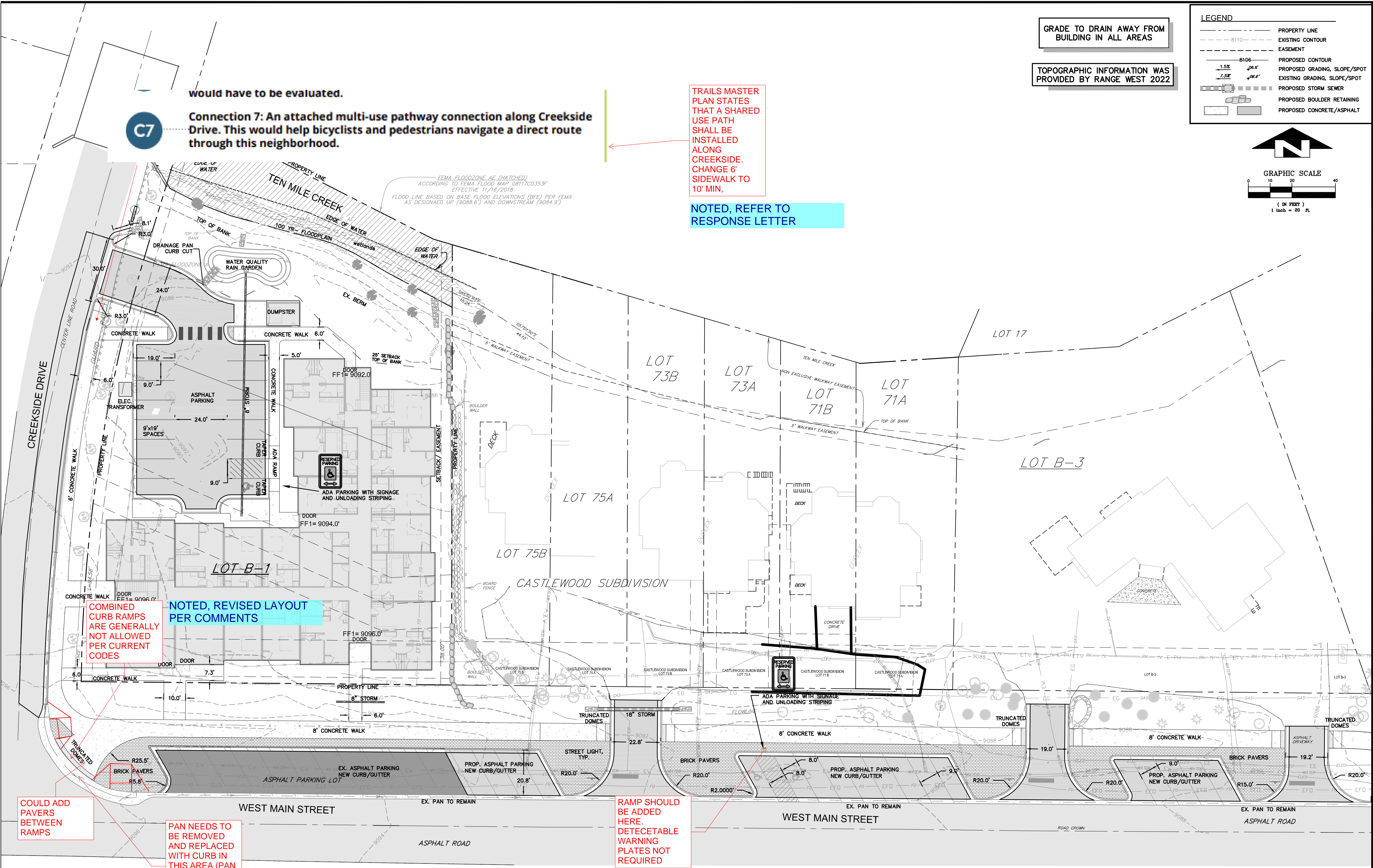
TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

TRAILS MASTER PLAN STATES THAT A SHARED USE PATH SHALL BE INSTALLED ALONG CREEKSIDE. CHANGE 6' SIDEWALK TO 10' MIN.

NOTED, REFER TO RESPONSE LETTER

would have to be evaluated.  
**Connection 7: An attached multi-use pathway connection along Creekside Drive. This would help bicyclists and pedestrians navigate a direct route through this neighborhood.**

**C7**



COMBINED CURB RAMPS ARE GENERALLY NOT ALLOWED PER CURRENT CODES

NOTED, REVISED LAYOUT PER COMMENTS

COULD ADD PAVERS BETWEEN RAMPS

PAN NEEDS TO BE REMOVED AND REPLACED WITH CURB IN THIS AREA (PAN ONLY ALLOWED IN DETECTABLE WARNING PLATE AREAS)

RAMP SHOULD BE ADDED HERE. DETECTABLE WARNING PLATES NOT REQUIRED

NOTED, REVISED LAYOUT PER COMMENTS. LOCATION HAS CHANGED PER PLANNING COMMENTS.

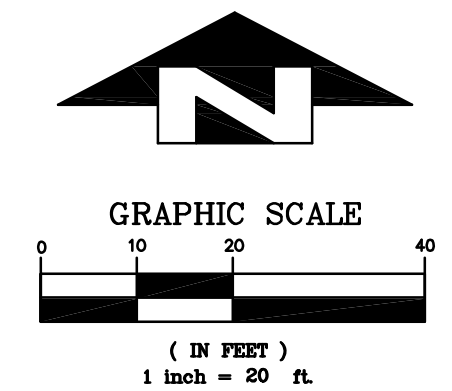
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GRADE TO DRAIN AWAY FROM BUILDING IN ALL AREAS

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

**LEGEND**

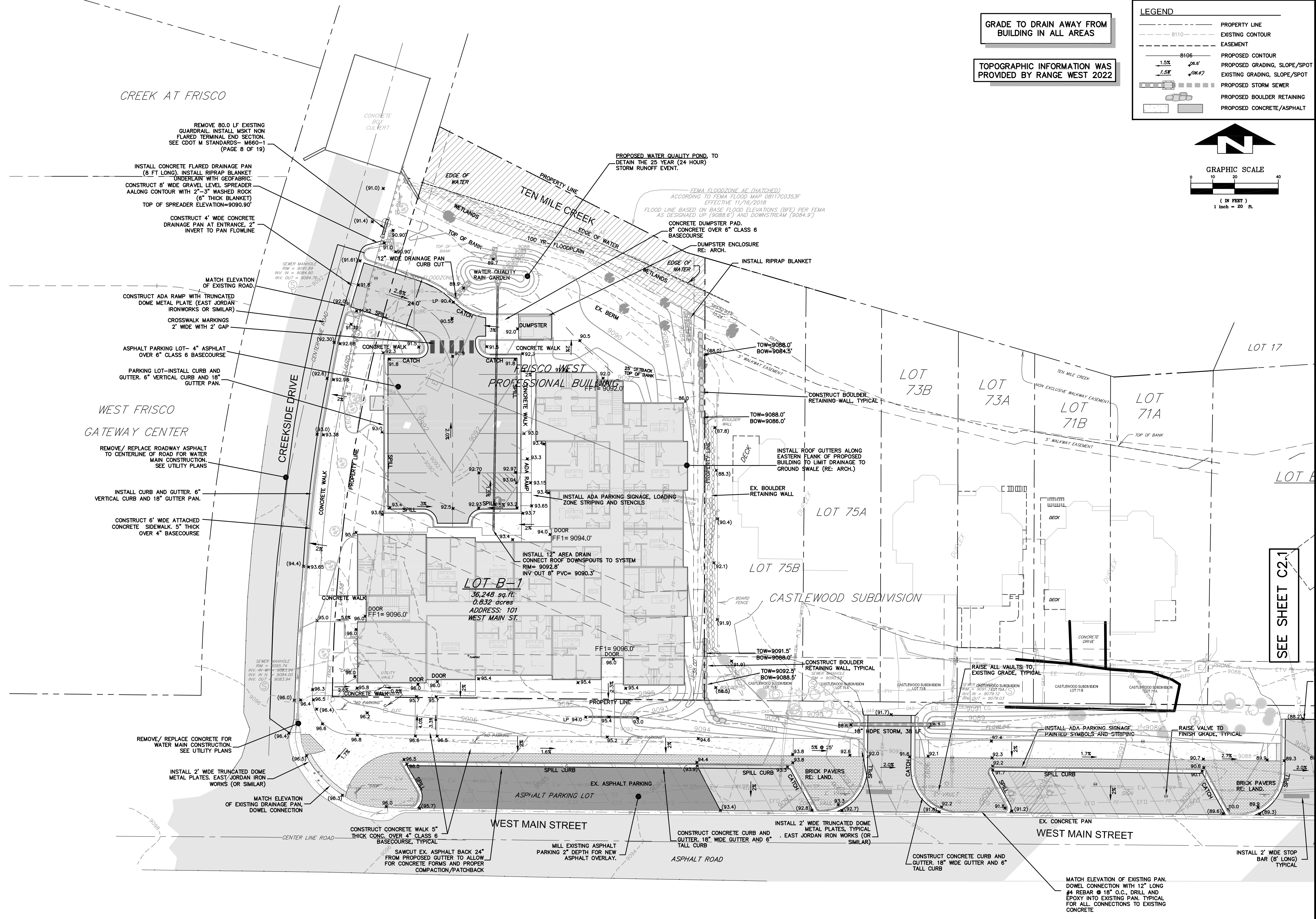
- 8110 PROPERTY LINE
- 8110 EXISTING CONTOUR
- 8106 EASEMENT
- 1.5% PROPOSED CONTOUR
- 1.5% PROPOSED GRADING, SLOPE/SPOT
- 1.5% EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT



**ALPINE ENGINEERING INC.**  
 24610 HWY G UNIT A9 PRO BOX 97  
 FORT COLLINS COLO 80504  
 WWW.ALPINECVIL.COM



**101 WEST MAIN STREET**  
 FRISCO, CO  
 GRADING AND DRAINAGE



SEE SHEET C2.1

NO.	DATE	REVISIONS
05/12/2023	PRELIMINARY PLAN	BY: MCW
12/12/2023	FINAL SITE PLAN	MCW
01/25/2024	RESPONSE TO COMMENTS	MCW, TSL
05/14/2024	FINAL SITE SUBMITTAL	MCW, TSL

DESIGNED	DRAWN	CHECKED	JOB NO.	DATE
MCW	MCW	TSL		05/11/2023

C:\file001\West Main-2023.dwg\Master\Grading-101 West Main.dwg, 5/3/2024, 10:01:15 AM, wendy

AS (HATCHED)  
 20 MAP 081170353F  
 7/6/2018  
 1 ELEVATIONS (BFE) PER FEMA  
 NO DOWNSCREEN (9084.5)

ASS 6  
 OSURE  
 INSTALL RIPRAP BLANKET

GRADE TO DRAIN AWAY FROM  
 BUILDING IN ALL AREAS

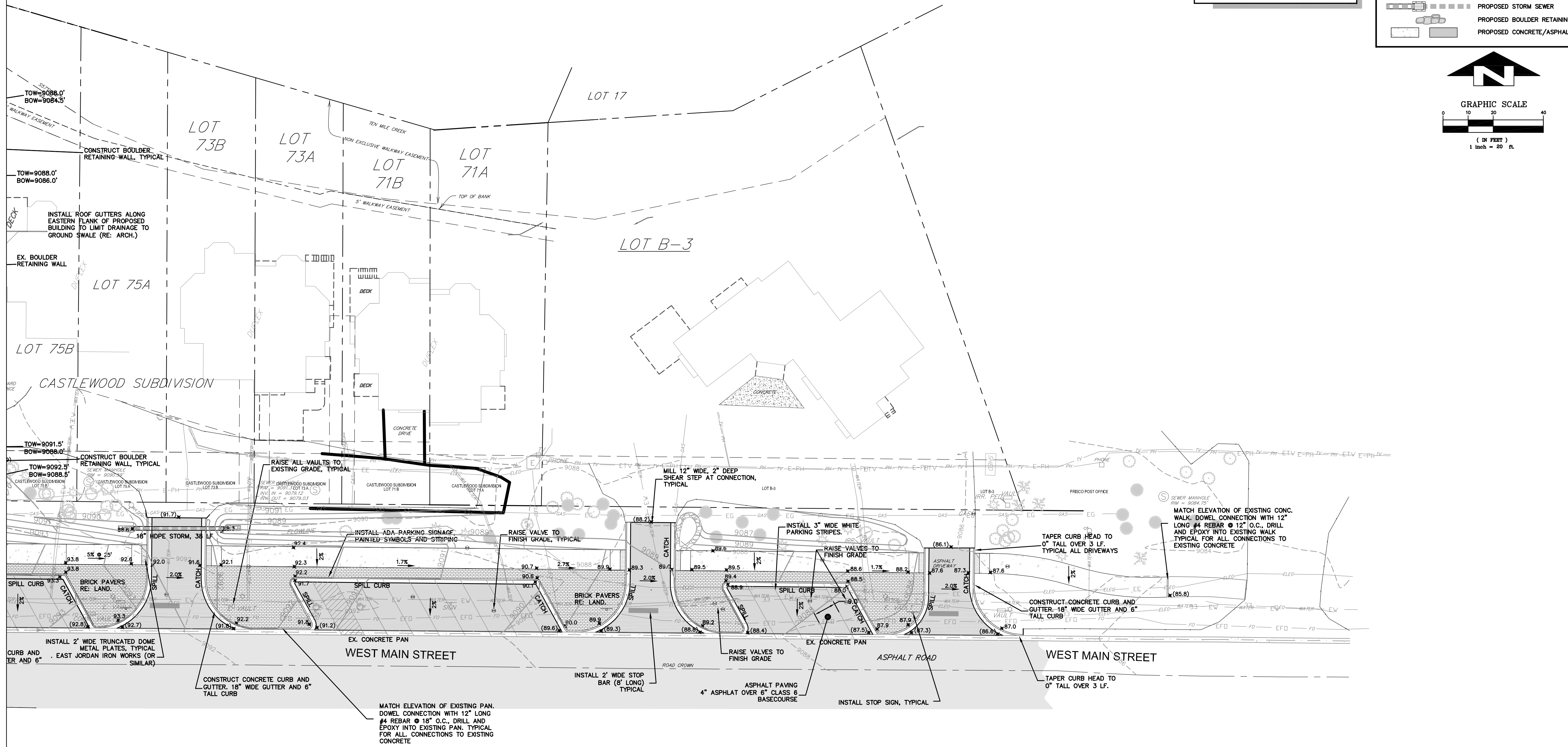
TOPOGRAPHIC INFORMATION WAS  
 PROVIDED BY RANGE WEST 2022

**LEGEND**

- PROPERTY LINE
- - - EXISTING CONTOUR
- - - EASEMENT
- - - PROPOSED CONTOUR
- 1.5% 48.6' PROPOSED GRADING, SLOPE/SPOT
- 1.5% 48.6' EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT

**GRAPHIC SCALE**

( IN FEET )  
 1 inch = 20 ft.



SEE SHEET C2.0

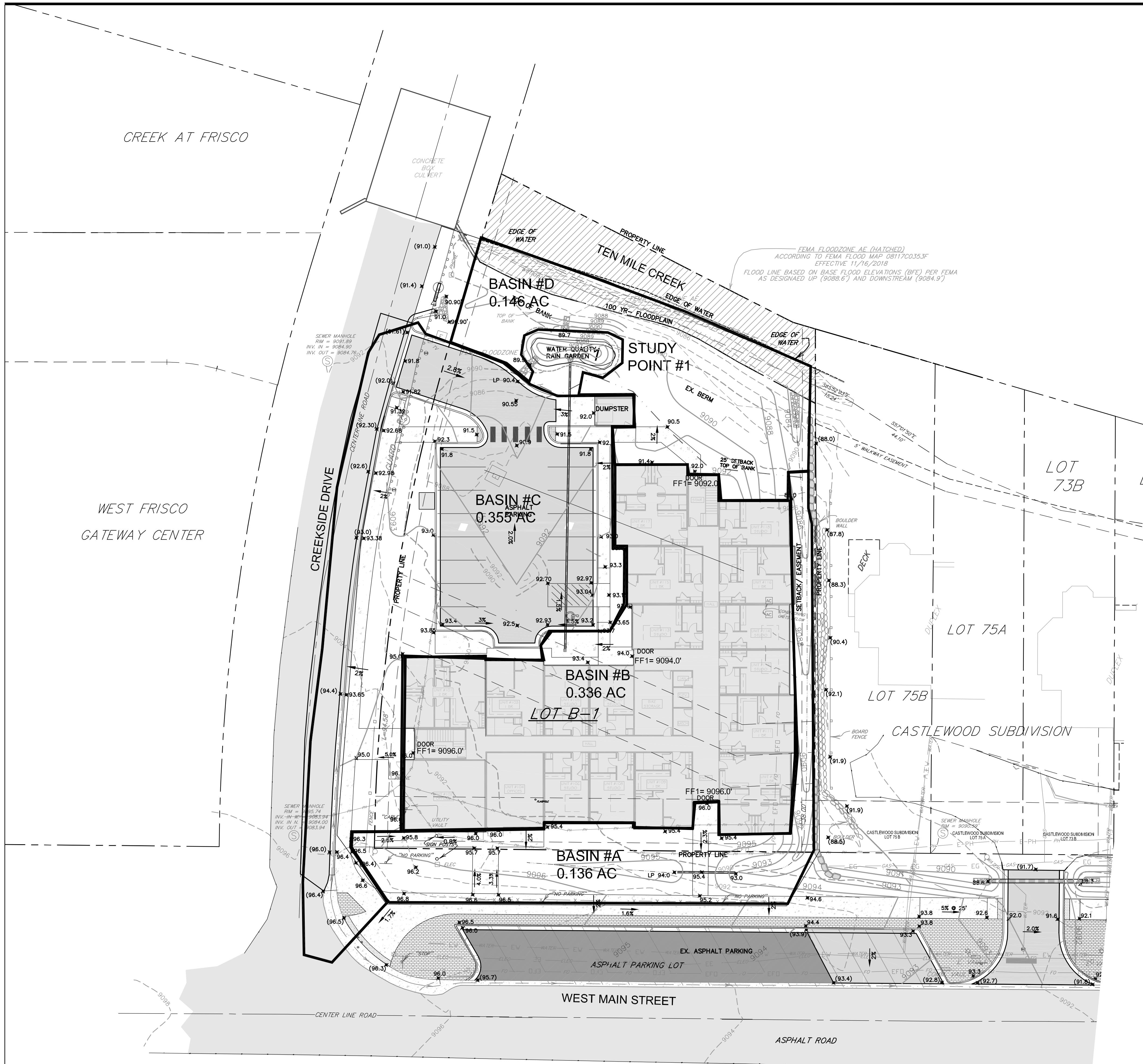
**101 WEST MAIN STREET**  
 FRISCO, CO  
 GRADING AND DRAINAGE

DESIGNED	DRAWN	CHECKED	JOB NO.	DATE
MCW	MCW	MCW, TSL		05/11/2023

NO.	DATE	REVISIONS
	05/12/2023	PRELIMINARY PLAN
	12/12/2023	FINAL SITE PLAN
	01/25/2024	RESPONSE TO COMMENTS
	05/14/2024	FINAL SITE SUBMITTAL

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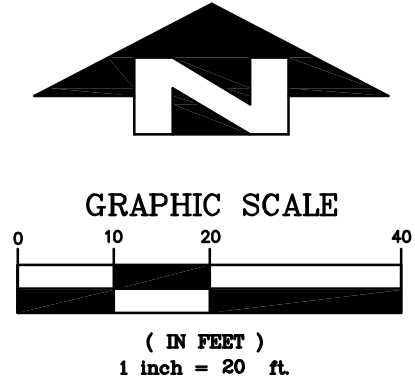


TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

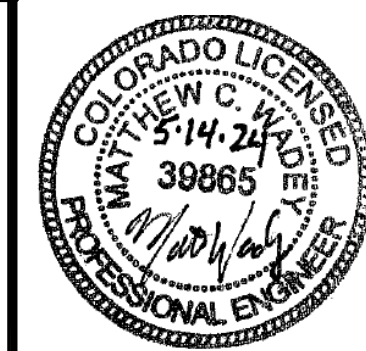
CONTRACTOR SHALL DEVELOP A MEANS AND METHODS TO CONTROL SEDIMENT DURING CONSTRUCTION TO ASSURE THAT NO SEDIMENT IS DISCHARGED OFF-SITE.

**LEGEND**

- PROPERTY LINE
- - - 8110 EXISTING CONTOUR
- - - EASEMENT
- - - 8106 PROPOSED CONTOUR
- - - 1.5% 08.6' PROPOSED GRADING, SLOPE/SPOT
- - - 1.5% 08.6' EXISTING GRADING, SLOPE/SPOT
- - - PROPOSED STORM SEWER
- - - PROPOSED BOULDER RETAINING
- - - PROPOSED CONCRETE/ASPHALT



**ALPINE ENGINEERING INC.**  
 34510 HWY 6 UNIT A9 P.O. BOX 97  
 FRISCO, TEXAS 79425  
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**101 WEST MAIN STREET  
 FRISCO, CO  
 DRAINAGE AREA MAP**

**HISTORIC vs. DEVELOPED STORM RUNOFF**

STUDY POINT	HISTORIC		DEVELOPED	
	25 YEAR (cfs)	100 YEAR (cfs)	25 YEAR (cfs)	100 YEAR (cfs)
1	1.45	2.65	2.91	4.13

Total Area = 0.945 AC (historic)  
 = 0.973 AC (developed)  
 Impervious (Roofs/Paving) = 0.642 AC  
 Grass open space (good) = 0.331 AC

**101 West Main Street  
 Frisco, CO  
 Stormwater Runoff**

Basins	Area (Acre)	Area (sqft)	Area Impervious	Area Grass	RCN	TC (hours)	Q10 (cfs)	Q25 (cfs)	Q100 (cfs)
A	0.136	5,924	0.026	0.11	68	0.10	0.00	0.00	0.11
B	0.336	14,636	0.328	0.008	97	0.10	0.73	0.91	1.24
C	0.355	15,464	0.282	0.073	91	0.10	0.54	0.72	1.08
D	0.146	6,360	0.006	0.14	63	0.10	0.00	0.00	0.07
<b>Developed</b>	<b>0.97</b>						<b>1.27</b>	<b>1.63</b>	<b>2.50</b>

**HISTORIC**

H	0.945	41,164	0	0.945	69	0.185	0.10	0.28	0.74
---	-------	--------	---	-------	----	-------	------	------	------

**TABLE 1  
 101 West Mains St, Frisco  
 DETENTION POND - DETENTION STORAGE VOLUME**

**POND**

Elevation	Area (ft <sup>2</sup> )	Avg. Area (ft <sup>2</sup> )	Depth (ft)	Volume (ft <sup>3</sup> )	Cumulative Volume (ft <sup>3</sup> )	Cumulative Volume (Ac-ft)	Cumulative Volume (Gallons)
9087	56	56	0	0	0	0.000	0
9088	199	128	1	128	128	0.003	954
9089	353	276	1	276	404	0.009	3,018
9089.7	518	438	0.7	305	708	0.016	5,298

TOTAL PROVIDED 708 cuf  
 TOTAL REQUIRED 488 cuf

NO.	DATE	REVISIONS
	05/12/2023	PRELIMINARY PLAN
	12/12/2023	FINAL SITE PLAN
	01/25/2024	RESPONSE TO COMMENTS
	05/11/2024	FINAL SITE SUBMITTAL

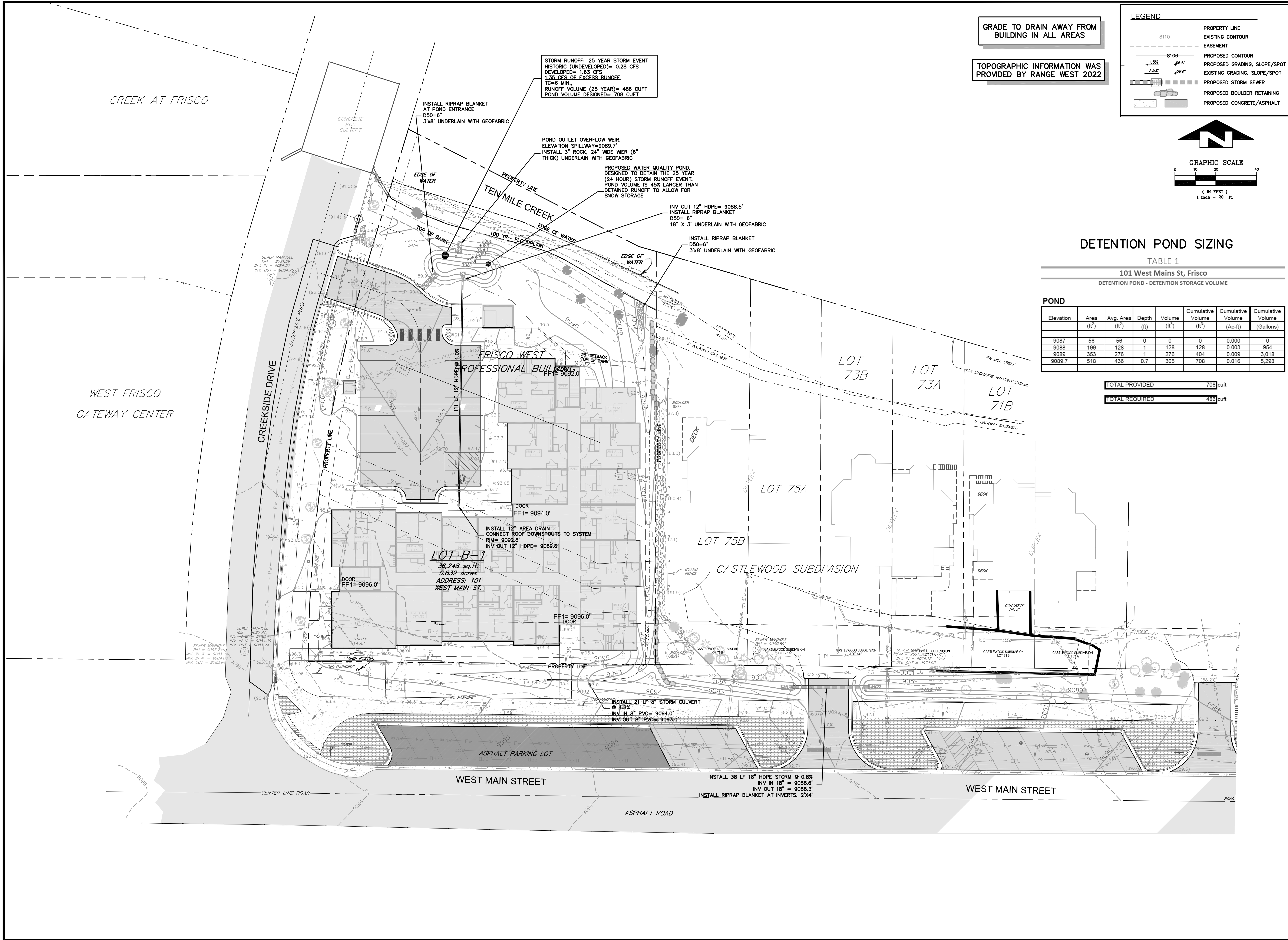
  

DESIGNED	DRAWN	CHECKED	JOB NO.	DATE
MCW	MCW	MCW, TSL		05/11/2023

**SHEET  
 C3.0**

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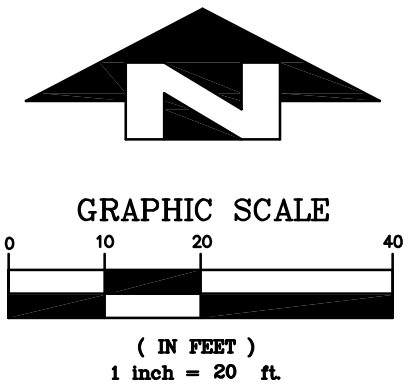


GRADE TO DRAIN AWAY FROM BUILDING IN ALL AREAS

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

**LEGEND**

	PROPERTY LINE
	EXISTING CONTOUR
	EASEMENT
	PROPOSED CONTOUR
	PROPOSED GRADING, SLOPE/SPOT
	EXISTING GRADING, SLOPE/SPOT
	PROPOSED STORM SEWER
	PROPOSED BOULDER RETAINING
	PROPOSED CONCRETE/ASPHALT



**DETENTION POND SIZING**

TABLE 1  
101 West Mains St, Frisco  
DETENTION POND - DETENTION STORAGE VOLUME

POND						
Elevation	Area (ft <sup>2</sup> )	Avg. Area (ft <sup>2</sup> )	Depth (ft)	Volume (ft <sup>3</sup> )	Cumulative Volume (ft <sup>3</sup> )	Cumulative Volume (Ac-ft)
9087	56	56	0	0	0	0.000
9088	199	128	1	128	128	0.003
9089	353	276	1	276	404	0.009
9089.7	518	436	0.7	305	708	0.016

TOTAL PROVIDED 708 cuft  
TOTAL REQUIRED 488 cuft



**101 WEST MAIN STREET**  
FRISCO, CO  
STORM SEWER PLAN

NO.	DATE	REVISIONS	BY
	05/12/2023	PRELIMINARY PLAN	MCW
	12/12/2023	FINAL SITE PLAN	MCW
	01/25/2024	RESPONSE TO COMMENTS	
	05/14/2024	FINAL SITE SUBMITTAL	

DESIGNED MCW  
DRAWN MCW  
CHECKED MCW\_TSL  
JOB NO. --  
DATE 05/11/2023

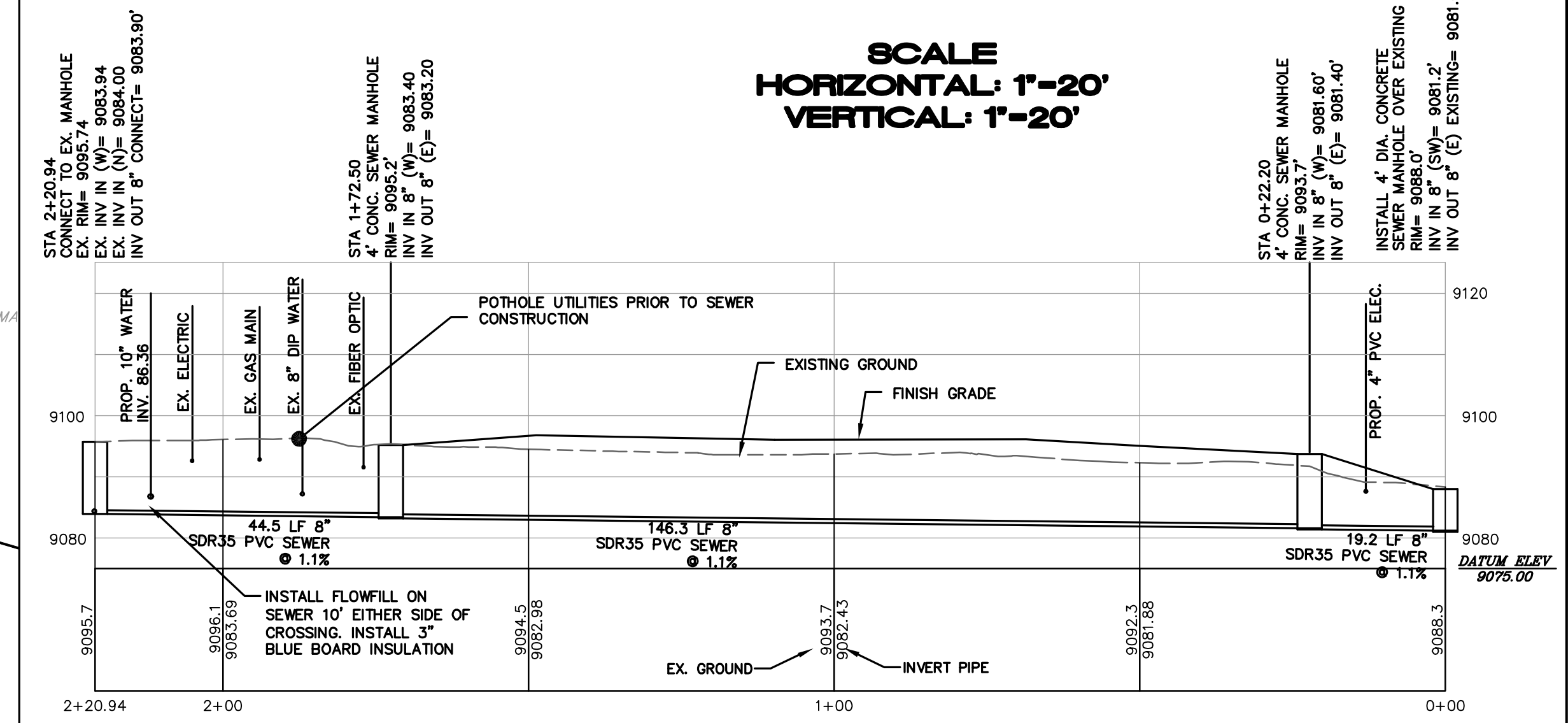
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CREEK AT FRISCO

### SEWER PROFILE

SCALE  
HORIZONTAL: 1"=20'  
VERTICAL: 1"=20'



## 101 WEST MAIN STREET FRISCO, CO UTILITY PLAN

- TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022
- REFER TO TOWN OF FRISCO WATER CONSTRUCTION STANDARDS
- ALL WATER MAIN FITTINGS SHALL HAVE MEGALUG RESTRAINED JOINTS AND CONCRETE THRUST BLOCKS
- MAINTAIN 8.5' BURY ON ALL WATER MAINS AND SERVICES
- REFER TO FRISCO SANITATION DISTRICT SEWER CONSTRUCTION STANDARDS
- SEWER MAIN/SERVICES. MAINTAIN 7.0 FT COVER IN UNPLOWED AREAS AND 9.0 FT COVER IN SNOW PLOWED AREAS. INSTALL INSULATION IF THESE DEPTHS ARE NOT ACHIEVED.

**NOTED, REVISED LAYOUT PER COMMENTS**

**WATER NEEDS TO BE 10' FROM SEWER**

**NOTED, REFER TO RESPONSE LETTER**

**NOTED, REFER TO RESPONSE LETTER**

**NOTED, REFER TO RESPONSE LETTER**

**MANHOLE TO BE LOCATED OUTSIDE SIDEWALK**

**NOTED, REFER TO RESPONSE LETTER**

**NOTED, REFER TO RESPONSE LETTER**

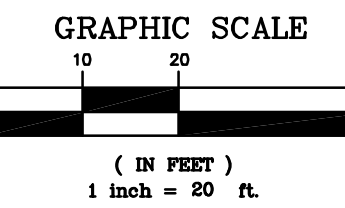
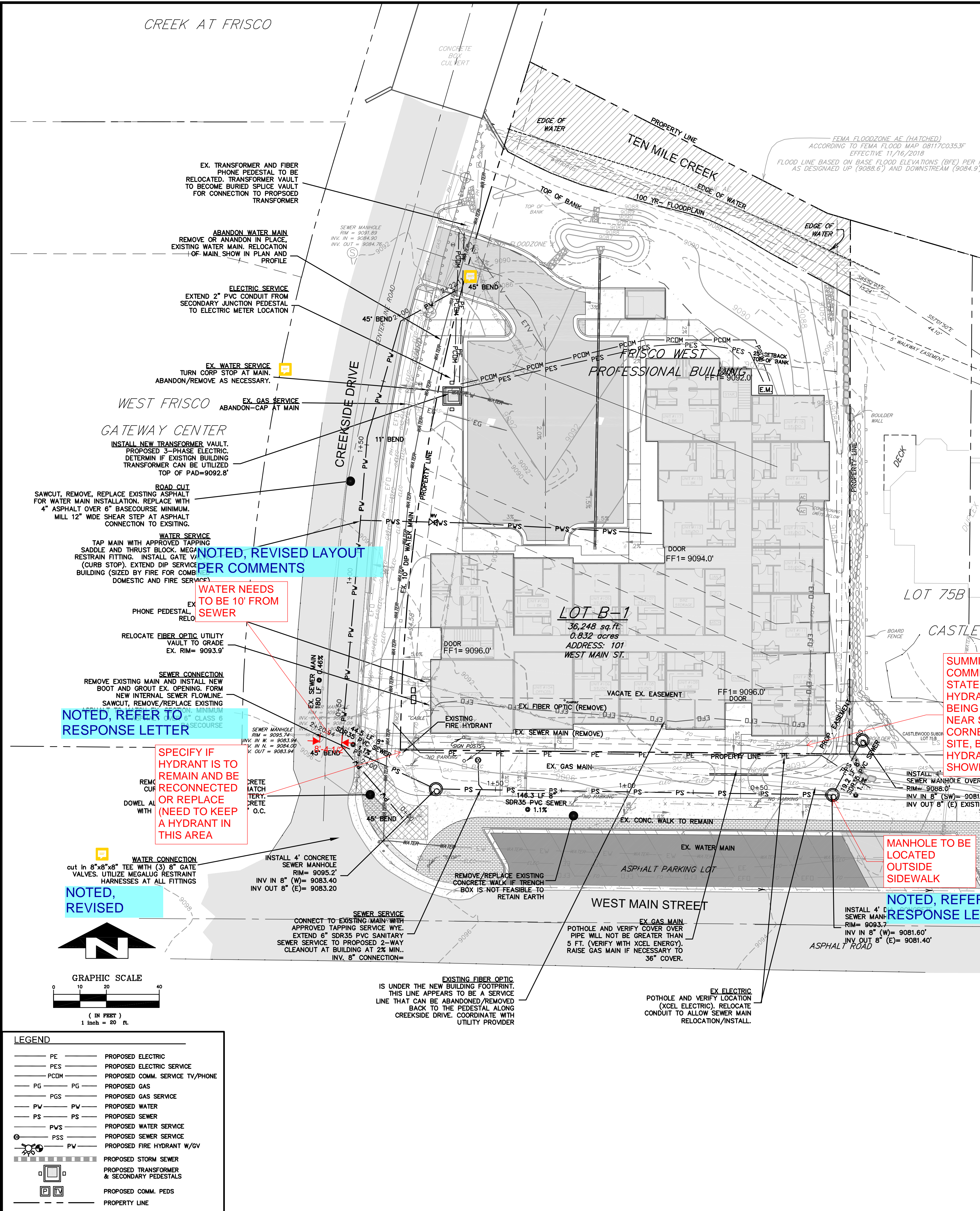
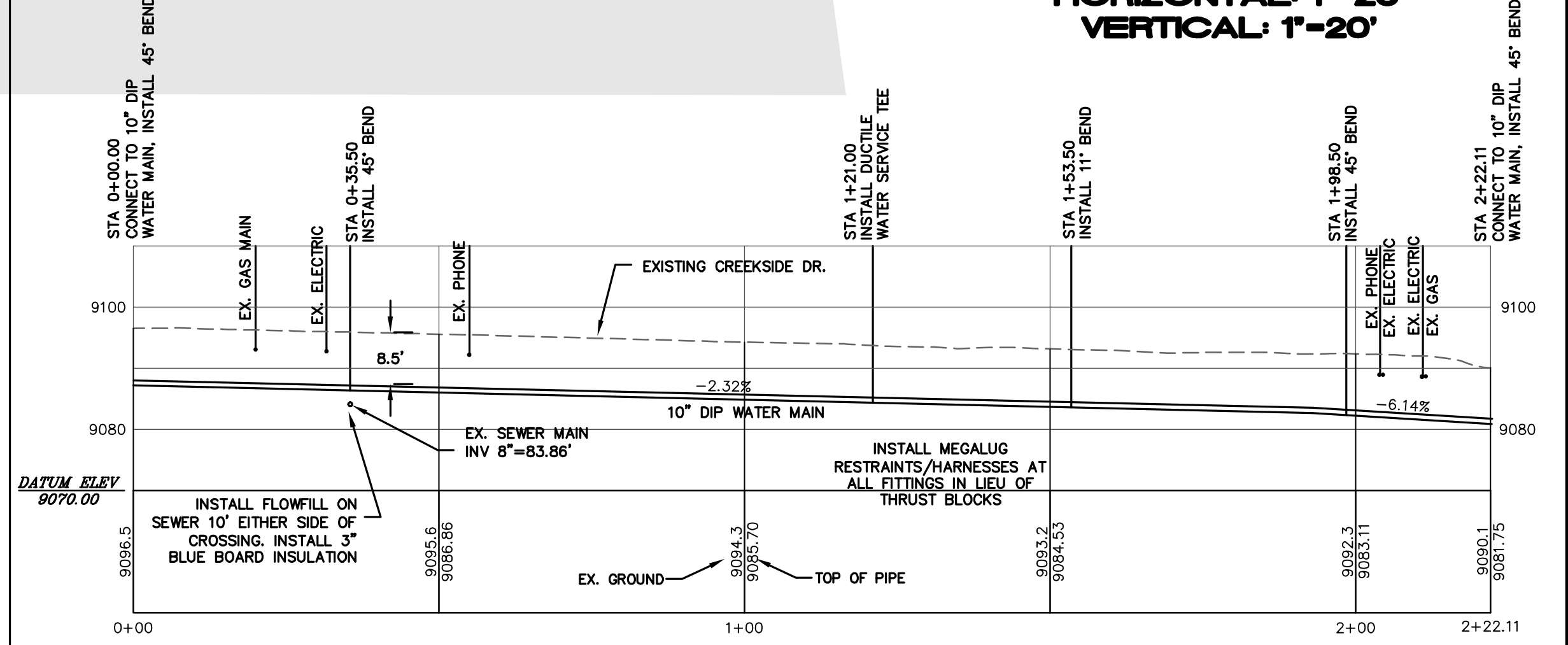
**NOTED, REFER TO RESPONSE LETTER**

**NOTED, REVISED**

SUMMIT FIRE RC COMMENTS STATE THAT A HYDRANT IS BEING ADDED NEAR SE CORNER OF SITE, BUT NO HYDRANT IS SHOWN

### WATER PROFILE

SCALE  
HORIZONTAL: 1"=20'  
VERTICAL: 1"=20'



**LEGEND**

PE	PROPOSED ELECTRIC
PES	PROPOSED ELECTRIC SERVICE
PCDM	PROPOSED COMM. SERVICE TV/PHONE
PG	PROPOSED GAS
PGS	PROPOSED GAS SERVICE
PW	PROPOSED WATER
PS	PROPOSED SEWER
PVS	PROPOSED WATER SERVICE
PSS	PROPOSED SEWER SERVICE
PV	PROPOSED FIRE HYDRANT W/GV
PS	PROPOSED STORM SEWER
PT	PROPOSED TRANSFORMER & SECONDARY PEDESTALS
PC	PROPOSED COMM. PEDS
PL	PROPERTY LINE

NO.	DATE	REVISIONS	BY
05/12/2023	05/12/2023	PRELIMINARY PLAN	MCW
12/12/2023	12/12/2023	FINAL SITE PLAN	MCW
01/25/2024	01/25/2024	RESPONSE TO COMMENTS	
05/14/2024	05/14/2024	FINAL SITE SUBMITTAL	

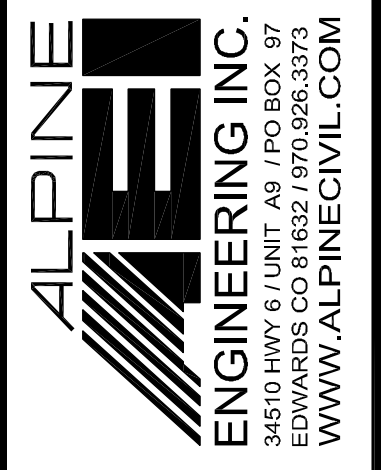
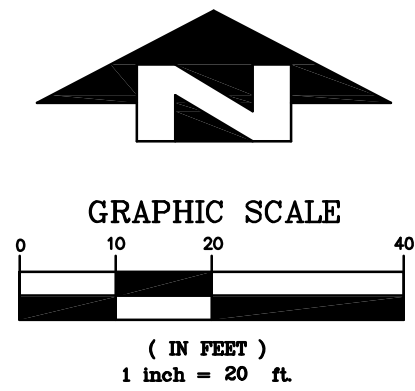
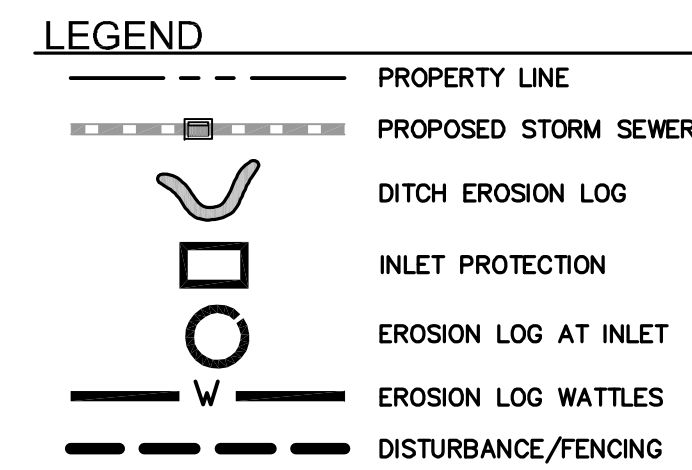
DESIGNED	MCW	DATE	05/11/2023
DRAWN	MCW		
CHECKED	MCW_TSL		
JOB NO.			

SHEET  
C4.0

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TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

CONTRACTOR SHALL DEVELOP A MEANS AND METHODS TO CONTROL SEDIMENT DURING CONSTRUCTION TO ASSURE THAT NO SEDIMENT IS DISCHARGED OFF-SITE.



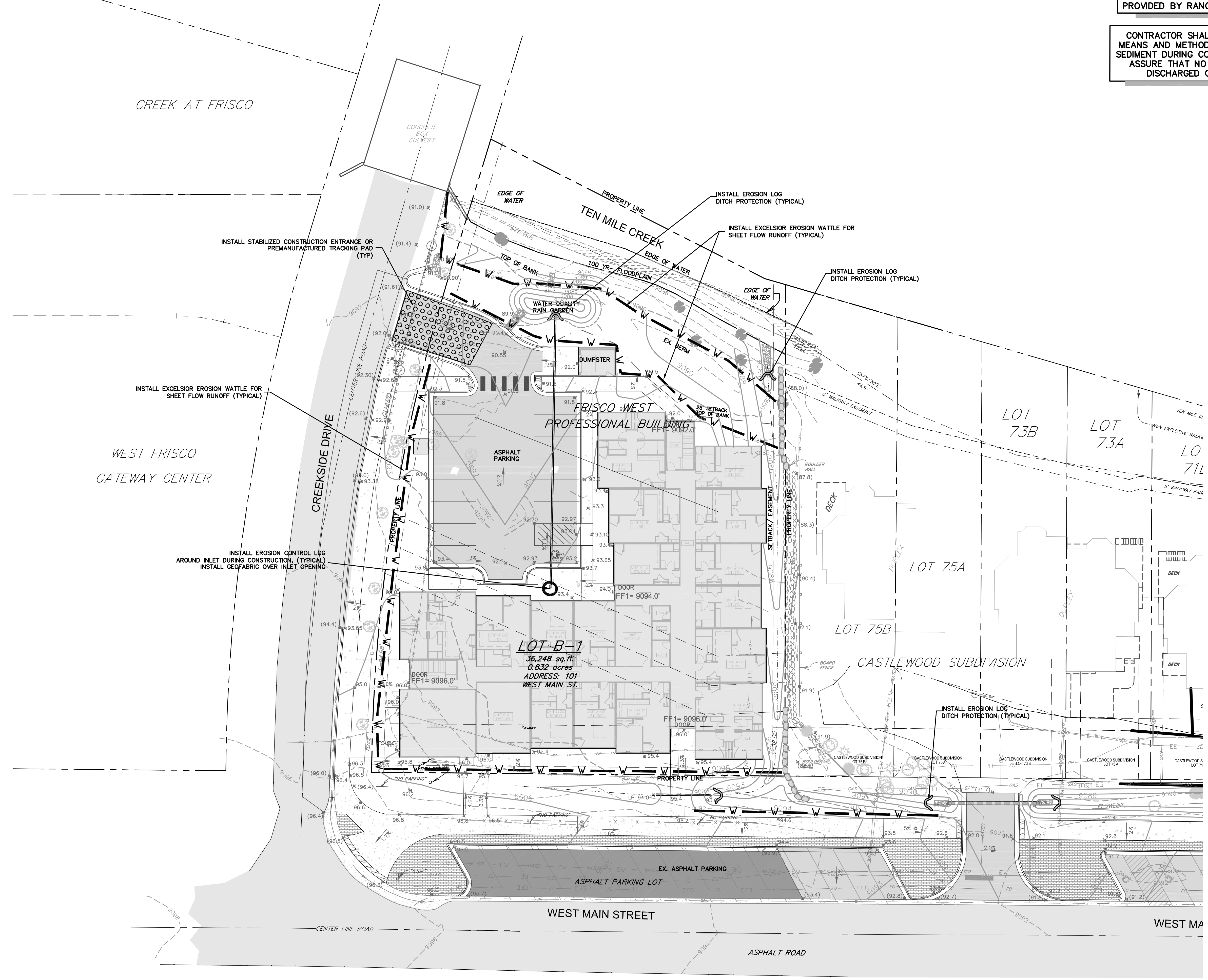
**101 WEST MAIN STREET**  
FRISCO, CO  
**EROSION CONTROL PLAN**

- GENERAL NOTES FOR SEDIMENT CONTROL**
- CONTRACTOR SHALL SUBMIT A CONSTRUCTION STAGING & MANAGEMENT PLAN IDENTIFYING CONSTRUCTION FENCING, STAGING, STORAGE & CONSTRUCTION TRAILER LOCATION PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
  - INSTALL AND MAINTAIN SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THESE PLANS AND AS NEEDED TO PREVENT SEDIMENT FROM DISCHARGING OFF-SITE OR ENTERING THE RIVER.
  - ALL PROPOSED SEDIMENT CONTROL MEASURES ARE TEMPORARY MEASURES UNLESS SPECIFIED OTHERWISE ON PLANS.
  - SEDIMENT CONTROL MEASURES MAY REQUIRE FIELD ADJUSTMENTS AT THE TIME OF CONSTRUCTION TO INSURE THAT THEIR INTENDED PURPOSE IS ACCOMPLISHED.
  - PROVIDE REGULAR INSPECTION AND MAINTENANCE OF ALL SEDIMENT CONTROL MEASURES TO INSURE THAT SEDIMENT CONTROL EFFICIENCY IS OBTAINED UNTIL FINAL STABILIZATION OF SITE HAS TAKEN PLACE.
  - INSTALL SEDIMENT CONTROL MEASURES AT THE ONSET OF GRADING OPERATIONS SO THAT EFFECTIVE SEDIMENT CONTROL CAN BE ACHIEVED DURING THE ENTIRE CONSTRUCTION PERIOD.
  - STABILIZE ALL POINTS OF INGRESS AND EGRESS WITH TRACKING PAD DURING CONSTRUCTION TO PREVENT TRACKING OF MUD ONTO PUBLIC WAYS.
  - FOR TEMPORARY STOCKPILES APPLY SEED, HYDROMULCH AND TACKIFIER IMMEDIATELY AFTER THEY ARE CONSTRUCTED FOR STABILIZATION. IF EROSION OCCURS AFTER APPLICATION OF THE TACKIFIER, USE EXCLESIOR C2 EROSION CONTROL FABRIC. INSTALL SILT FENCE BELOW STOCKPILES TO CAPTURE SEDIMENT.
  - THE TERM "REVEGETATION" ON THIS PLAN MEANS THE SUCCESSFUL GERMINATION AND ESTABLISHMENT OF STABLE GRASS COVER FROM A PROPERLY PREPARED SEEDBED CONTAINING THE SPECIFIED AMOUNTS OF FERTILIZER IN ACCORDANCE WITH APPLICABLE STANDARDS AND SPECIFICATIONS. REFER TO LANDSCAPE PLANS FOR SEED MIX, FERTILIZER TYPE, MULCH, TACKIFIER AND APPLICATION RATES.
  - IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE APPROPRIATE MEASURES TO INSURE THAT NO SEDIMENT LADEN WATER IS DISCHARGED FROM THE SITE.
  - APPROVAL SHALL BE REQUESTED UPON FINAL STABILIZATION OF ALL SITES BEFORE REMOVAL OF SEDIMENT CONTROLS.
  - CONTRACTOR SHALL OBTAIN AND CONFORM TO STORMWATER DISCHARGE PERMIT AND ALL ENVIRONMENTAL PERMITS AND KEEP STREETS CLEAN AND FREE OF SEDIMENT.
  - REMOVAL AND CLEANUP OF ANY SEDIMENT THAT LEAVES THE SITE IS THE RESPONSIBILITY OF THE CONTRACTOR.

- CONSTRUCTION SEQUENCE OF EROSION/SEDIMENT CONTROL MEASURES**  
BEFORE COMMENCING GRADING OR CONSTRUCTION
- CONSTRUCT STABILIZED CONSTRUCTION ENTRANCES AT ALL POINTS OF INGRESS AND EGRESS.
  - CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO ASSURE THAT NO SEDIMENT LEAVES THE SITE.
  - CONSTRUCT SILT FENCE AND WATTLES AND ALL SEDIMENT CONTROL DEVICES.
  - BEGIN DEMOLITION, EXCAVATION AND CONSTRUCTION.
  - INSTALL EROSION CONTROL MEASURES AFTER DITCHES AND SWALES HAVE BEEN CONSTRUCTED AND TOPSOIL AND SEED HAVE BEEN PLACED. INSTALL INLET PROTECTION IN ALL INLETS AS THEY ARE CONSTRUCTED.
  - TOPSOIL AND REVEGETATE ALL DISTURBED AREAS WITH APPROVED SEED MIX PER LANDSCAPE PLAN.
  - CONTRACTOR SHALL REMOVE SEDIMENT CONTROL FACILITIES AFTER FINAL STABILIZATION.

- FUGITIVE DUST CONTROL**  
THE CONTRACTOR IS RESPONSIBLE TO CONTROL FUGITIVE DUST AND TO INCORPORATE THE FOLLOWING:
- ALL UNPAVED ROADS AND OTHER DISTURBED AREAS ON SITE SHALL BE WATERED TO MINIMIZE FUGITIVE DUST.
  - HAUL ROADS SHALL BE TREATED WITH MAGNESIUM CHLORIDE IF WATER IS NOT CONTROLLING THE DUST.
  - ALL DISTURBED SURFACE AREAS SHALL BE REVEGETATED OR SURFACED PER THE LANDSCAPE PLAN AS SOON AS POSSIBLE.
  - MUD AND DIRT CARRYOUT ONTO PAVED SURFACES SHALL BE PREVENTED. ANY MUD AND DIRT CARRYOUT ONTO PAVED SURFACES SHALL BE CLEANED UP DAILY.

DESIGNED	NO.	DATE	REVISIONS
MCW	05/12/2023		PRELIMINARY PLAN
MCW	12/12/2023		FINAL SITE PLAN
MCW, TSL	01/25/2024		RESPONSE TO COMMENTS
MCW, TSL	05/14/2024		FINAL SITE SUBMITTAL
JOB NO.			
DATE	05/11/2023		



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LABEL	ITEM	COLOR	DESCRIPTION
M1	ASPHALT SHINGLE ROOFING		---GAF TIMBERLINE HD (LIFETIME): 'CHARCOAL'
M2	STANDING SEAM METAL ROOFING		---BRIDGER STEEL TRU SNAP: 16" W PANELS, 1.5" RIB KYNAR 500 'MATTE BLACK'
M3	FASCIA		---DIAMOND KOTE: FASCIA 'GRAPHITE'
M4	SOFFIT		---DIAMOND KOTE: VENTED SOFFIT 'CLAY'
M5	HORIZONTAL SIDING		---DIAMOND KOTE: HORIZONTAL SIDING 'COFFEE'
M6	VERTICAL SIDING		---DIAMOND KOTE: VERTICAL SIDING 'TERRA BRONZE'
M7	ACCENT SIDING		---CUSTOM METAL PANELS: BLACKENED STEEL W/ MATTE PERMALAC FINISH
M8	DOORS & WINDOWS		---ALUMINUM CLAD WOOD: JELD-WEN 'BLACK'
M9	EXPOSED TIMBERS		---STAIN WITH SUPERDECK 2318 'TEAK'
M10	STONE VENEER		---GALLEGOS CORPORATION "TIMBER RIDGE" DRY STACKED

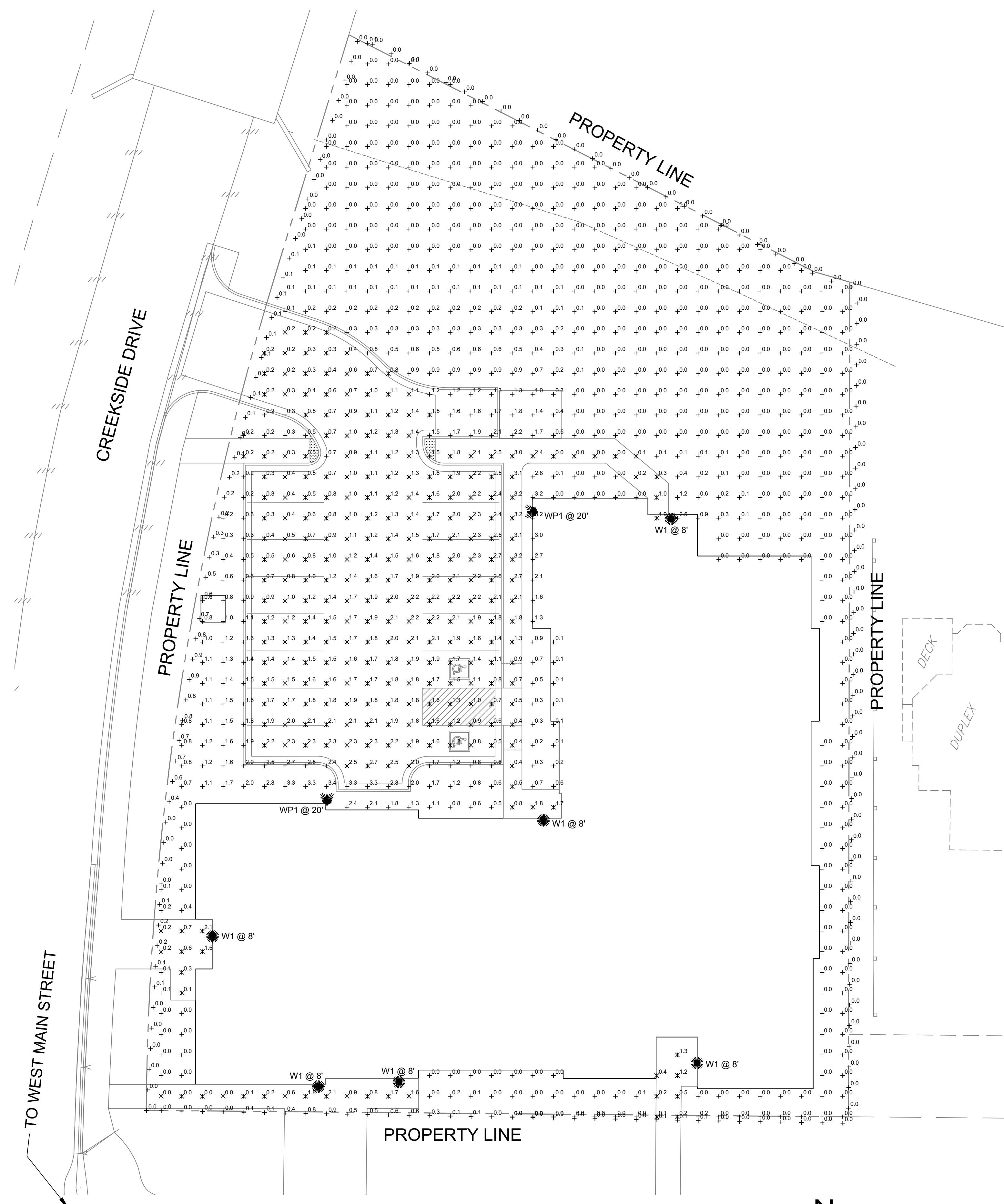
1  
A3.5 EXTERIOR MATERIALS LEGEND  
N.T.S.

101 WEST MAIN STREET  
LOT B-1, AMENDED WEST FRISCO 70, FILING #2  
TOWN OF FRISCO, COLORADO  
TITLE: EXTERIOR MATERIALS LEGEND

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ISSUE	DATE
PRELIM	7 DEC 2022
SKETCH	9 JAN 2023
REVIEW	2 MAY 2023
SKETCH	3 NOV 2023
PLANNING	13 MAY 2024

PROJECT# 2161



**1 LIGHTING SITE PLAN AND PHOTOMETRIC**  
 SCALE: 1" = 20'-0"

**GENERAL NOTES**

1. ANY PROPOSED LIGHT FIXTURES INSTALLED ON PRIVATE PROPERTY, ADJACENT TO THE PUBLIC RIGHT-OF-WAY, SHALL BE ORIENTED IN SUCH A MANNER OR LIMITED IN LUMEN OUTPUT TO PREVENT GLARE PROBLEMS AND SHALL NOT EXCEED NATIONAL I.E.S. LIGHTING STANDARDS FOR DISABILITY GLARE.
2. ONLY LIGHTING USED TO ACCENT ARCHITECTURAL FEATURES, LANDSCAPING, OR ART MAY BE DIRECTED UPWARD, PROVIDED THAT THE FIXTURES SHALL BE LOCATED, AIMED, OR SHIELDED TO MINIMIZE LIGHT SPILL INTO THE NIGHT SKY.
3. ALL PROVIDED EXTERIOR LUMINAIRES SHALL BE FULLY SHIELDED AND FULLY CUT-OFF.
4. VALUES SHOWN ARE MAINTAINED HORIZONTAL ILLUMINANCE VALUES MEASURED AT GRADE.

LIGHTING FIXTURE SCHEDULE									
MARK	QUANTITY	DESCRIPTION	MANUFACTURER	CATALOG NUMBER	MOUNTING	FINISH	LED		TOTAL WATTAGE
							CT	CRI	
W1	3	ENTRY WALL SCONCE	SOLAVANTI	KOTZA65-D6742AANW-30	WALL	BLACK	3000	80	6
WP1	3	AREA LIGHTING - WALL PACK	CREE	XSPW-B-WM-4ME-8L-30K-UL-BK	WALL	BLACK	3000	80	77

Statistics					
Description	Avg	Max	Min	Max/Min	Avg/Min
NW SIDEWALK	0.3 fc	0.5 fc	0.2 fc	2.5:1	1.5:1
PARKING LOT	1.4 fc	3.2 fc	0.0 fc	N/A	N/A
PROJECT SCOPE	0.6 fc	3.4 fc	0.0 fc	N/A	N/A
SE SIDEWALK	0.6 fc	1.3 fc	0.1 fc	13.0:1	6.0:1
SITE BOUNDARY	0.1 fc	0.9 fc	0.0 fc	N/A	N/A
SW SIDEWALK	0.8 fc	2.1 fc	0.0 fc	N/A	N/A
WEST SIDEWALK	0.7 fc	2.1 fc	0.1 fc	21.0:1	7.0:1
NORTH SIDEWALK	1.4 fc	2.7 fc	0.2 fc	13.5:1	7.0:1

**FIXTURE TYPE "WP1"**

XSPW	B	WM	Optic	Lumen Package*	CCT	Voltage	Color Options	Options
XSPW	B	WM	2ME Type III Medium	ZL 2,470 lumens 4L 4,270 lumens 4ME 6,100 lumens 8L 8,475 lumens	3000K 70 CRI 4000K 75 CRI 5000K 90 CRI	UL Universal 120-277V UL Universal 347-480V 3L 247V 4L 347V 50K/70 CRI 57K/70 CRI	BK Black BZ Bronze SV Silver WH White	ML Multi-Level Refer to ML_Spec sheet for details - Available with UL voltage only P Bulb Protocol - Not available with ML or PML options - Available with UL and 84 voltages only PML Programmable Multi-Level Refer to PML_Spec sheet for details - Available with UL voltage only

Type IV Medium Distribution		Electrical Data*	
Lumen Package	Initial Delivered Lumens*	Lumen Package	CCT/CRI
ZL	2,470	B1 U0 G1	3000K/70 CRI
4L	4,270	B1 U0 G1	4000K/70 CRI
4L	4,100	B1 U0 G2	50K/70 CRI
8L	8,475	B1 U0 G2	57K/70 CRI

3000K  
 Initial Delivered Lumens\*  
 BUG Ratings\*\* Per TM-15-11  
 System Watts  
 Efficacy  
 120V

4.8" (122mm)  
 3.8" (97mm)  
 9.3" (236mm)  
 17" (430mm)  
 Multi-Level Sensor location (ordered as an option)

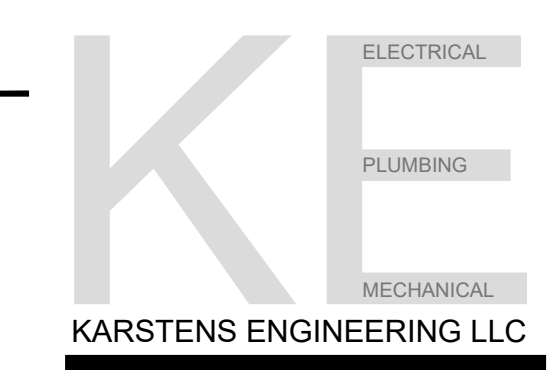
RESTL Test Report # PL 1415-001A  
 XSPW-B-WM-4ME-8L-30K-UL  
 Initial Delivered Lumens: 8,743

XSPW-B-WM-4ME-8L-30K-UL  
 Mounting Height: 15' 6" (4.7 m)  
 Initial Delivered Lumens: 8,475  
 Initial FC at grade

**FIXTURE TYPE "W1"**

1.53"  
 7.87"  
 4.92"  
 D67472A

FAMILY	ITEM#	WATTAGE	LUMENS	CCT	BEAM	FINISH	DRIVER	IP RATING	INPUT VOLTAGE
KOTZA65	D67472AANW D67472AANW	2x6W 6W	2x500lm 500lm	30 3000K 30 3000K	G GENERAL	AN ANTHRACITE	N NON-DIM/ 65	IP65	120/277 120V-277V



Contact:  
 Trevor Karstens, PE  
 trevor@karstensengineering.com  
 913.219.3848

**101 WEST MAIN STREET  
 LOT B-1 AMENDED WEST FRISCO 70 FILING #2  
 TOWN OF FRISCO, COLORADO**

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**REVISION:**

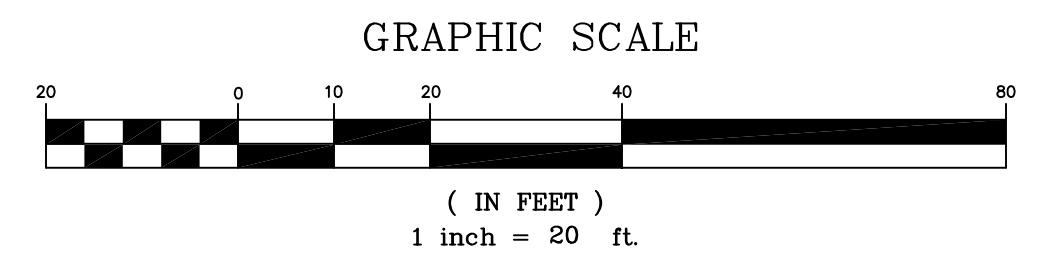
**PLANNING SUBMITTAL**  
 DATE: 12/12/2023  
 DRAWN BY: TBK  
 CHECKED BY: TBK

**PHOTOMETRIC**

**E01**

# AN IMPROVEMENT SURVEY PLAT AND TOPOGRAPHIC MAP OF LOT B-1, AMENDED WEST FRISCO 70, FILING NO. 2

ACCORDING TO THE PLAT RECORDED AT REC. NO. 140796  
TOWN OF FRISCO, SUMMIT COUNTY, COLORADO



ELEVATIONS BASED ON U.S.G.S. DATUM (NAVD 88)  
DATE OF FIELD SURVEY: 03/22/07  
DATE OF FIELD SURVEY UPDATE: 04/25/13  
DATE OF FIELD SURVEY UPDATE: 12/20/2022  
CONTOUR INTERVAL=2 FEET

### LEGEND

- FOUND REBAR & PLASTIC CAP (PLS 26292)
- FOUND No. 4 REBAR
- WATER VALVE
- ⊕ FIRE HYDRANT
- UTILITY PEDESTAL
- ☆ LIGHT POLE
- ⊙ SEWER MANHOLE
- PINE TREE WITH TRUNK DIAMETER
- SPRUCE TREE WITH TRUNK DIAMETER
- ASPEN TREE WITH TRUNK DIAMETER
- ⊞ TRANSFORMER
- x — x — RAIL FENCE
- SS — SS — SS — SANITARY SEWER
- GAS — GAS — GAS — GAS LINE
- WATER — WATER — WATER — WATER LINE
- ELEC — ELEC — ELEC — ELECTRIC LINE
- PH — PH — PH — PHONE LINE
- FO — FO — FO — FIBER OPTIC LINE

EASEMENT IS ALMOST ENTIRELY WITHIN WATER OF TEN MILE CREEK. RECOMMEND CHANGING TO "OPEN SPACE AND WALKWAY" EASEMENT AND/OR EXPANDING/RELOCATING TO THE SOUTH TO MAKE IT USEABLE

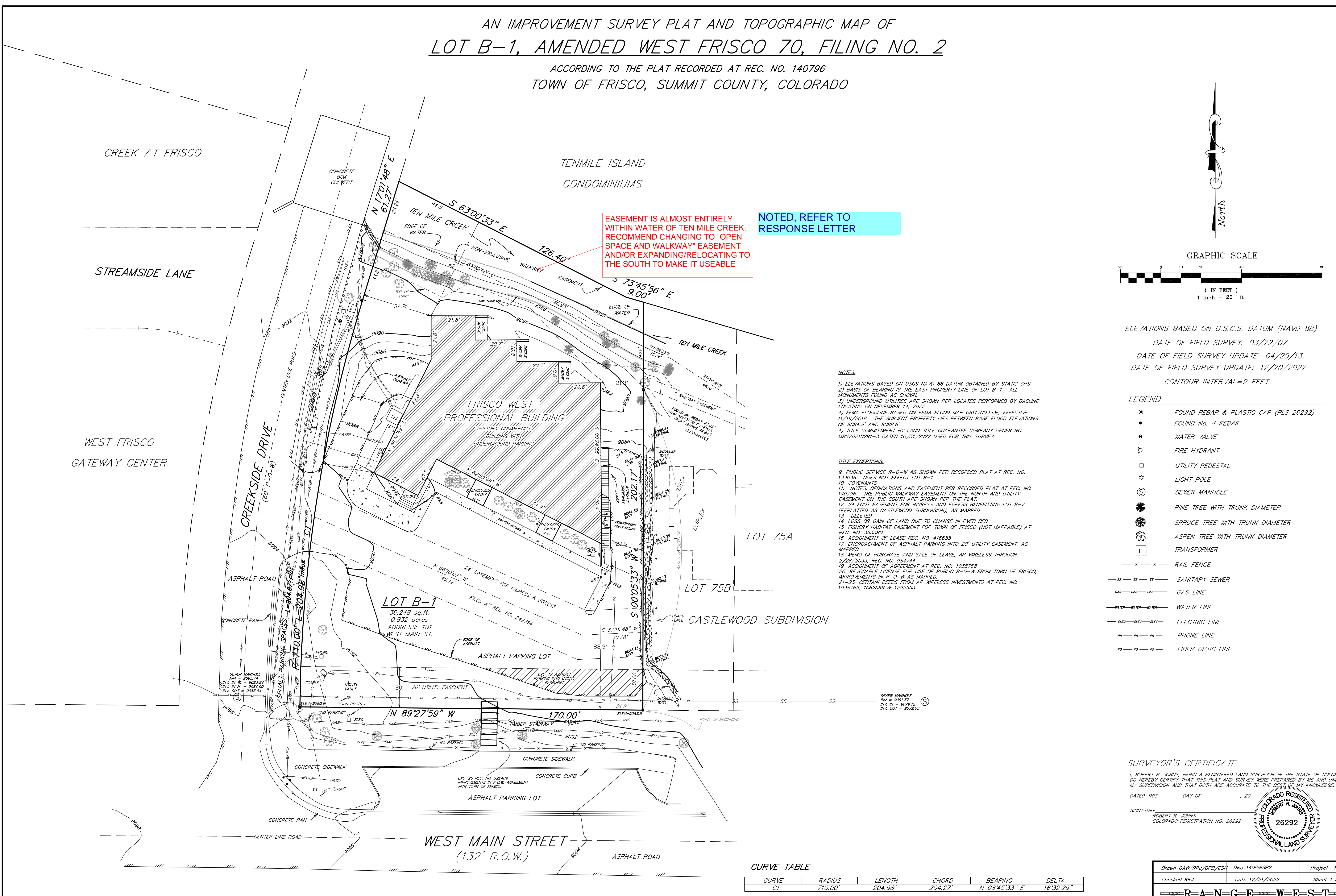
NOTED, REFER TO RESPONSE LETTER

### NOTES:

- 1) ELEVATIONS BASED ON USGS NAVD 88 DATUM OBTAINED BY STATIC GPS
- 2) BASIS OF BEARING IS THE EAST PROPERTY LINE OF LOT B-1. ALL MONUMENTS FOUND AS SHOWN.
- 3) UNDERGROUND UTILITIES ARE SHOWN PER LOCATES PERFORMED BY BASLINE LOCATING ON DECEMBER 14, 2022
- 4) FEMA FLOODLINE BASED ON FEMA FLOOD MAP 08117C0353F, EFFECTIVE 11/16/2018. THE SUBJECT PROPERTY LIES BETWEEN BASE FLOOD ELEVATIONS OF 9084.9' AND 9088.6'.
- 4) TITLE COMMITMENT BY LAND TITLE GUARANTEE COMPANY ORDER NO. MRG20210291-3 DATED 10/31/2022 USED FOR THIS SURVEY.

### TITLE EXCEPTIONS:

9. PUBLIC SERVICE R-O-W AS SHOWN PER RECORDED PLAT AT REC. NO. 133038. DOES NOT EFFECT LOT B-1
10. COVENANTS
11. NOTES, DEDICATIONS AND EASEMENT PER RECORDED PLAT AT REC. NO. 140796. THE PUBLIC WALKWAY EASEMENT ON THE NORTH AND UTILITY EASEMENT ON THE SOUTH ARE SHOWN PER THE PLAT.
12. 24 FOOT EASEMENT FOR INGRESS AND EGRESS BENEFITTING LOT B-2 (REPLATTED AS CASTLEWOOD SUBDIVISION), AS MAPPED
13. DELETED
14. LOSS OR GAIN OF LAND DUE TO CHANGE IN RIVER BED
15. FISHERY HABITAT EASEMENT FOR TOWN OF FRISCO (NOT MAPPABLE) AT REC. NO. 393380
16. ASSIGNMENT OF LEASE REC. NO. 416655
17. ENCROACHMENT OF ASPHALT PARKING INTO 20' UTILITY EASEMENT, AS MAPPED.
18. MEMO OF PURCHASE AND SALE OF LEASE, AP WIRELESS THROUGH 2,218/2033 REC. NO. 984744
19. ASSIGNMENT OF AGREEMENT AT REC. NO. 1038769
20. REVOCABLE LICENSE FOR USE OF PUBLIC R-O-W FROM TOWN OF FRISCO, IMPROVEMENTS IN R-O-W AS MAPPED.
- 21-23. CERTAIN DEEDS FROM AP WIRELESS INVESTMENTS AT REC. NO. 1038769, 1062569 & 1292553.



### CURVE TABLE

CURVE	RADIUS	LENGTH	CHORD	BEARING	DELTA
C1	710.00'	204.98'	204.27'	N 08°45'33" E	16°32'29"

### SURVEYOR'S CERTIFICATE

I, ROBERT R. JOHNS, BEING A REGISTERED LAND SURVEYOR IN THE STATE OF COLORADO, DO HEREBY CERTIFY THAT THIS PLAT AND SURVEY WERE PREPARED BY ME AND UNDER MY SUPERVISION AND THAT BOTH ARE ACCURATE TO THE BEST OF MY KNOWLEDGE.

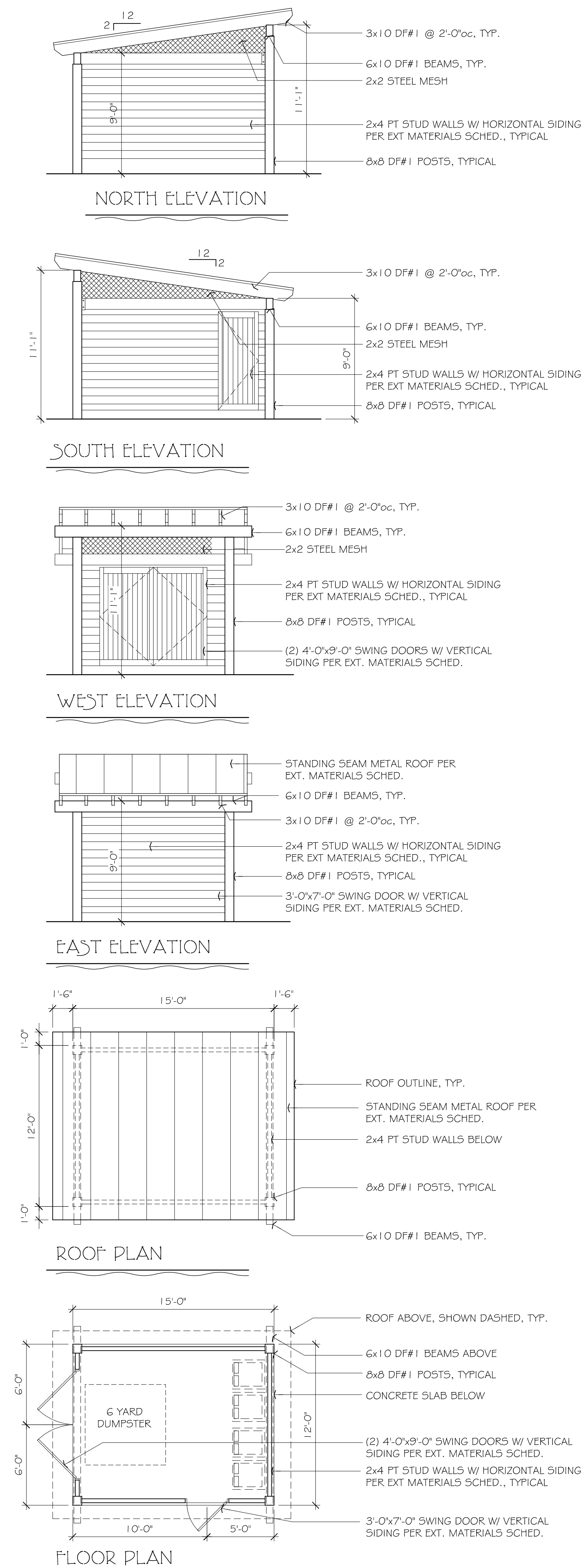
DATED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_

SIGNATURE: ROBERT R. JOHNS  
COLORADO REGISTRATION NO. 26292

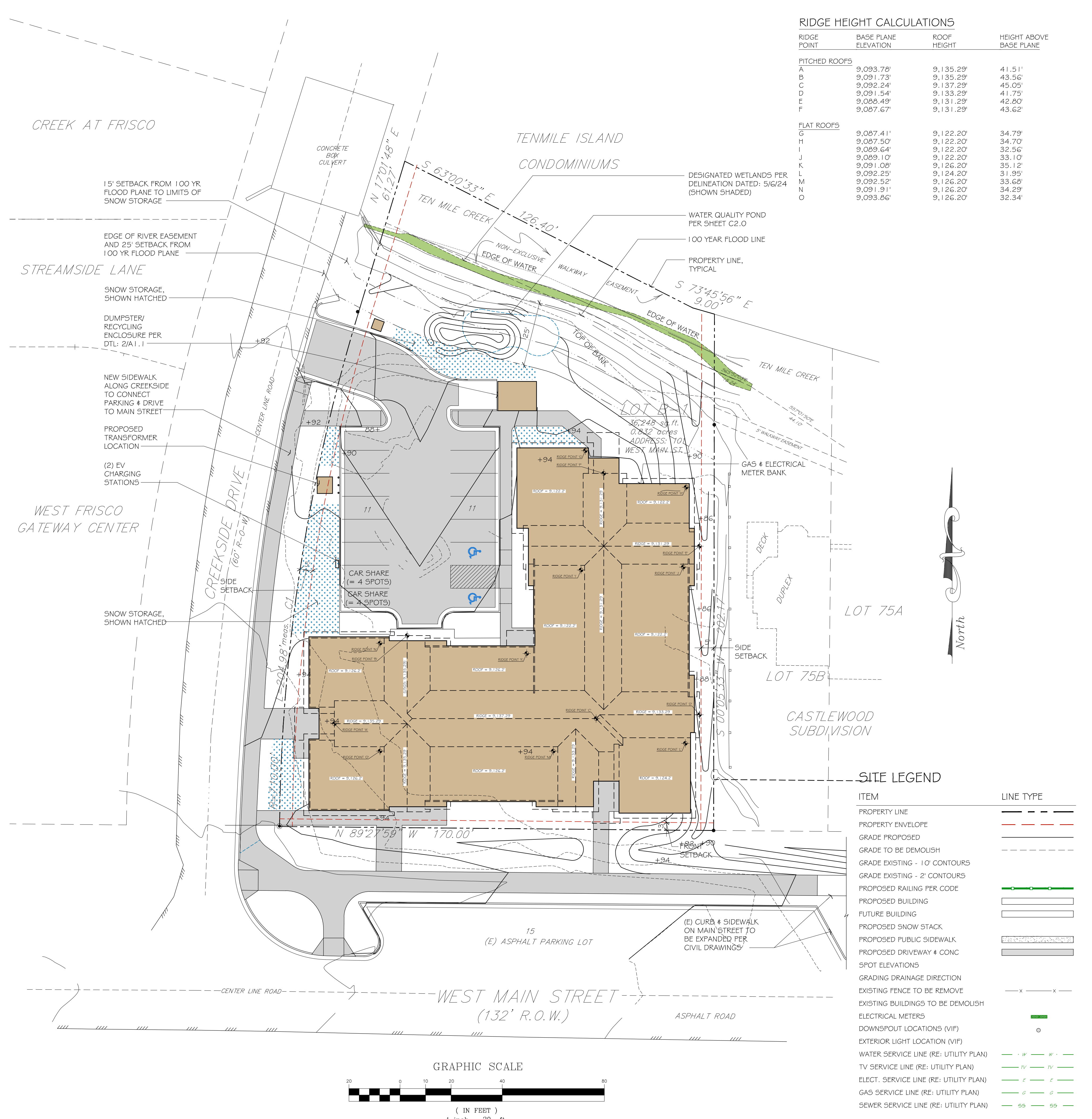


Drawn GAW/RRJ/DPB/ESH	Dwg 140891SP2	Project 14089_22
Checked RRJ	Date 12/21/2022	Sheet 1 of 1
<b>R-A-N-G-E-W-E-S-T</b> ENGINEERS & SURVEYORS INC.		
P.O. Box 589 Silverthorne, CO 80498 970-468-6281		

NOTE: ACCORDING TO COLORADO LAW, YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT, IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.



2 DUMPSTER ENCLOSURE DETAILS  
SCALE: 3/16" = 1'-0"



RIDGE HEIGHT CALCULATIONS

RIDGE POINT	BASE PLANE ELEVATION	ROOF HEIGHT	HEIGHT ABOVE BASE PLANE
<b>PITCHED ROOFS</b>			
A	9,093.78'	9,135.29'	41.51'
B	9,091.73'	9,135.29'	43.56'
C	9,092.24'	9,137.29'	45.05'
D	9,091.54'	9,133.29'	41.75'
E	9,088.49'	9,131.29'	42.80'
F	9,087.67'	9,131.29'	43.62'
<b>FLAT ROOFS</b>			
G	9,087.41'	9,122.20'	34.79'
H	9,087.50'	9,122.20'	34.70'
I	9,089.64'	9,122.20'	32.56'
J	9,089.10'	9,122.20'	33.10'
K	9,091.08'	9,126.20'	35.12'
L	9,092.25'	9,124.20'	31.95'
M	9,092.52'	9,126.20'	33.68'
N	9,091.91'	9,126.20'	34.29'
O	9,093.86'	9,126.20'	32.34'

SITE LEGEND

ITEM	LINE TYPE
PROPERTY LINE	---
PROPERTY ENVELOPE	---
GRADE PROPOSED	---
GRADE TO BE DEMOLISH	---
GRADE EXISTING - 10' CONTOURS	---
GRADE EXISTING - 2' CONTOURS	---
PROPOSED RAILING PER CODE	---
PROPOSED BUILDING	---
FUTURE BUILDING	---
PROPOSED SNOW STACK	---
PROPOSED PUBLIC SIDEWALK	---
PROPOSED DRIVEWAY & CONC	---
SPOT ELEVATIONS	o
GRADING DRAINAGE DIRECTION	---
EXISTING FENCE TO BE REMOVE	-x-x-
EXISTING BUILDINGS TO BE DEMOLISH	---
ELECTRICAL METERS	o
DOWNSPOUT LOCATIONS (VIP)	o
EXTERIOR LIGHT LOCATION (VIP)	o
WATER SERVICE LINE (RE: UTILITY PLAN)	--- W --- W ---
TV SERVICE LINE (RE: UTILITY PLAN)	--- TV --- TV ---
ELECT. SERVICE LINE (RE: UTILITY PLAN)	--- E --- E ---
GAS SERVICE LINE (RE: UTILITY PLAN)	--- G --- G ---
SEWER SERVICE LINE (RE: UTILITY PLAN)	--- S --- S ---

101 WEST MAIN STREET  
LOT D-1, AMENDED WEST FRISCO 70, FILING #2  
TOWN OF FRISCO, COLORADO

TITLE

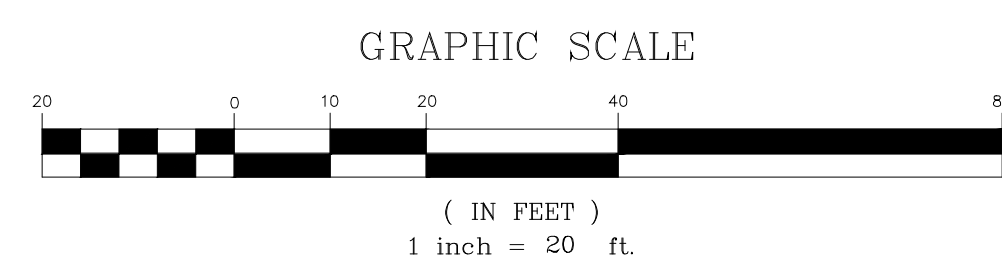
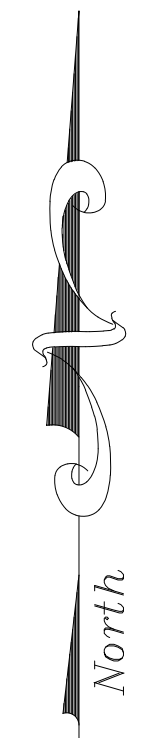
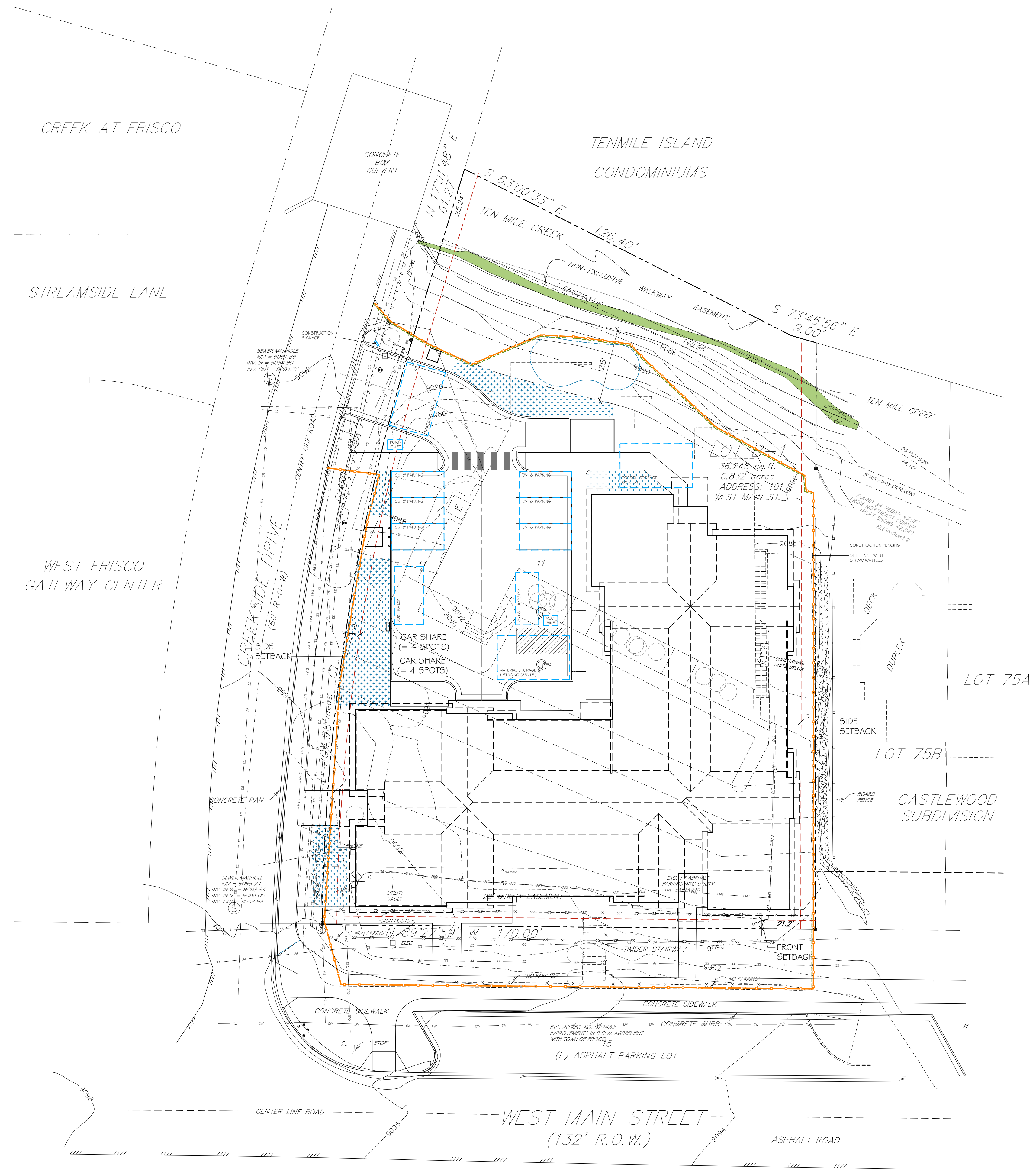
PROPOSED SITE PLAN

ISSUE:	DATE:
DRC	7 DEC 2022
SKETCH	9 JAN 2023
SKETCH	3 MAY 2023
UPDATE	8 AUG 2023
DRC	26 OCT 2023
SKETCH	3 NOV 2023
PLANNING	13 MAY 2024
UPDATE	31 MAY 2024

PROJECT #: 22105

CONST. MANAGEMENT LEGEND

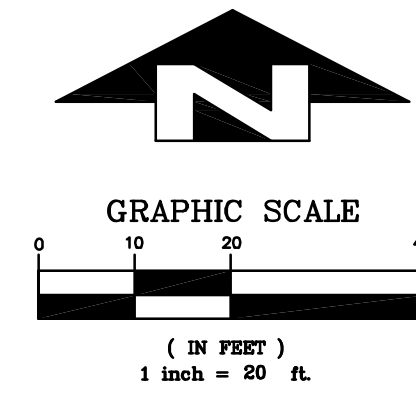
ITEM	LINE TYPE
PROPERTY LINE	--- ---
PROPERTY ENVELOPE	— — — —
CONSTRUCTION FENCE	— — — —
SILT FENCE (AS REQUIRED)	— — — —
CONSTRUCTION PHASE ELEMENTS	— — — —
PROPOSED BUILDING	— — — —
FUTURE BUILDING	— — — —
WATER SERVICE LINE (RE: UTILITY PLAN)	— W — W —
TV SERVICE LINE (RE: UTILITY PLAN)	— TV — TV —
ELECT. SERVICE LINE (RE: UTILITY PLAN)	— E — E —
GAS SERVICE LINE (RE: UTILITY PLAN)	— G — G —
SEWER SERVICE LINE (RE: UTILITY PLAN)	— SS — SS —
EXISTING FENCE TO BE REMOVED	— X — X —
ELECTRICAL METERS (RE: UTILITY PLAN)	— M —



101 WEST MAIN STREET  
LOT D-1, AMENDED WEST FRISCO 70, FILING #2  
TOWN OF FRISCO, COLORADO

CONSTRUCTION MANAGEMENT PLAN

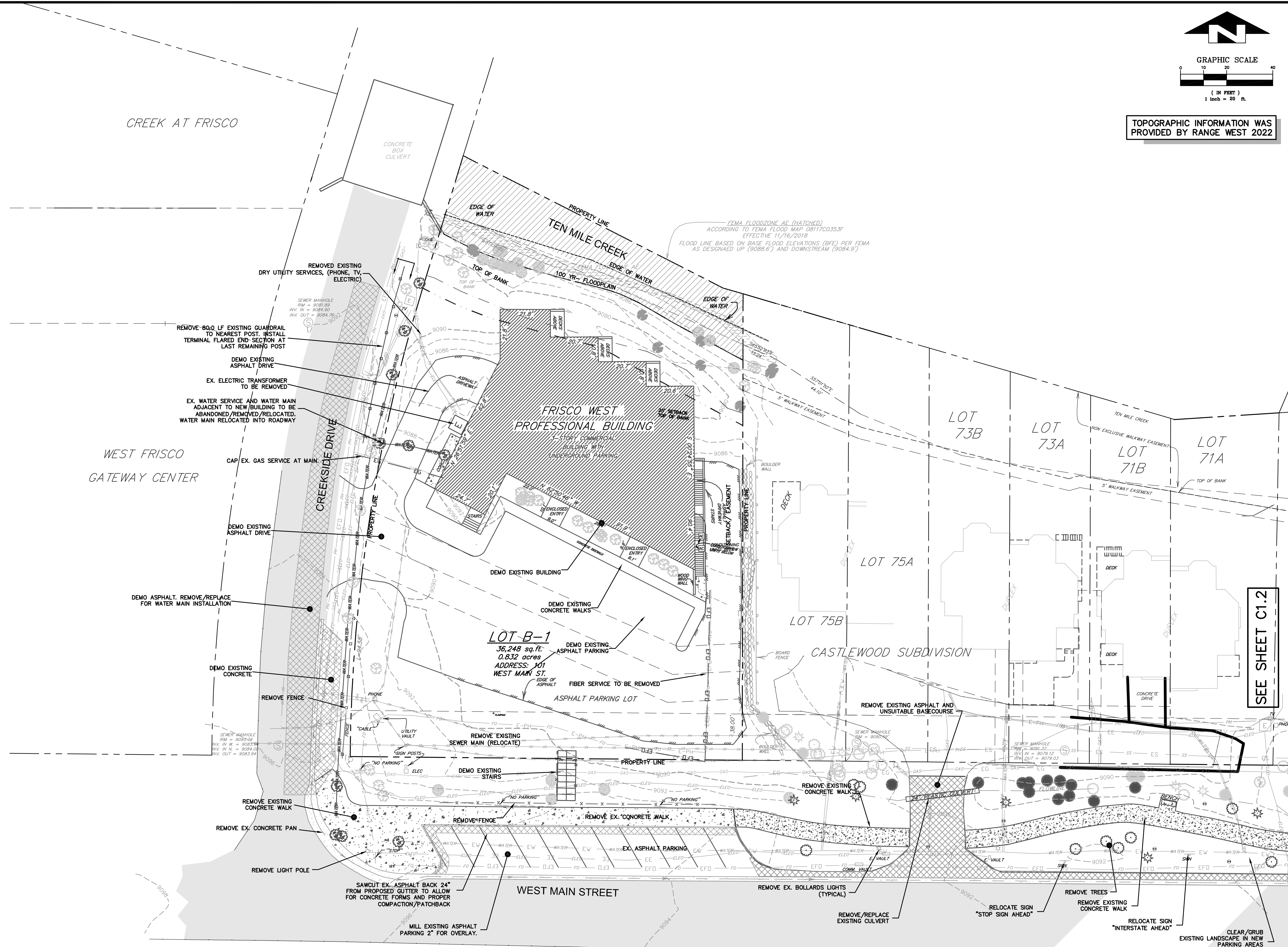
ISSUE:	DATE:
DRC	7 DEC 2022
SKETCH	9 JAN 2023
SKETCH	3 MAY 2023
UPDATE	8 AUG 2023
DRC	26 OCT 2023
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PLANNING	13 MAY 2024
UPDATE	31 MAY 2024



TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022



**101 WEST MAIN STREET**  
FRISCO, CO  
EXISTING CONDITIONS/DEMOLITION



SEE SHEET C1.2

NO.	DATE	REVISIONS	BY
05/12/2023	PRELIMINARY PLAN	MCW	
12/12/2023	FINAL SITE PLAN	MCW	
01/25/2024	RESPONSE TO COMMENTS		
05/14/2024	FINAL SITE SUBMITTAL		
05/31/2024	RESPONSE TO COMMENTS		

DESIGNED	MCW	DATE	05/11/2023
DRAWN	MCW		
CHECKED	MCW_TSL		
JOB NO.			

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FEMA FLOODZONE AE (HATCHED)  
 ACCORDING TO FEMA FLOOD MAP 08177C0353F  
 EFFECTIVE 11/16/2018  
 100' LINE BASED ON BASE FLOOD ELEVATIONS (BFE) PER FEMA  
 AS DESIGNATED UP (9088.6') AND DOWNSTREAM (9084.9')



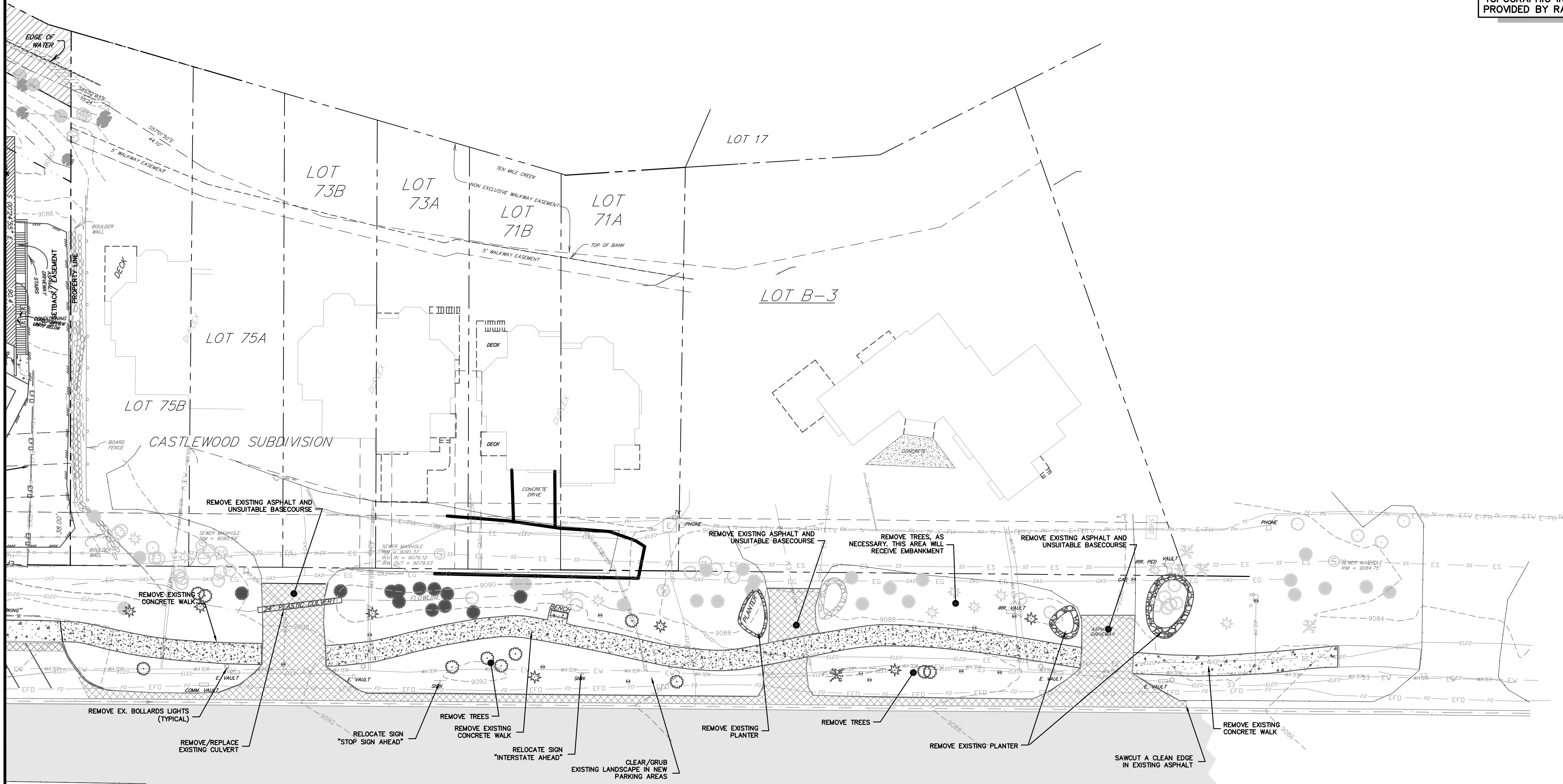
GRAPHIC SCALE  
 ( IN FEET )  
 1 inch = 20 ft.

TOPOGRAPHIC INFORMATION WAS  
 PROVIDED BY RANGE WEST 2022

ALPINE  
**ENGINEERING INC.**  
 34510 HWY 6 UNIT A9 P.O. BOX 97  
 EDWARDS CO BR527 8702963073  
 WWW.ALPINEEIVL.COM



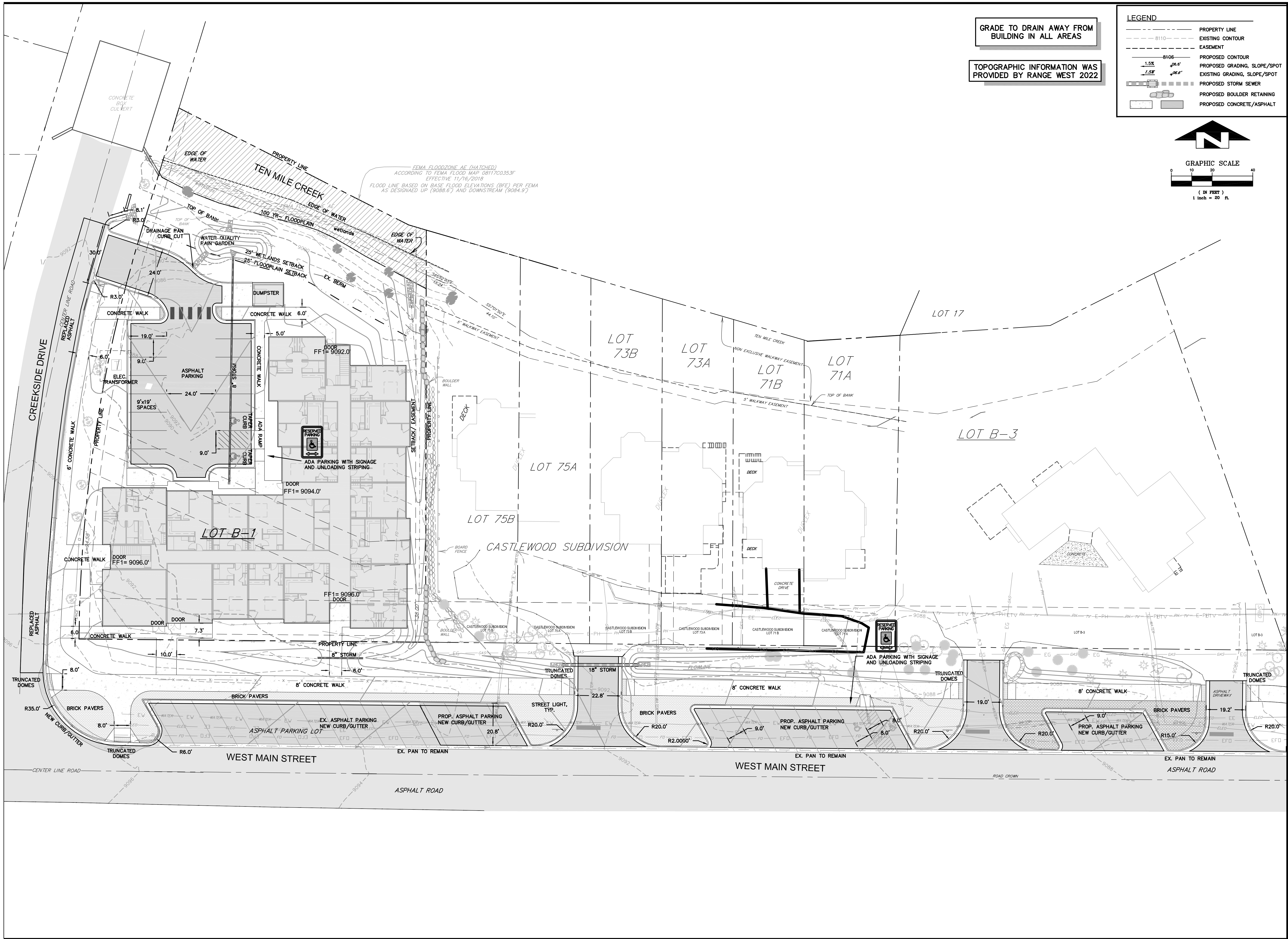
**101 WEST MAIN STREET**  
**FRISCO, CO**  
**EXISTING CONDITIONS/DEMOLITION**



SEE SHEET C1.1

DESIGNED MCW	NO.	DATE	REVISIONS	BY
DRAWN MCW	05/12/2023		PRELIMINARY PLAN	MCW
CHECKED MCW_TSL	12/12/2023		FINAL SITE PLAN	MCW
JOB NO. --	01/25/2024		RESPONSE TO COMMENTS	
DATE 05/11/2023	05/14/2024		FINAL SITE SUBMITTAL	
	05/31/2024		RESPONSE TO COMMENTS	

SHEET  
 C1.2

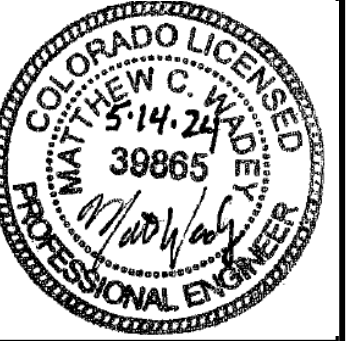
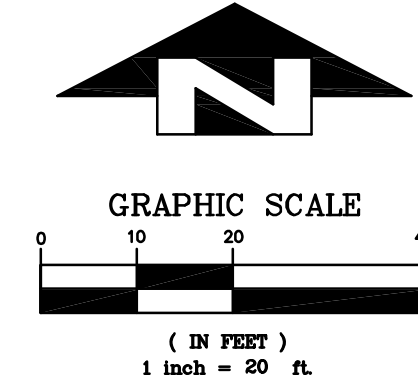


GRADE TO DRAIN AWAY FROM BUILDING IN ALL AREAS

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

**LEGEND**

	PROPERTY LINE
	EXISTING CONTOUR
	EASEMENT
	PROPOSED CONTOUR
	PROPOSED GRADING, SLOPE/SPOT
	EXISTING GRADING, SLOPE/SPOT
	PROPOSED STORM SEWER
	PROPOSED BOULDER RETAINING
	PROPOSED CONCRETE/ASPHALT



**101 WEST MAIN STREET**  
FRISCO, CO  
SITE LAYOUT PLAN

NO.	DATE	REVISIONS	BY
05/12/2023	PRELIMINARY PLAN	MCW	MCW
12/12/2023	FINAL SITE PLAN	MCW	MCW
01/25/2024	RESPONSE TO COMMENTS		
05/14/2024	FINAL SITE SUBMITTAL		
05/31/2024	RESPONSE TO COMMENTS		

DESIGNED	MCW	DATE	05/11/2023
DRAWN	MCW		
CHECKED	MCW, TSL		
JOB NO.			

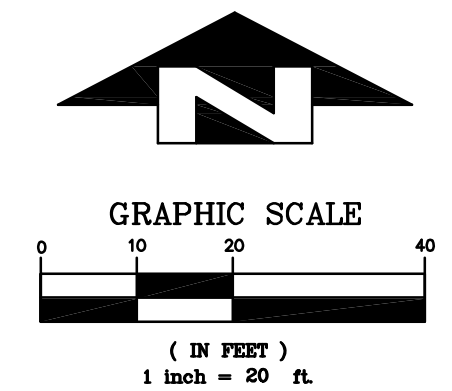
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GRADE TO DRAIN AWAY FROM BUILDING IN ALL AREAS

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

**LEGEND**

- 8110 PROPERTY LINE
- 8110 EXISTING CONTOUR
- 8106 EASEMENT
- 1.5% PROPOSED CONTOUR
- 1.5% PROPOSED GRADING, SLOPE/SPOT
- 1.5% EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT



**ALPINE ENGINEERING INC.**  
 5410 HWY G UNIT A9 FPO BOX 97  
 FORT COLLINS COLO 80526  
 WWW.ALPINECVIL.COM



**101 WEST MAIN STREET**  
 FRISCO, CO  
 GRADING AND DRAINAGE

CREEK AT FRISCO

REMOVE 80.0 LF EXISTING GUARDRAIL, INSTALL MSKT NON FLARED TERMINAL END SECTION. SEE CDOT M STANDARDS- M660-1 (PAGE 8 OF 19)

INSTALL CONCRETE FLARED DRAINAGE PAN (8 FT LONG), INSTALL RIPRAP BLANKET UNDERLAIN WITH GEOTEXTILE. CONSTRUCT 8" WIDE GRAVEL LEVEL SPREADER ALONG CONTOUR WITH 2"-3" WASHED ROCK (6" THICK BLANKET) TOP OF SPREADER ELEVATION=9090.90'

CONSTRUCT 4" WIDE CONCRETE DRAINAGE PAN AT ENTRANCE, 2" INVERT TO PAN FLOWLINE

MATCH ELEVATION OF EXISTING ROAD.

CONSTRUCT ADA RAMP WITH TRUNCATED DOME METAL PLATE (EAST JORDAN IRONWORKS OR SIMILAR)

CROSSWALK MARKINGS 2" WIDE WITH 2" GAP

ASPHALT PARKING LOT- 4" ASPHALT OVER 6" CLASS 6 BASECOURSE

PARKING LOT-INSTALL CURB AND GUTTER. 6" VERTICAL CURB AND 18" GUTTER PAN.

WEST FRISCO GATEWAY CENTER

REMOVE/ REPLACE ROADWAY ASPHALT TO CENTERLINE OF ROAD FOR WATER MAIN CONSTRUCTION. SEE UTILITY PLANS

INSTALL CURB AND GUTTER. 6" VERTICAL CURB AND 18" GUTTER PAN.

CONSTRUCT 6" WIDE ATTACHED CONCRETE SIDEWALK. 5" THICK OVER 4" BASECOURSE

REMOVE/ REPLACE CONCRETE FOR WATER MAIN CONSTRUCTION. SEE UTILITY PLANS

CONSTRUCT ADA RAMP. INSTALL 2" WIDE TRUNCATED DOME METAL PLATES. EAST JORDAN IRONWORKS (OR SIMILAR)

ASSURE (4) EX. GATE VALVES BEHIND BACK OF NEW CURBING. ADJUST TO NEW GRADE

PATCHBACK ASPHALT AT CONC. PAN REMOVAL CENTER LINE ROAD. SAWCUT EXISTING DRAINAGE PAN. DOWEL CONNECTION WITH NEW GUTTER (2) #4 REBAR.

CONSTRUCT CONCRETE WALK 5" THICK CONC. OVER 4" CLASS 6 BASECOURSE, TYPICAL

MILL EXISTING ASPHALT PARKING 2" DEPTH FOR NEW ASPHALT OVERLAY.

CONSTRUCT CONCRETE CURB AND GUTTER. 18" WIDE GUTTER AND 6" TALL CURB

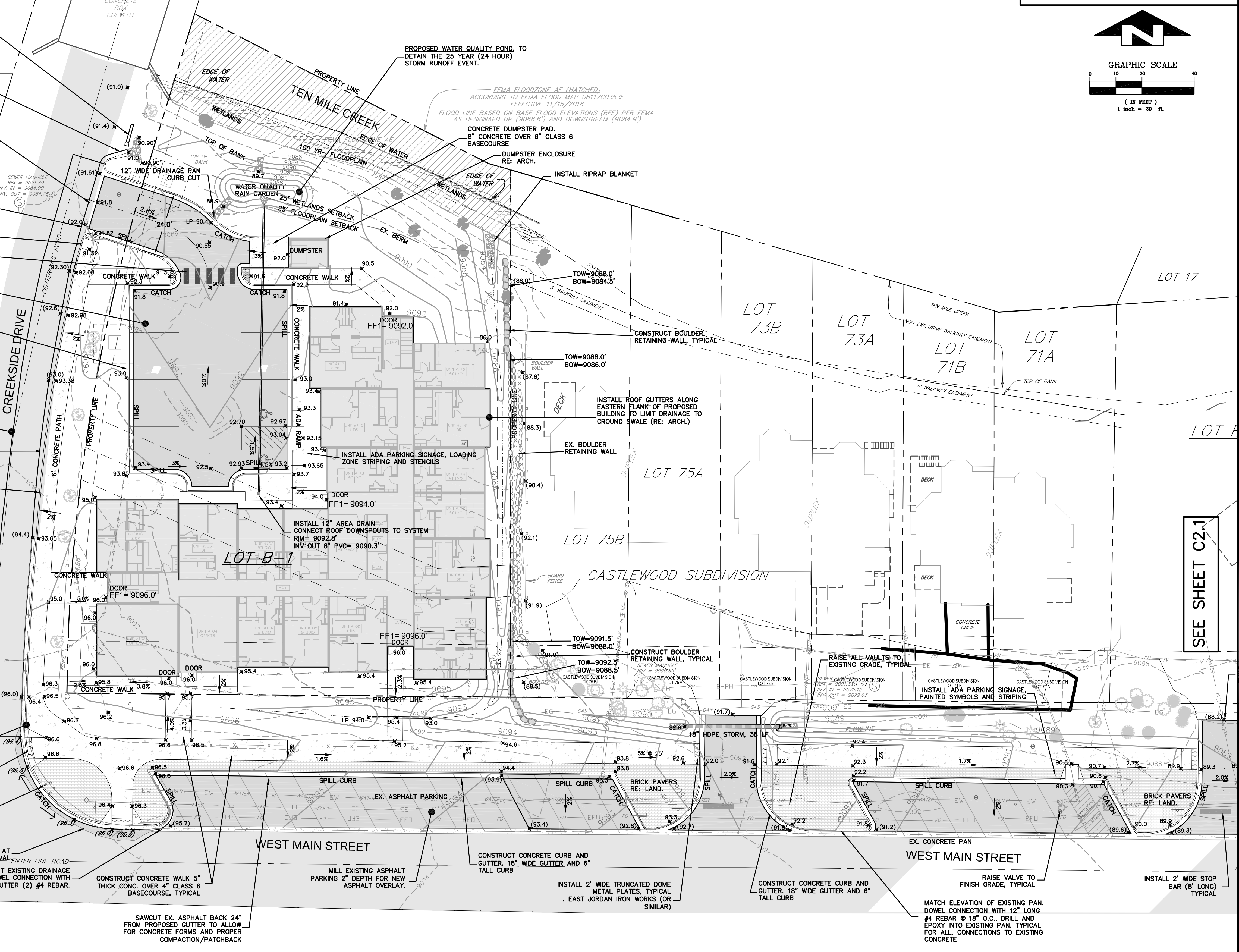
INSTALL 2" WIDE TRUNCATED DOME METAL PLATES, TYPICAL. EAST JORDAN IRONWORKS (OR SIMILAR)

CONSTRUCT CONCRETE CURB AND GUTTER. 18" WIDE GUTTER AND 6" TALL CURB

RAISE VALVE TO FINISH GRADE, TYPICAL

INSTALL 2" WIDE STOP BAR (8' LONG) TYPICAL

MATCH ELEVATION OF EXISTING PAN. DOWEL CONNECTION WITH 12" LONG #4 REBAR @ 18" O.C., DRILL AND EPOXY INTO EXISTING PAN. TYPICAL FOR ALL CONNECTIONS TO EXISTING CONCRETE



SEE SHEET C2.1

NO.	DATE	REVISIONS	BY
05/12/2023	PRELIMINARY PLAN	MCW	
12/12/2023	FINAL SITE PLAN	MCW	
01/25/2024	RESPONSE TO COMMENTS	MCW	
05/14/2024	FINAL SITE SUBMITTAL	MCW	
05/31/2024	RESPONSE TO COMMENTS	MCW	

DESIGNED MCW  
 DRAWN MCW  
 CHECKED MCW, TSL  
 JOB NO. ---  
 DATE 05/11/2023

**SHEET C2.0**

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AS (HATCHED)  
 20 MAP 0811700353F  
 7/6/2018  
 1 ELEVATIONS (BFE) PER FEMA  
 NO DOWNSTREAM (9084.5)

ASS 6  
 OSURE  
 INSTALL RIPRAP BLANKET

GRADE TO DRAIN AWAY FROM  
 BUILDING IN ALL AREAS

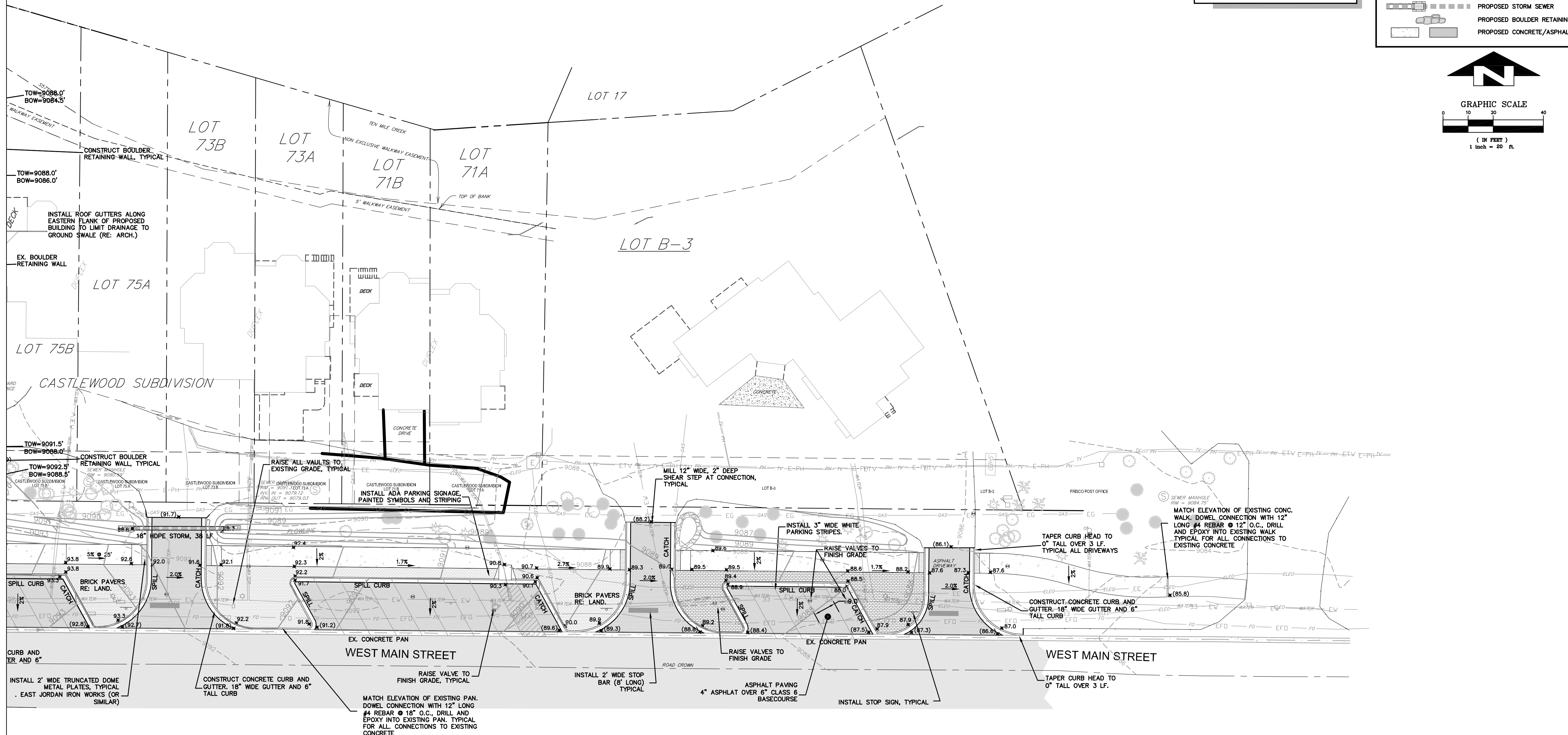
TOPOGRAPHIC INFORMATION WAS  
 PROVIDED BY RANGE WEST 2022

**LEGEND**

- PROPERTY LINE
- - - EXISTING CONTOUR
- - - EASEMENT
- - - PROPOSED CONTOUR
- 1.5% 48.6' PROPOSED GRADING, SLOPE/SPOT
- 1.5% 48.6' EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT

**GRAPHIC SCALE**

( IN FEET )  
 1 inch = 20 ft.

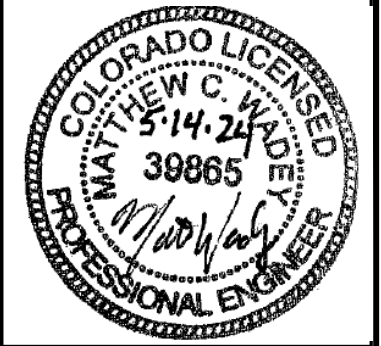


**101 WEST MAIN STREET**  
 FRISCO, CO  
**GRADING AND DRAINAGE**

SEE SHEET C2.0

NO.	DATE	REVISIONS	BY
01	05/11/2023	PRELIMINARY PLAN	MCW
02	12/12/2023	FINAL SITE PLAN	MCW
03	01/25/2024	RESPONSE TO COMMENTS	
04	05/14/2024	FINAL SITE SUBMITTAL	
05	05/31/2024	RESPONSE TO COMMENTS	

SHEET  
 C2.1



**101 WEST MAIN STREET  
 FRISCO, CO  
 DRAINAGE AREA MAP**

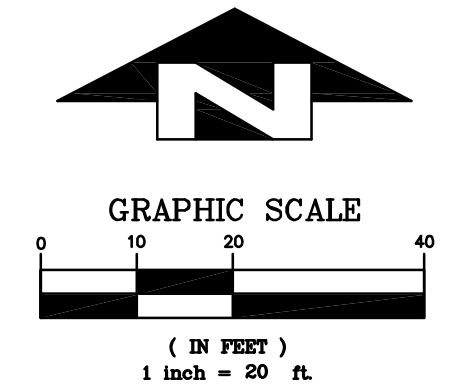
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	12/12/2023	FINAL SITE PLAN
	01/25/2024	RESPONSE TO COMMENTS
	05/14/2024	FINAL SITE SUBMITTAL
	05/31/2024	RESPONSE TO COMMENTS

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

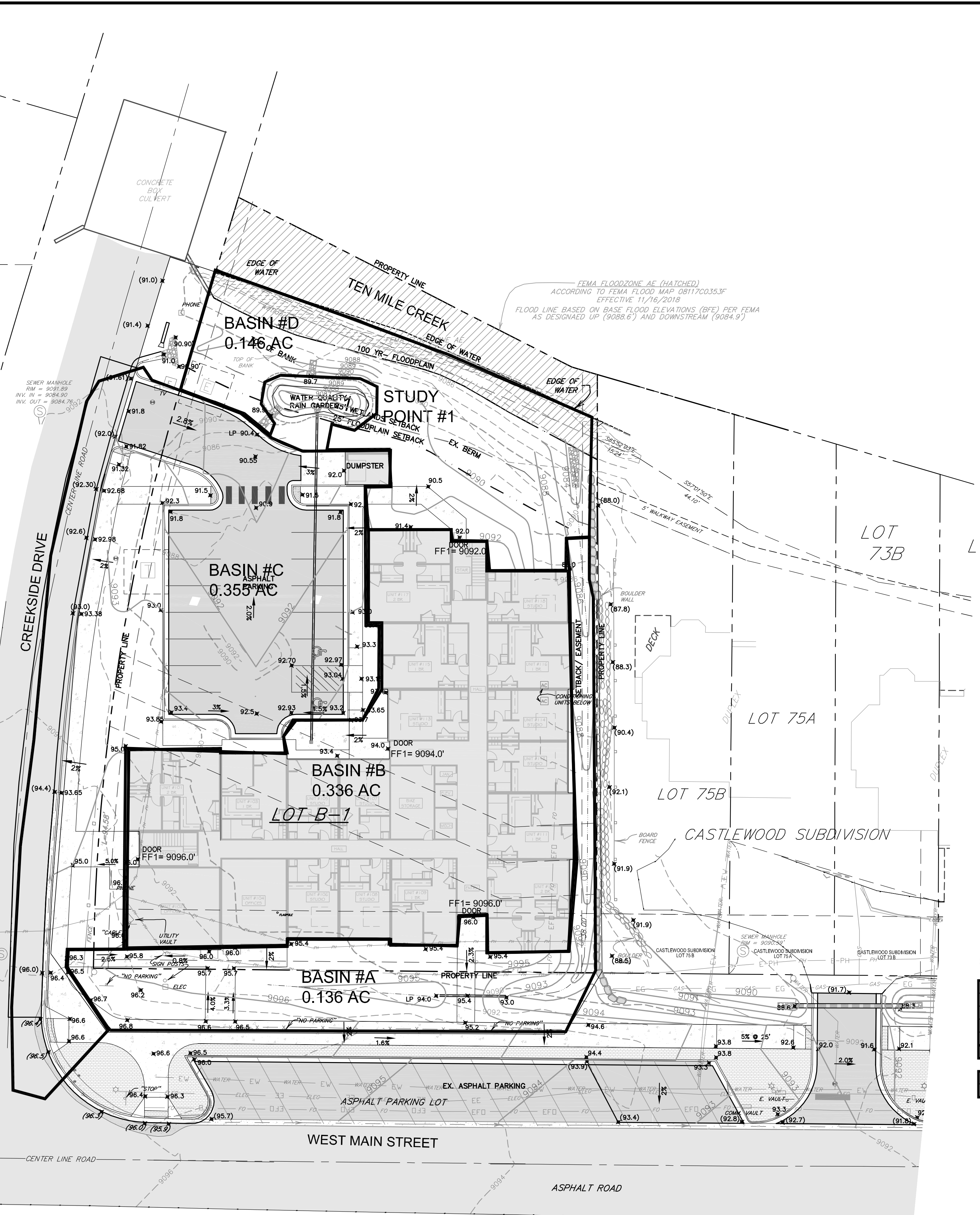
CONTRACTOR SHALL DEVELOP A MEANS AND METHODS TO CONTROL SEDIMENT DURING CONSTRUCTION TO ASSURE THAT NO SEDIMENT IS DISCHARGED OFF-SITE.

**LEGEND**

- PROPERTY LINE
- EXISTING CONTOUR
- EASEMENT
- PROPOSED CONTOUR
- PROPOSED GRADING, SLOPE/SPOT
- EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT



FEMA FLOODZONE AE (HATCHED) ACCORDING TO FEMA FLOOD MAP 0817030353F EFFECTIVE 11/16/2018  
 FLOOD LINE BASED ON BASE FLOOD ELEVATIONS (BFE) PER FEMA AS DESIGNATED UP (9088.6') AND DOWNSTREAM (9084.9')



**HISTORIC vs. DEVELOPED STORM RUNOFF**

STUDY POINT	HISTORIC		DEVELOPED	
	25 YEAR (cfs)	100 YEAR (cfs)	25 YEAR (cfs)	100 YEAR (cfs)
1	1.45	2.65	2.91	4.13

Total Area = 0.945 AC (historic)  
 = 0.973 AC (developed)  
 Impervious (Roofs/Paving) = 0.642 AC  
 Grass open space (good) = 0.331 AC

**101 West Main Street  
 Frisco, CO  
 Stormwater Runoff**

Basins	Area (Acre)	Area (sqft)	Area Impervious	Area Grass	RCN	TC (hours)	Q10 (cfs)	Q25 (cfs)	Q100 (cfs)
A	0.136	5,924	0.026	0.11	68	0.10	0.00	0.00	0.11
B	0.336	14,636	0.328	0.008	97	0.10	0.73	0.91	1.24
C	0.355	15,464	0.282	0.073	91	0.10	0.54	0.72	1.08
D	0.146	6,360	0.006	0.14	63	0.10	0.00	0.00	0.07
<b>Developed</b>	<b>0.97</b>						<b>1.27</b>	<b>1.63</b>	<b>2.50</b>

HISTORIC									
H	0.945	41,164	0	0.945	69	0.185	0.10	0.28	0.74

**TABLE 1  
 101 West Mains St, Frisco  
 DETENTION POND - DETENTION STORAGE VOLUME**

**POND**

Elevation	Area (ft <sup>2</sup> )	Avg. Area (ft <sup>2</sup> )	Depth (ft)	Volume (ft <sup>3</sup> )	Cumulative Volume (ft <sup>3</sup> )	Cumulative Volume (Ac-ft)	Cumulative Volume (Gallons)
9087	56	56	0	0	0	0.000	0
9088	199	128	1	128	128	0.003	954
9089	353	276	1	276	404	0.009	3,018
9089.7	518	436	0.7	305	708	0.016	5,298

TOTAL PROVIDED 708 cuf  
 TOTAL REQUIRED 488 cuf

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**101 WEST MAIN STREET  
 FRISCO, CO  
 STORM SEWER PLAN**

DESIGNED	MCW	BY	REVISIONS	NO.	DATE
DRAWN	MCW	MCW	PRELIMINARY PLAN		05/12/2023
CHECKED	MCW_TSL	MCW	FINAL SITE PLAN		12/12/2023
JOB NO.			RESPONSE TO COMMENTS		01/25/2024
DATE			FINAL SITE SUBMITTAL		05/14/2024
			RESPONSE TO COMMENTS		05/31/2024

**LEGEND**

- PROPERTY LINE
- EXISTING CONTOUR
- EASEMENT
- PROPOSED CONTOUR
- PROPOSED GRADING, SLOPE/SPOT
- EXISTING GRADING, SLOPE/SPOT
- PROPOSED STORM SEWER
- PROPOSED BOULDER RETAINING
- PROPOSED CONCRETE/ASPHALT

**GRAPHIC SCALE**  
 0 10 20 40  
 ( IN FEET )  
 1 inch = 20 ft.

GRADE TO DRAIN AWAY FROM BUILDING IN ALL AREAS

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

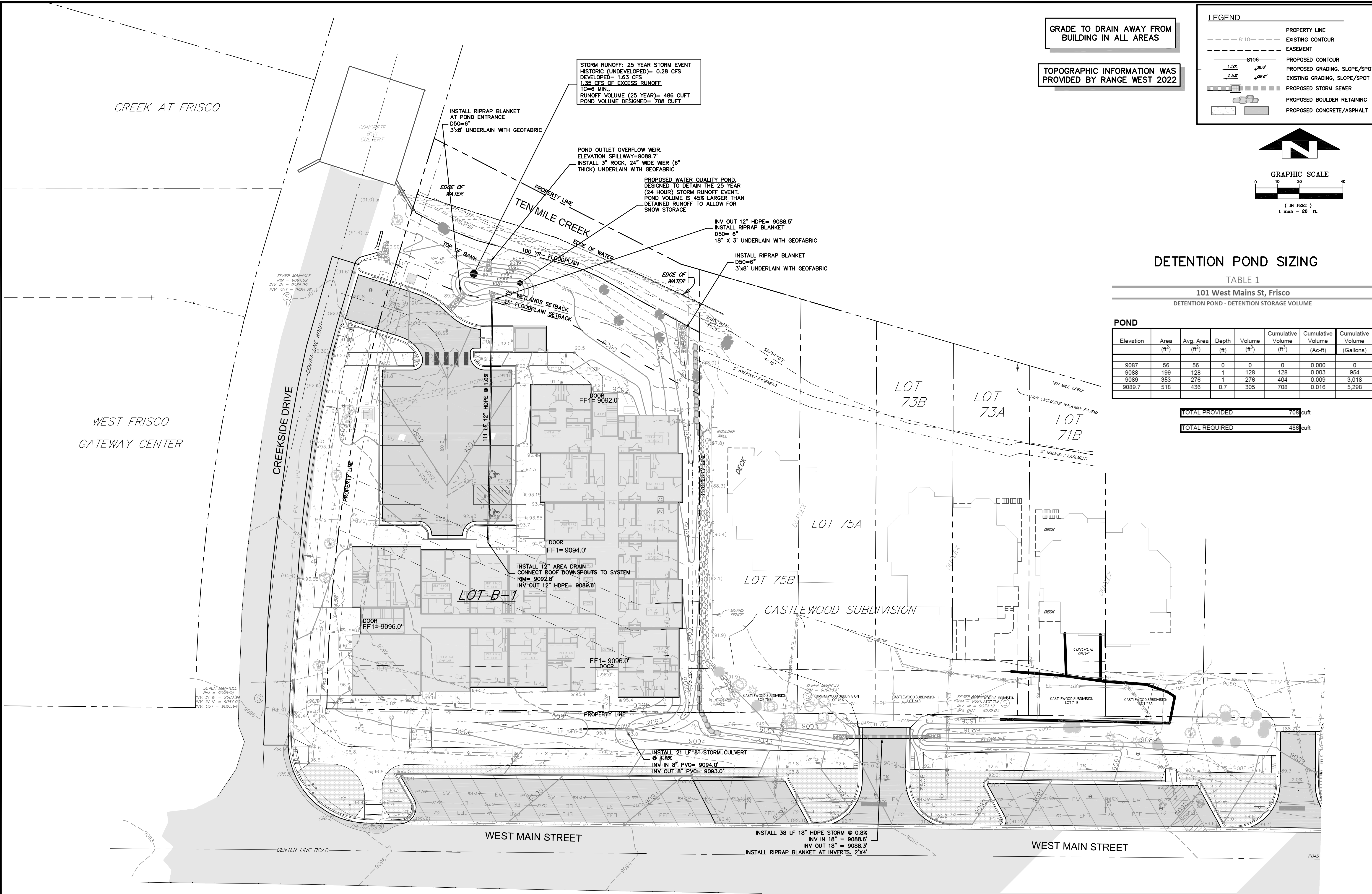
STORM RUNOFF: 25 YEAR STORM EVENT  
 HISTORIC (UNDEVELOPED) = 0.28 CFS  
 DEVELOPED = 1.63 CFS  
 1.35 CFS OF EXCESS RUNOFF  
 TC = 6 MIN.  
 RUNOFF VOLUME (25 YEAR) = 486 CUFT  
 POND VOLUME DESIGNED = 708 CUFT

**DETENTION POND SIZING**

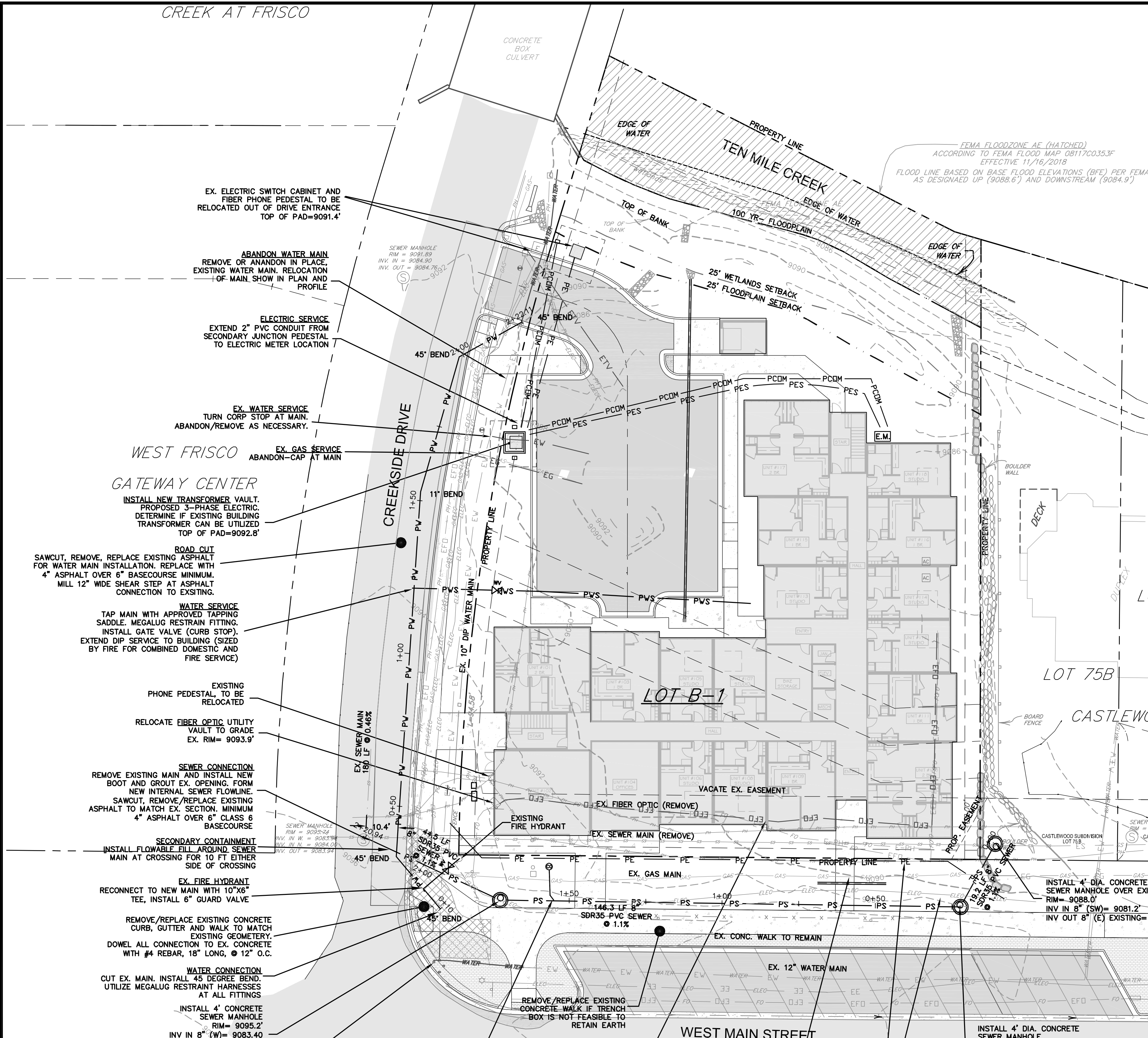
TABLE 1  
 101 West Mains St, Frisco  
 DETENTION POND - DETENTION STORAGE VOLUME

POND							
Elevation	Area (ft <sup>2</sup> )	Avg. Area (ft <sup>2</sup> )	Depth (ft)	Volume (ft <sup>3</sup> )	Cumulative Volume (ft <sup>3</sup> )	Cumulative Volume (Ac-ft)	Cumulative Volume (Gallons)
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9089.7	518	436	0.7	305	708	0.016	5,298

TOTAL PROVIDED = 708 cuft  
 TOTAL REQUIRED = 486 cuft

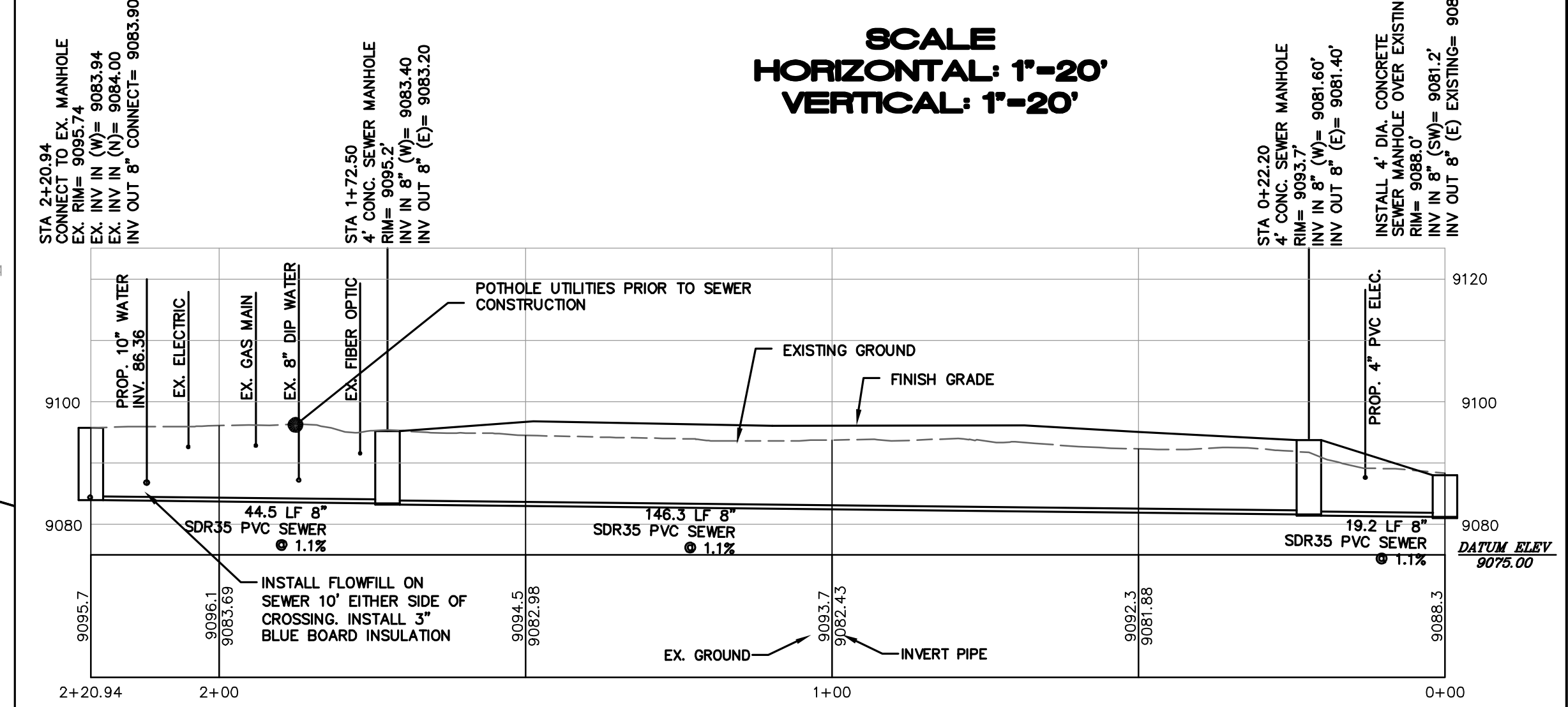


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SEWER PROFILE

SCALE HORIZONTAL: 1"=20' VERTICAL: 1"=20'



101 WEST MAIN STREET  
FRISCO, CO  
UTILITY PLAN

TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

REFER TO TOWN OF FRISCO WATER CONSTRUCTION STANDARDS

ALL WATER MAIN FITTINGS SHALL HAVE MEGALUG RESTRAINED JOINTS

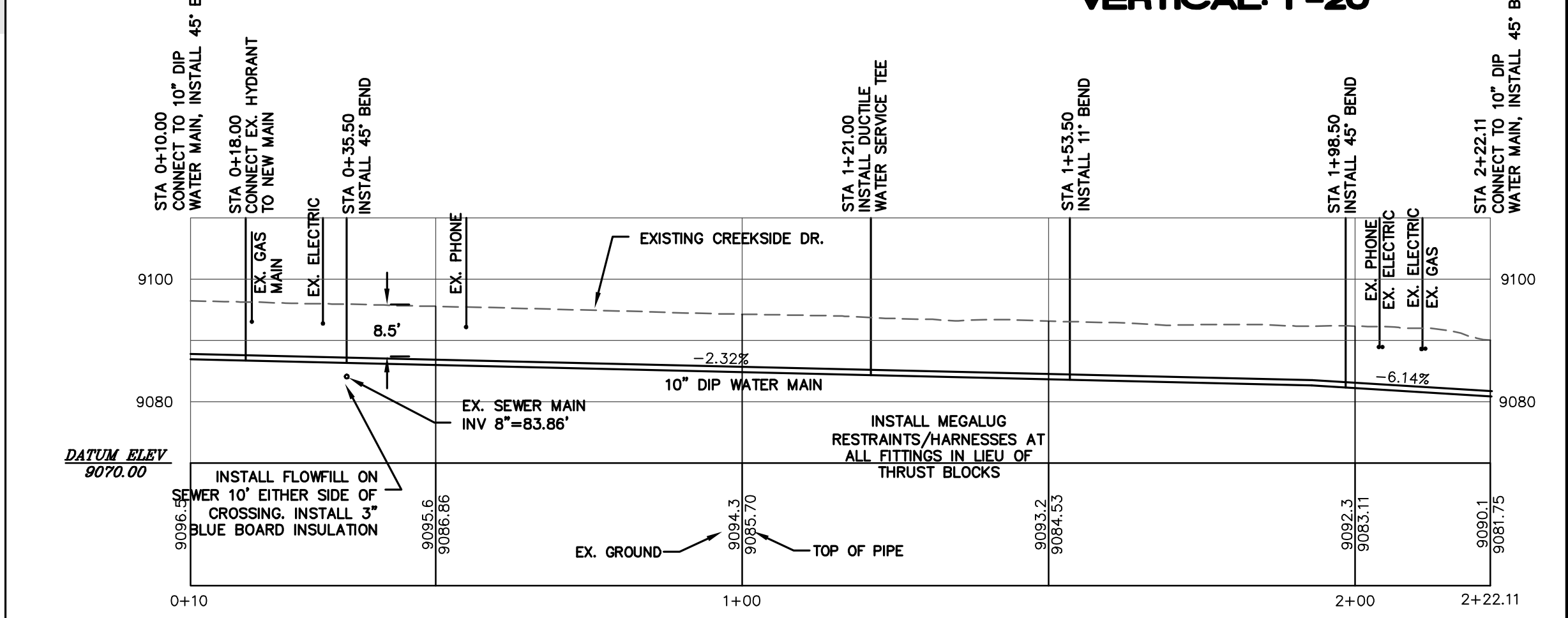
MAINTAIN 8.5' BURY ON ALL WATER MAINS AND SERVICES

REFER TO FRISCO SANITATION DISTRICT SEWER CONSTRUCTION STANDARDS

SEWER MAIN/SERVICES. MAINTAIN 7.0 FT COVER IN UNPLOWED AREAS AND 9.0 FT COVER IN SNOW PLOWED AREAS. INSTALL INSULATION IF THESE DEPTHS ARE NOT ACHIEVED.

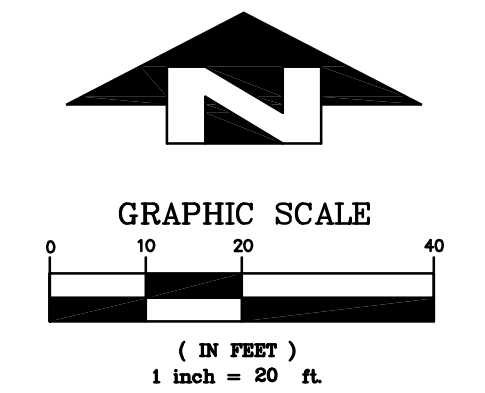
WATER PROFILE

SCALE HORIZONTAL: 1"=20' VERTICAL: 1"=20'



LEGEND

PE	PROPOSED ELECTRIC
PES	PROPOSED ELECTRIC SERVICE
PCDM	PROPOSED COMM. SERVICE TV/PHONE
PG	PROPOSED GAS
PGS	PROPOSED GAS SERVICE
PW	PROPOSED WATER
PS	PROPOSED SEWER
PVS	PROPOSED WATER SERVICE
PSS	PROPOSED SEWER SERVICE
PV	PROPOSED FIRE HYDRANT W/GV
PSW	PROPOSED STORM SEWER
PT	PROPOSED TRANSFORMER & SECONDARY PEDESTALS
PCP	PROPOSED COMM. PEDS
PL	PROPERTY LINE



NO.	DATE	REVISIONS	BY
05/12/2023	PRELIMINARY PLAN	MCW	
12/12/2023	FINAL SITE PLAN	MCW	
01/25/2024	RESPONSE TO COMMENTS	MCW	
05/14/2024	FINAL SITE SUBMITTAL		
05/31/2024	RESPONSE TO COMMENTS		

DESIGNED MCW  
DRAWN MCW  
CHECKED MCW\_TSL  
JOB NO. ---  
DATE 05/11/2023

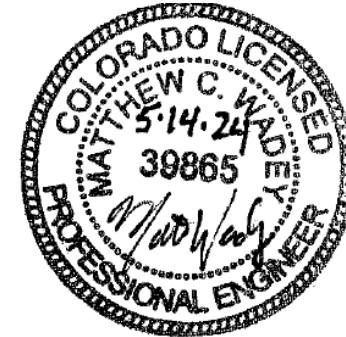
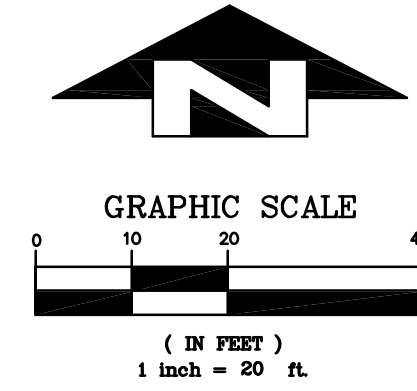
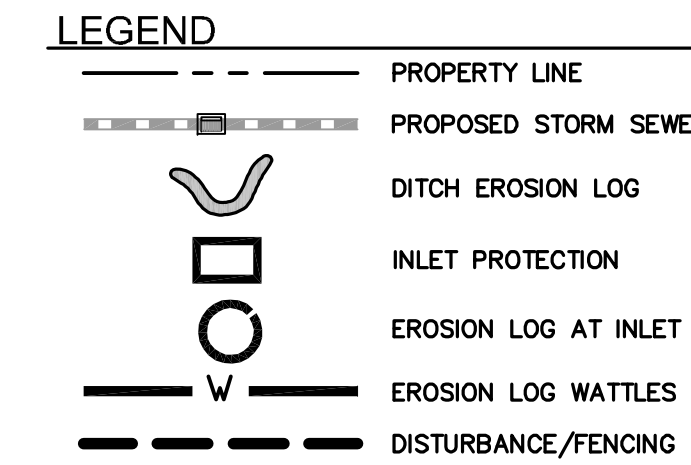
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TOPOGRAPHIC INFORMATION WAS PROVIDED BY RANGE WEST 2022

CONTRACTOR SHALL DEVELOP A MEANS AND METHODS TO CONTROL SEDIMENT DURING CONSTRUCTION TO ASSURE THAT NO SEDIMENT IS DISCHARGED OFF-SITE.



**101 WEST MAIN STREET  
FRISCO, CO**  
EROSION CONTROL PLAN

**GENERAL NOTES FOR SEDIMENT CONTROL**

- CONTRACTOR SHALL SUBMIT A CONSTRUCTION STAGING & MANAGEMENT PLAN IDENTIFYING CONSTRUCTION FENCING, STAGING, STORAGE & CONSTRUCTION TRAILER LOCATION PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES.
- INSTALL AND MAINTAIN SEDIMENT CONTROL MEASURES IN ACCORDANCE WITH THESE PLANS AND AS NEEDED TO PREVENT SEDIMENT FROM DISCHARGING OFF-SITE OR ENTERING THE RIVER.
- ALL PROPOSED SEDIMENT CONTROL MEASURES ARE TEMPORARY MEASURES UNLESS SPECIFIED OTHERWISE ON PLANS.
- SEDIMENT CONTROL MEASURES MAY REQUIRE FIELD ADJUSTMENTS AT THE TIME OF CONSTRUCTION TO INSURE THAT THEIR INTENDED PURPOSE IS ACCOMPLISHED.
- PROVIDE REGULAR INSPECTION AND MAINTENANCE OF ALL SEDIMENT CONTROL MEASURES TO INSURE THAT SEDIMENT CONTROL EFFICIENCY IS OBTAINED UNTIL FINAL STABILIZATION OF SITE HAS TAKEN PLACE.
- INSTALL SEDIMENT CONTROL MEASURES AT THE ONSET OF GRADING OPERATIONS SO THAT EFFECTIVE SEDIMENT CONTROL CAN BE ACHIEVED DURING THE ENTIRE CONSTRUCTION PERIOD.
- STABILIZE ALL POINTS OF INGRESS AND EGRESS WITH TRACKING PAD DURING CONSTRUCTION TO PREVENT TRACKING OF MUD ONTO PUBLIC WAYS.
- FOR TEMPORARY STOCKPILES APPLY SEED, HYDROMULCH AND TACKIFIER IMMEDIATELY AFTER THEY ARE CONSTRUCTED FOR STABILIZATION. IF EROSION OCCURS AFTER APPLICATION OF THE TACKIFIER, USE EXCLESIOR C2 EROSION CONTROL FABRIC. INSTALL SILT FENCE BELOW STOCKPILES TO CAPTURE SEDIMENT.
- THE TERM "REVEGETATION" ON THIS PLAN MEANS THE SUCCESSFUL GERMINATION AND ESTABLISHMENT OF STABLE GRASS COVER FROM A PROPERLY PREPARED SEEDBED CONTAINING THE SPECIFIED AMOUNTS OF FERTILIZER IN ACCORDANCE WITH APPLICABLE STANDARDS AND SPECIFICATIONS. REFER TO LANDSCAPE PLANS FOR SEED MIX, FERTILIZER TYPE, MULCH, TACKIFIER AND APPLICATION RATES.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE APPROPRIATE MEASURES TO INSURE THAT NO SEDIMENT LADEN WATER IS DISCHARGED FROM THE SITE.
- APPROVAL SHALL BE REQUESTED UPON FINAL STABILIZATION OF ALL SITES BEFORE REMOVAL OF SEDIMENT CONTROLS.
- CONTRACTOR SHALL OBTAIN AND CONFORM TO STORMWATER DISCHARGE PERMIT AND ALL ENVIRONMENTAL PERMITS AND KEEP STREETS CLEAN AND FREE OF SEDIMENT.
- REMOVAL AND CLEANUP OF ANY SEDIMENT THAT LEAVES THE SITE IS THE RESPONSIBILITY OF THE CONTRACTOR

**CONSTRUCTION SEQUENCE OF EROSION/SEDIMENT CONTROL MEASURES**

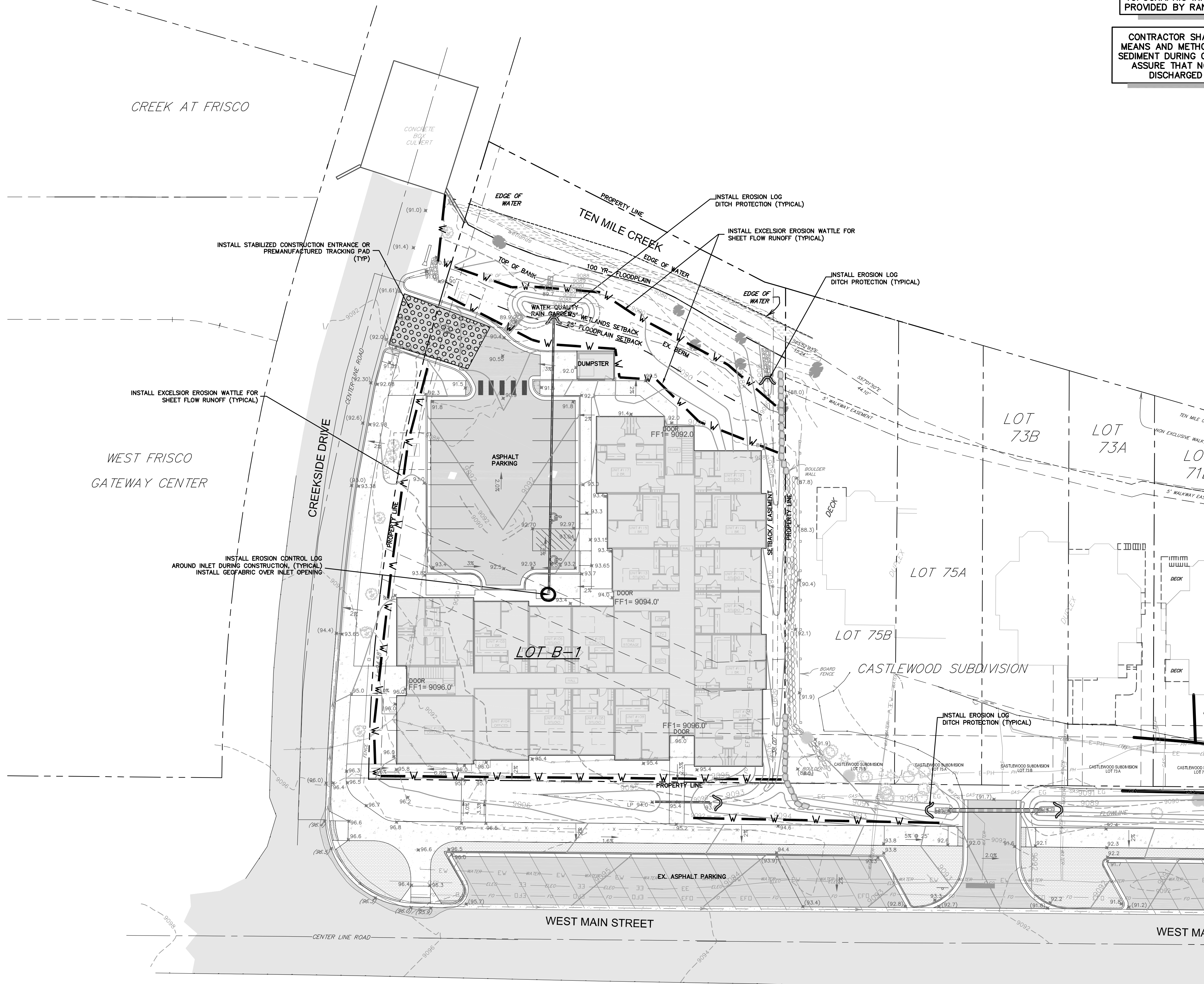
BEFORE COMMENCING GRADING OR CONSTRUCTION

- CONSTRUCT STABILIZED CONSTRUCTION ENTRANCES AT ALL POINTS OF INGRESS AND EGRESS.
- CONTRACTOR SHALL TAKE APPROPRIATE MEASURES TO ASSURE THAT NO SEDIMENT LEAVES THE SITE.
- CONSTRUCT SILT FENCE AND WATTLES AND ALL SEDIMENT CONTROL DEVICES.
- BEGIN DEMOLITION, EXCAVATION AND CONSTRUCTION.
- INSTALL EROSION CONTROL MEASURES AFTER DITCHES AND SWALES HAVE BEEN CONSTRUCTED AND TOPSOIL AND SEED HAVE BEEN PLACED. INSTALL INLET PROTECTION IN ALL INLETS AS THEY ARE CONSTRUCTED.
- TOPSOIL AND REVEGETATE ALL DISTURBED AREAS WITH APPROVED SEED MIX PER LANDSCAPE PLAN.
- CONTRACTOR SHALL REMOVE SEDIMENT CONTROL FACILITIES AFTER FINAL STABILIZATION.

**FUGITIVE DUST CONTROL**

THE CONTRACTOR IS RESPONSIBLE TO CONTROL FUGITIVE DUST AND TO INCORPORATE THE FOLLOWING:

- ALL UNPAVED ROADS AND OTHER DISTURBED AREAS ON SITE SHALL BE WATERED TO MINIMIZE FUGITIVE DUST.
- HAUL ROADS SHALL BE TREATED WITH MAGNESIUM CHLORIDE IF WATER IS NOT CONTROLLING THE DUST.
- ALL DISTURBED SURFACE AREAS SHALL BE REVEGETATED OR SURFACED PER THE LANDSCAPE PLAN AS SOON AS POSSIBLE.
- MUD AND DIRT CARRYOUT ONTO PAVED SURFACES SHALL BE PREVENTED. ANY MUD AND DIRT CARRYOUT ONTO PAVED SURFACES SHALL BE CLEANED UP DAILY.



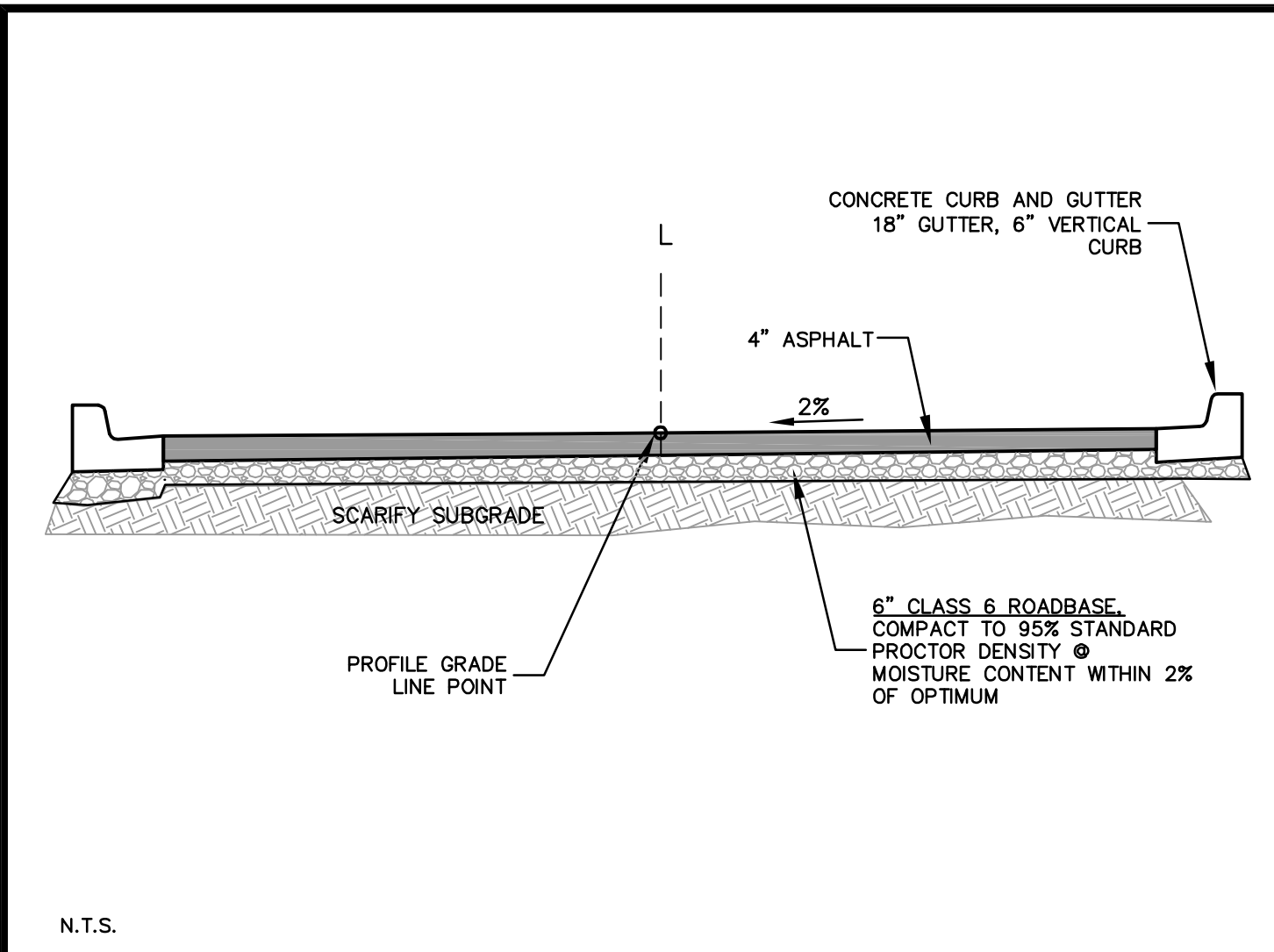
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MCW	05/12/2023		PRELIMINARY PLAN
MCW	12/12/2023		FINAL SITE PLAN
MCW, TSL	01/25/2024		RESPONSE TO COMMENTS
MCW, TSL	05/14/2024		FINAL SITE SUBMITTAL
MCW, TSL	05/31/2024		RESPONSE TO COMMENTS

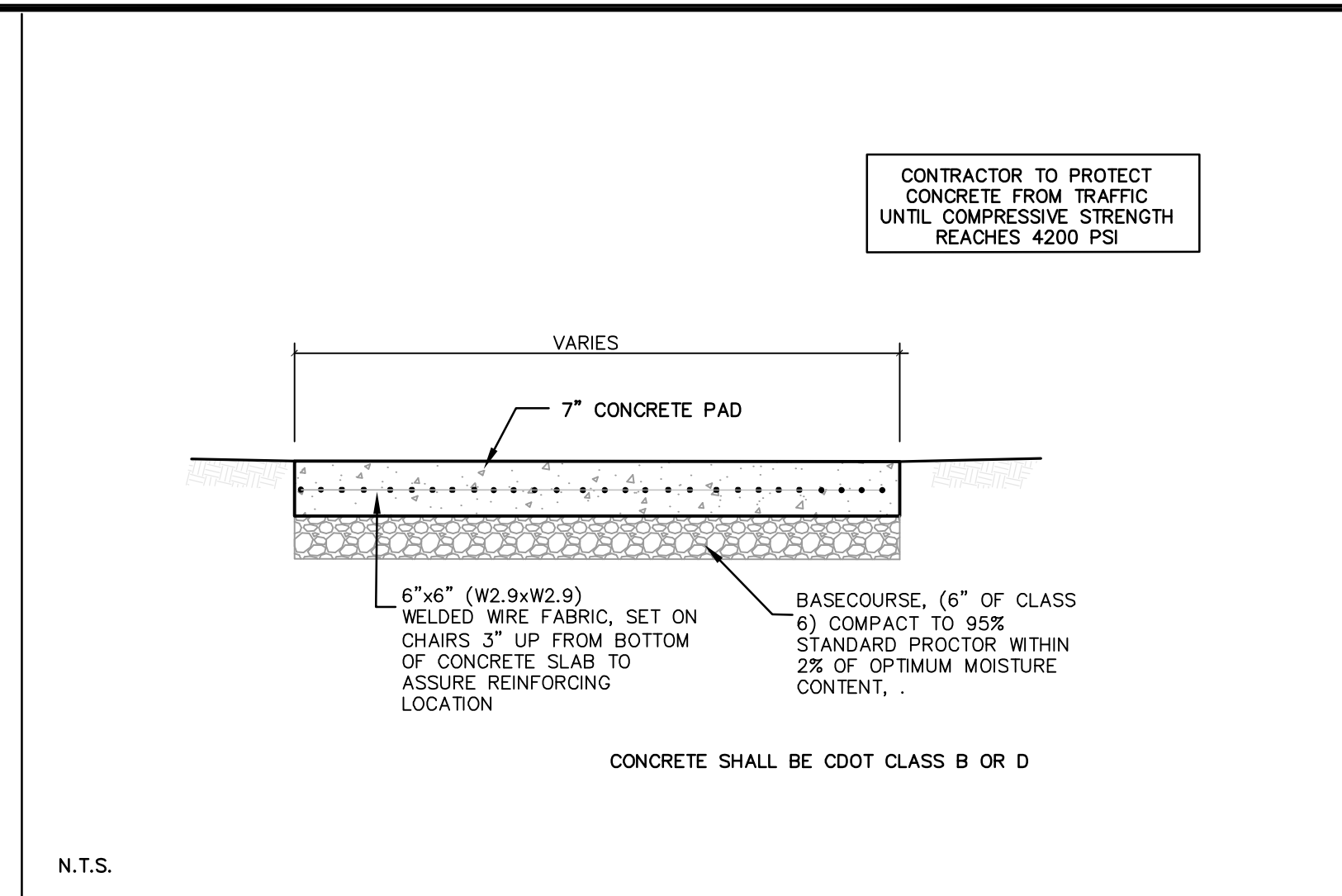
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MCW	05/11/2023	



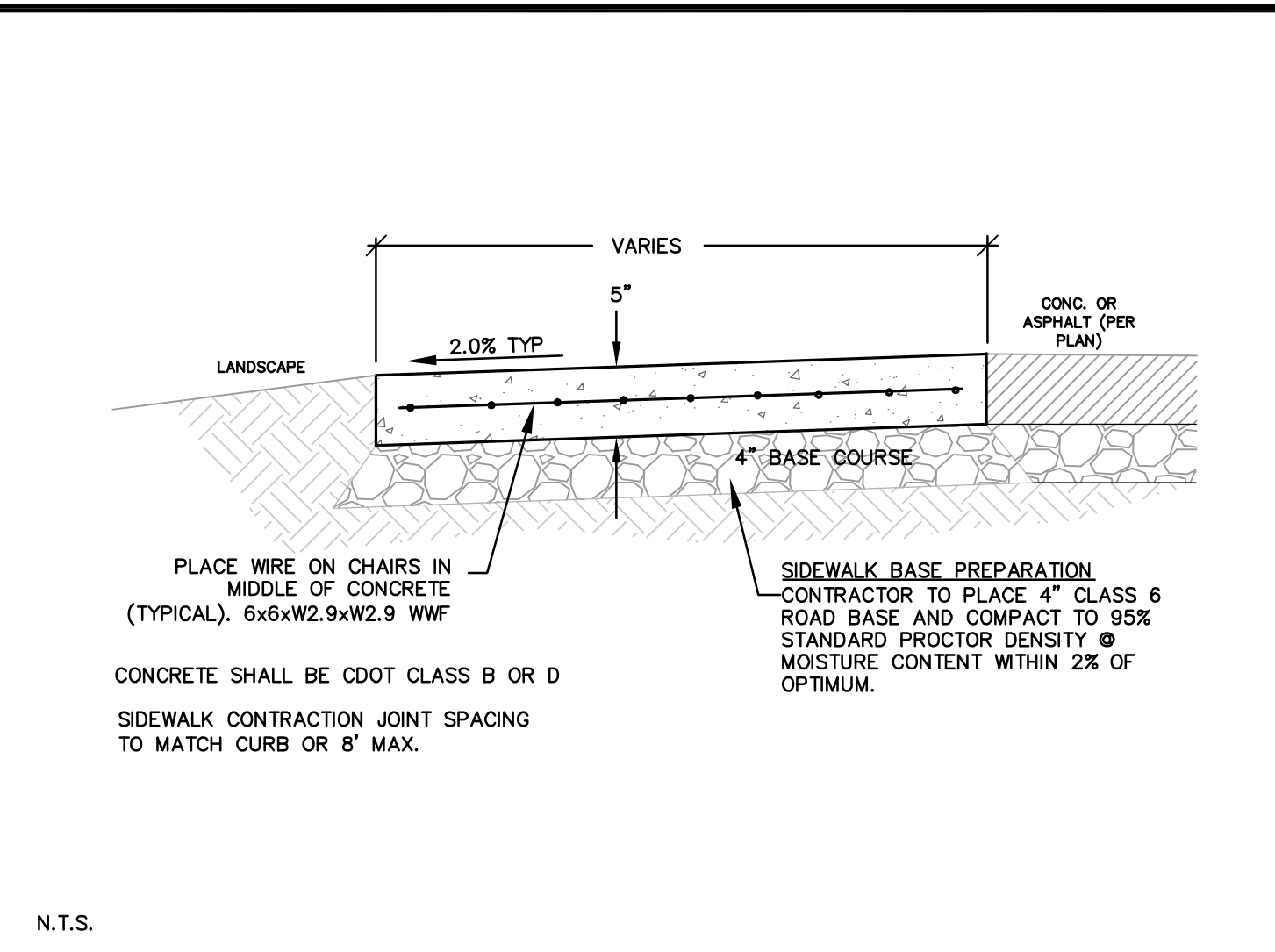
101 WEST MAIN STREET  
 FRISCO, CO  
 DETAILS



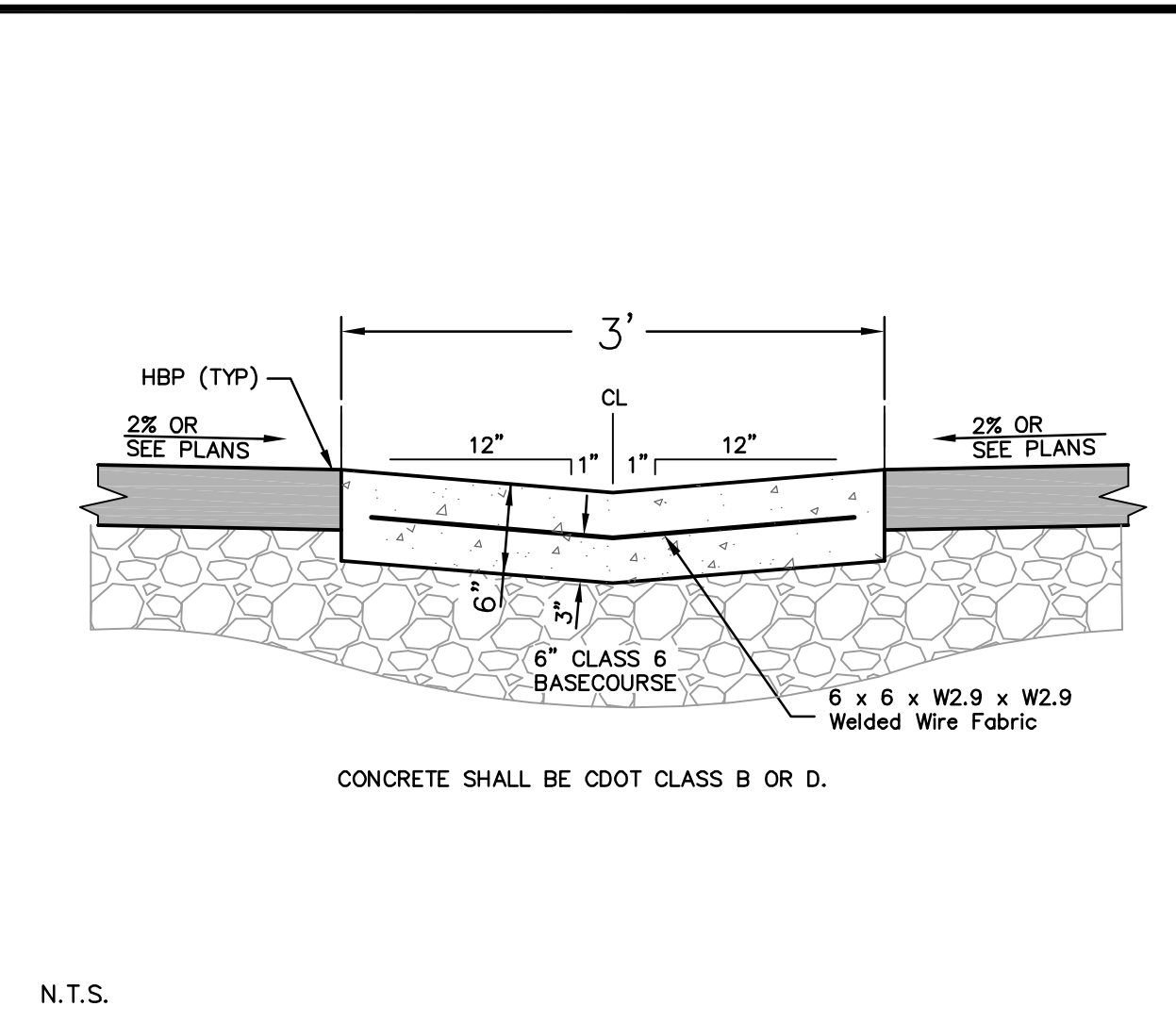
A ASPHALT SECTION



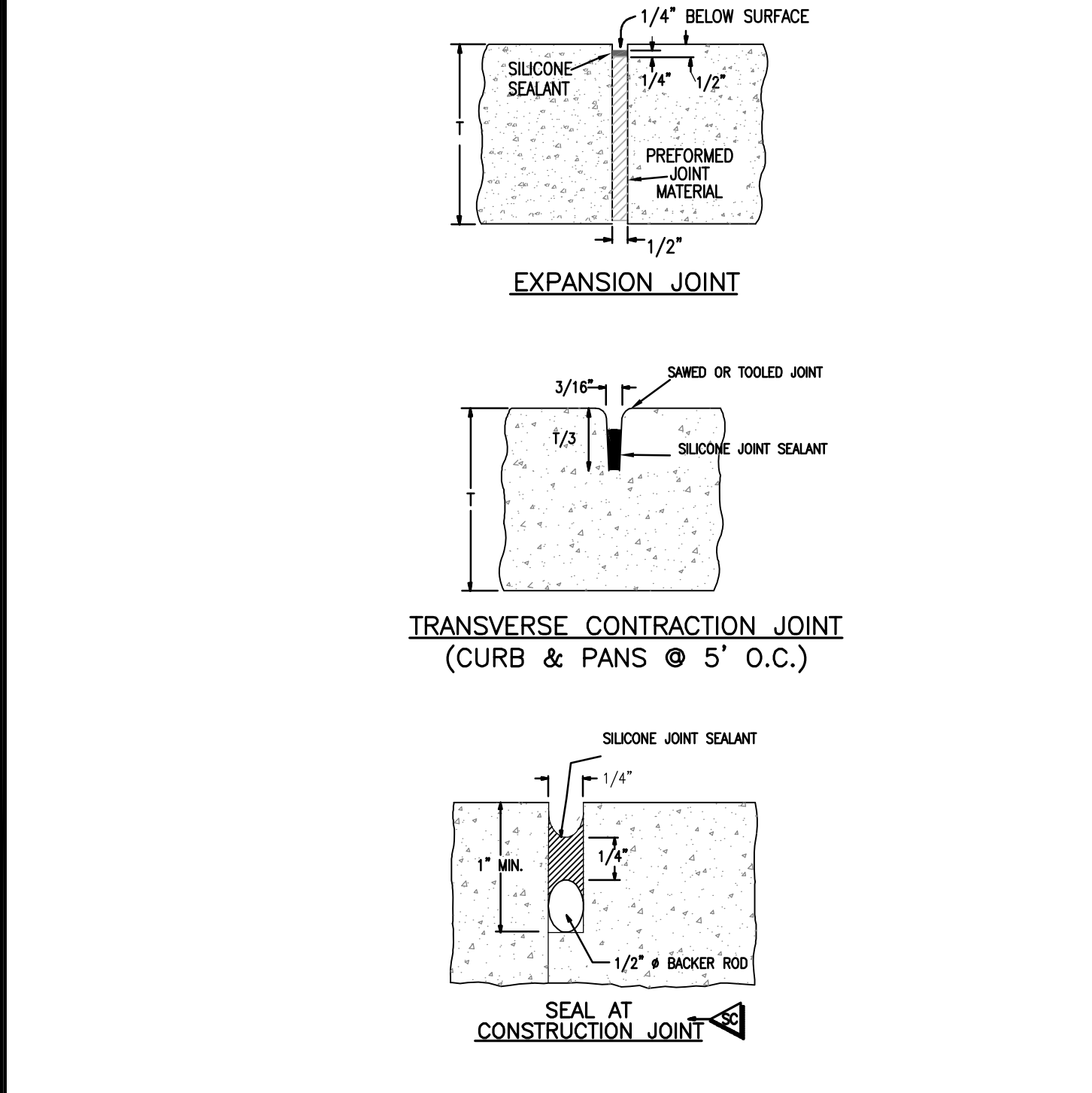
B CONCRETE DUMPSTER PAD SECTION



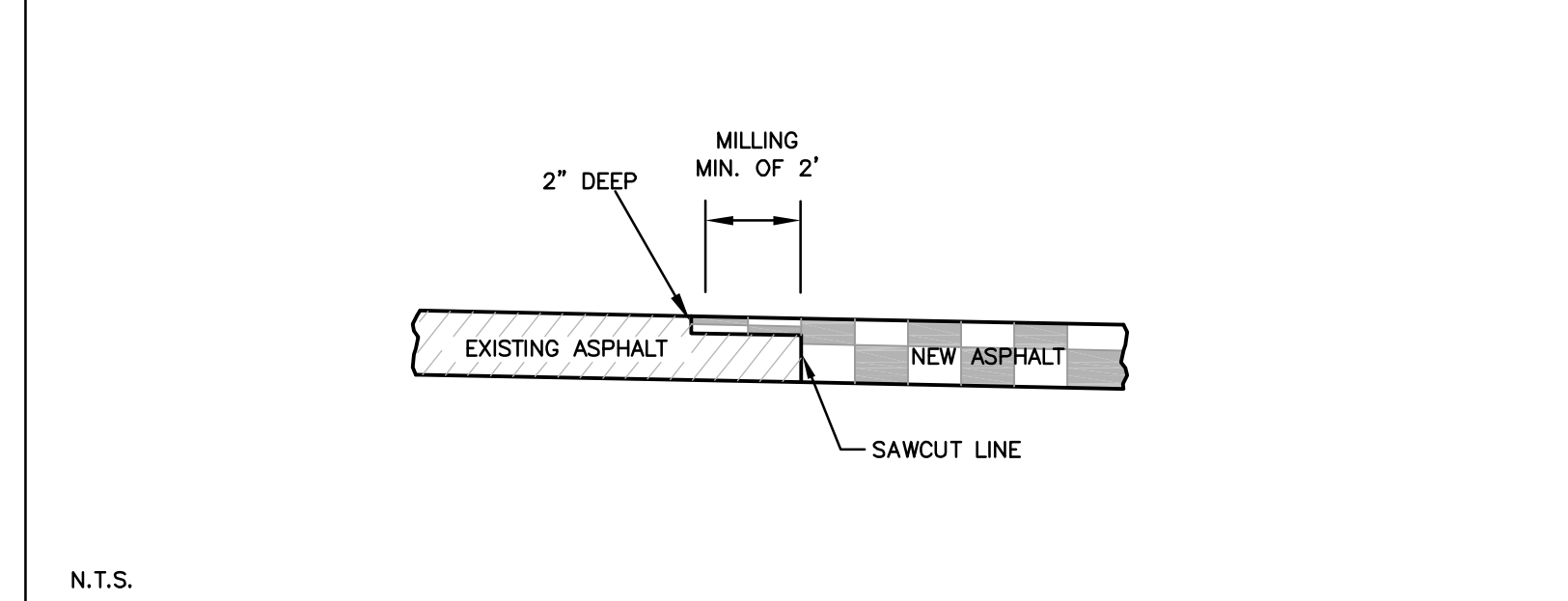
C CONCRETE SIDEWALK SECTION



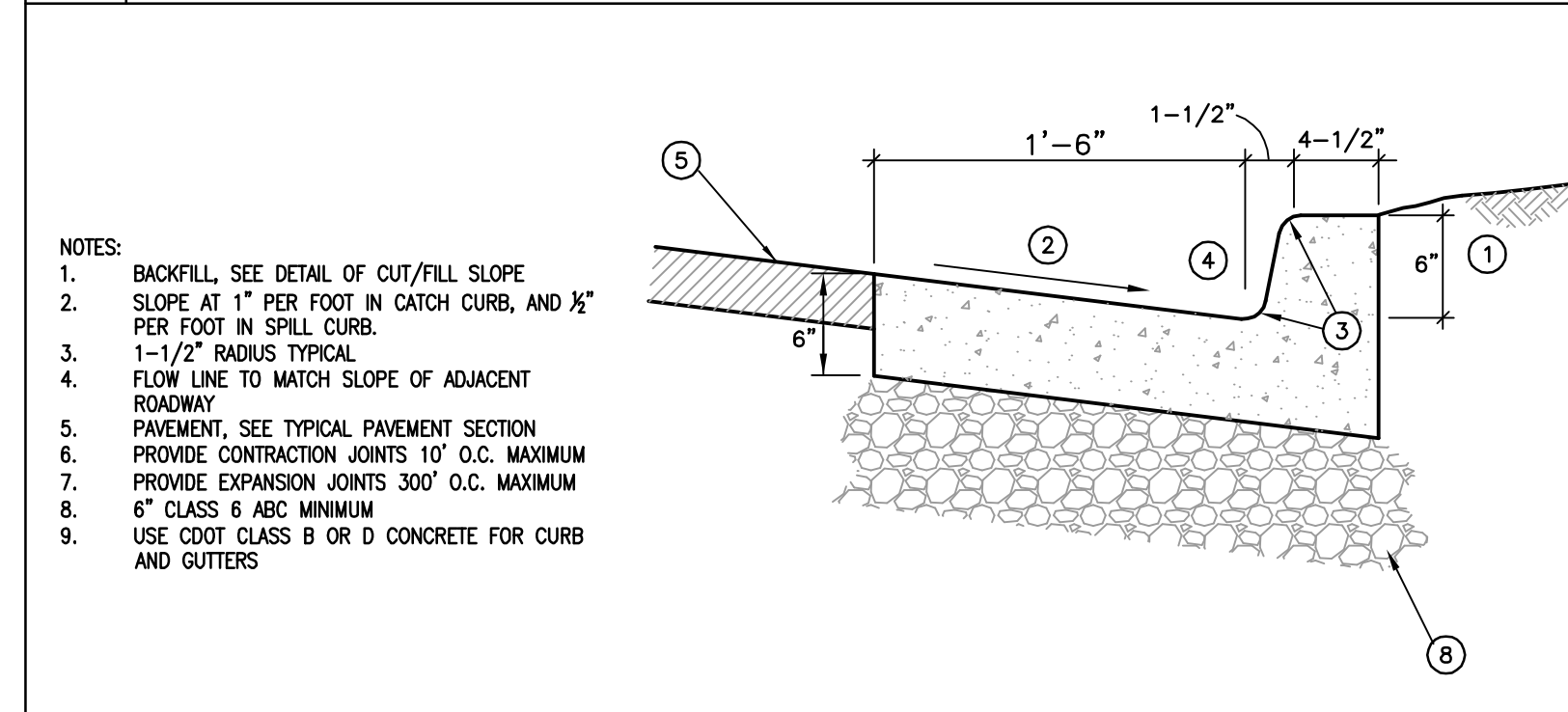
D CONCRETE PAN



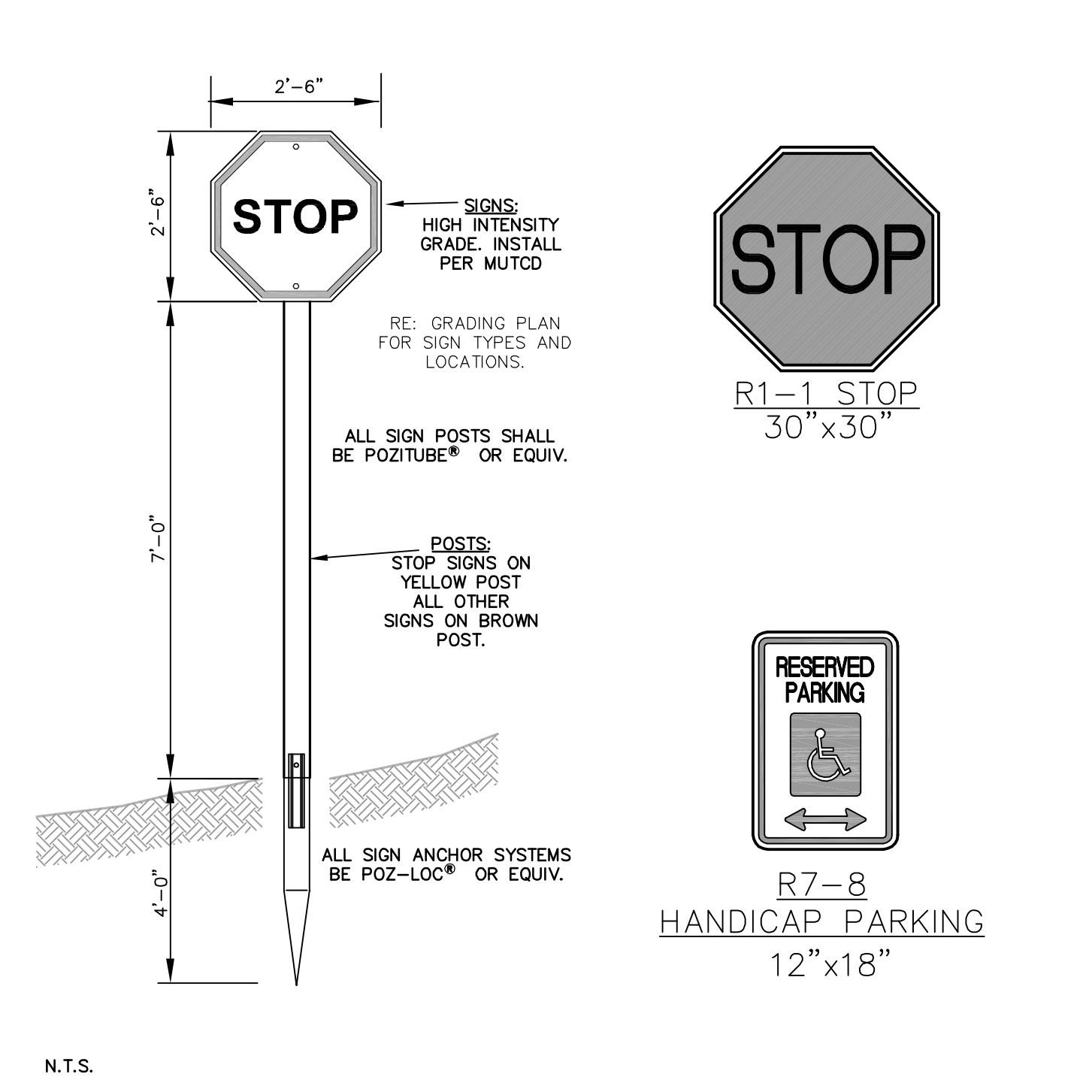
E CONCRETE JOINTING



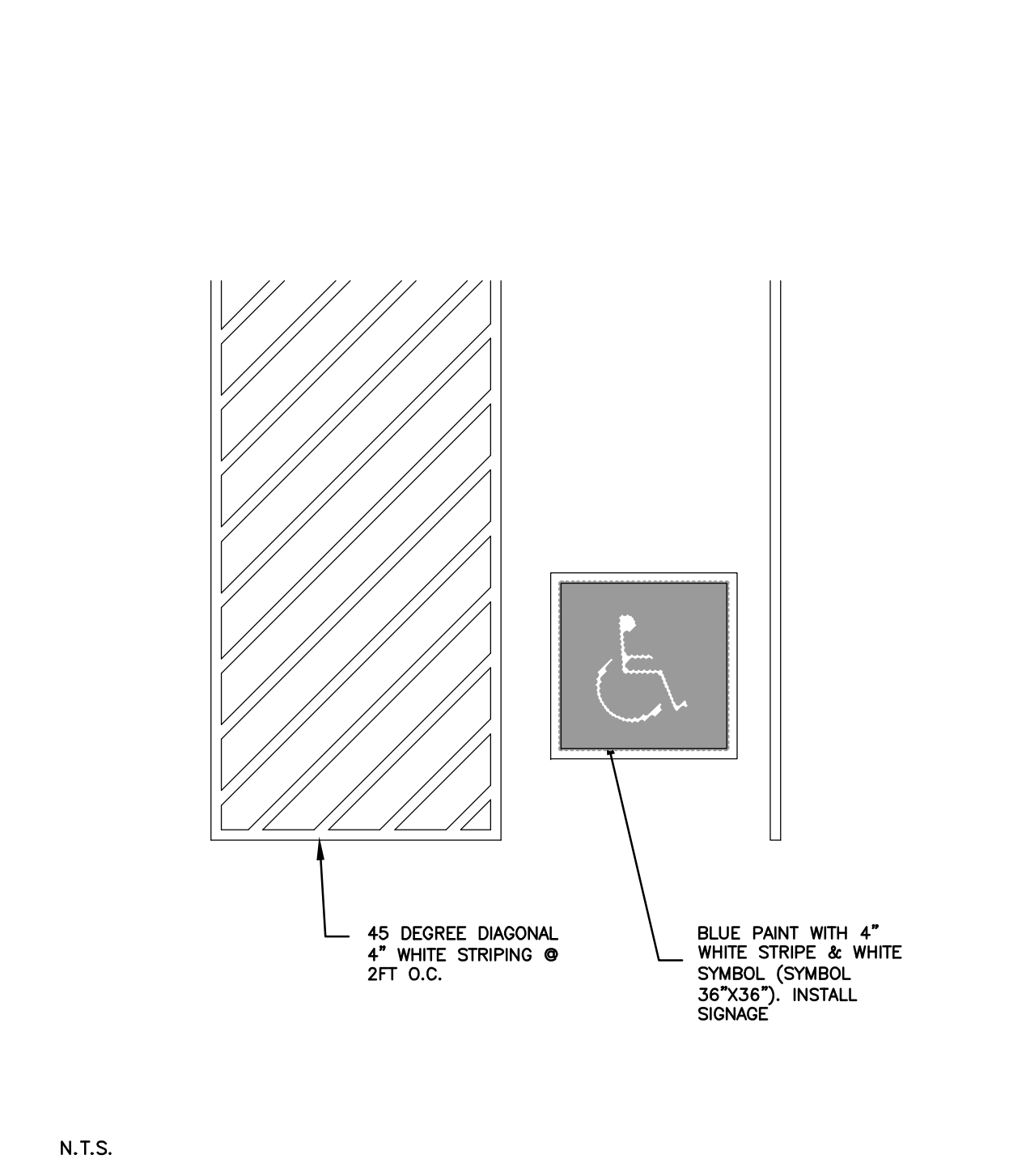
F SHEAR STEP



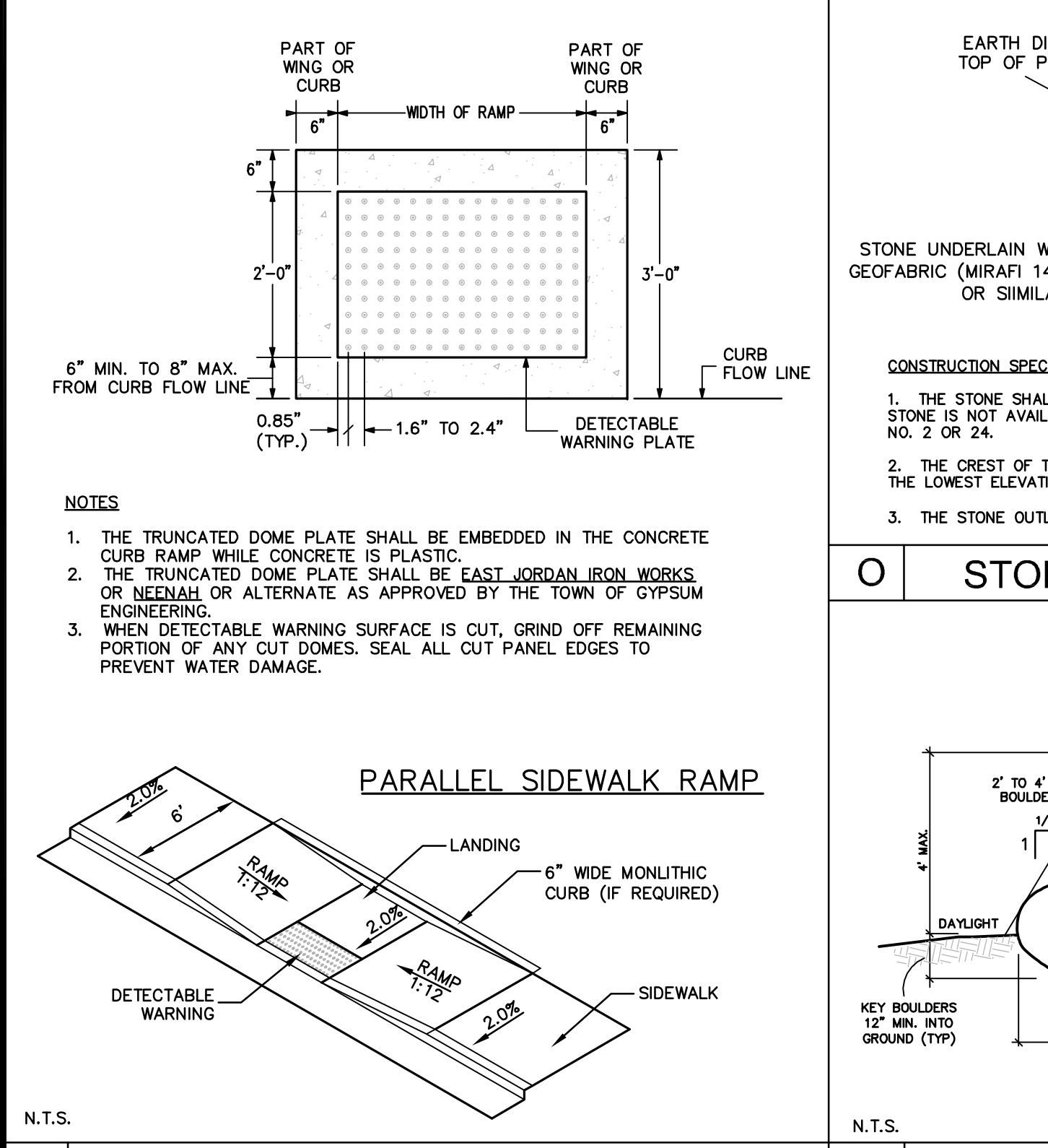
G CURB & GUTTER



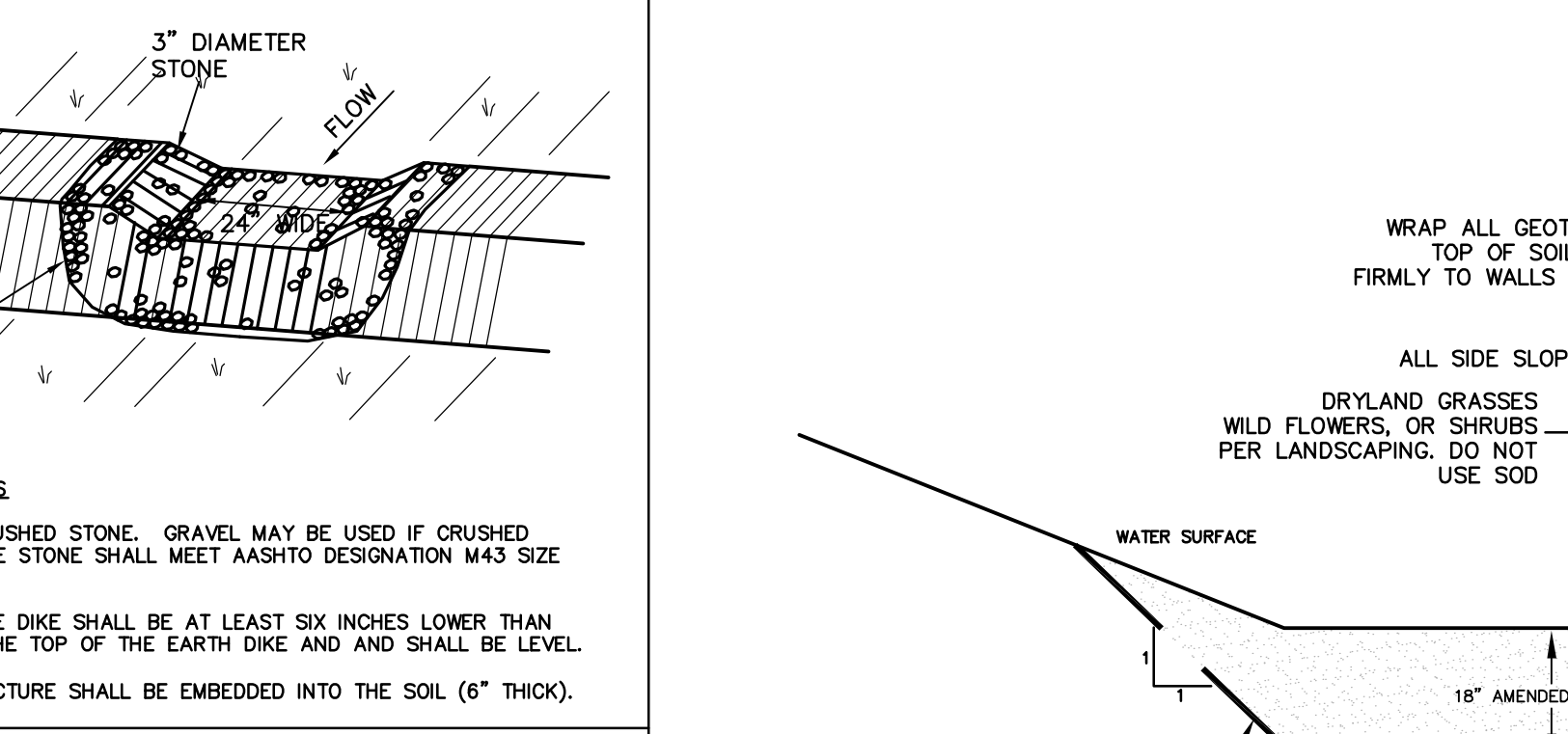
H SIGNAGE



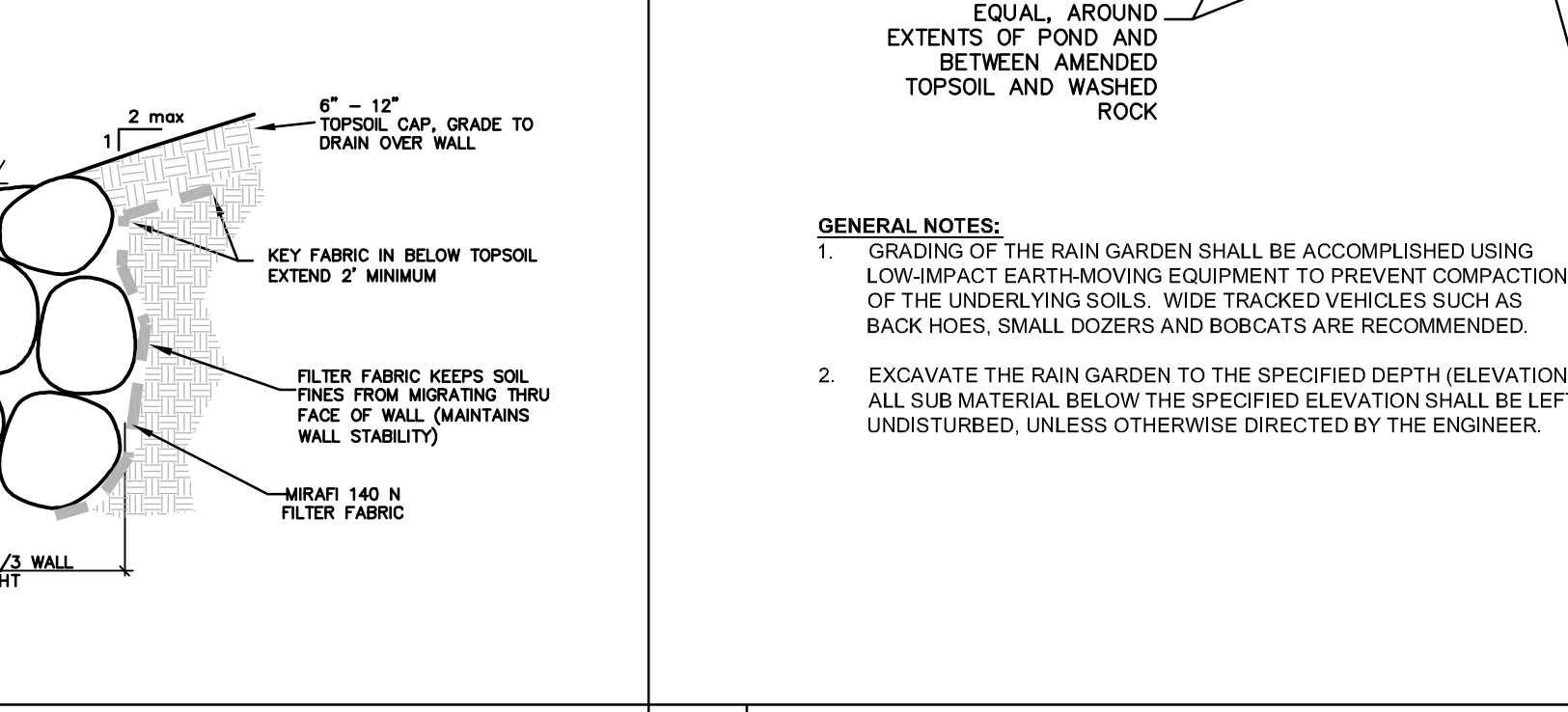
I HANDICAP PARKING



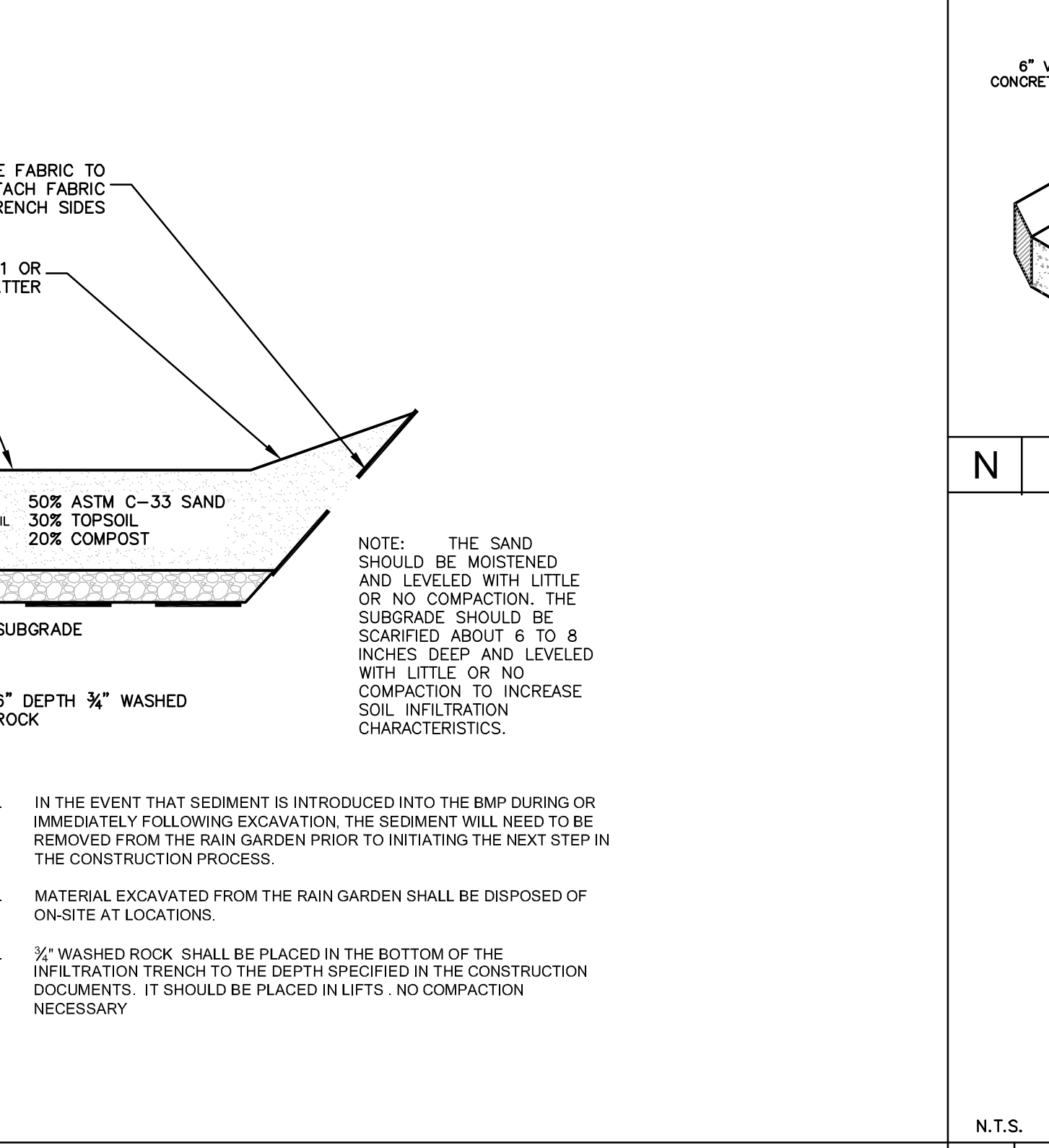
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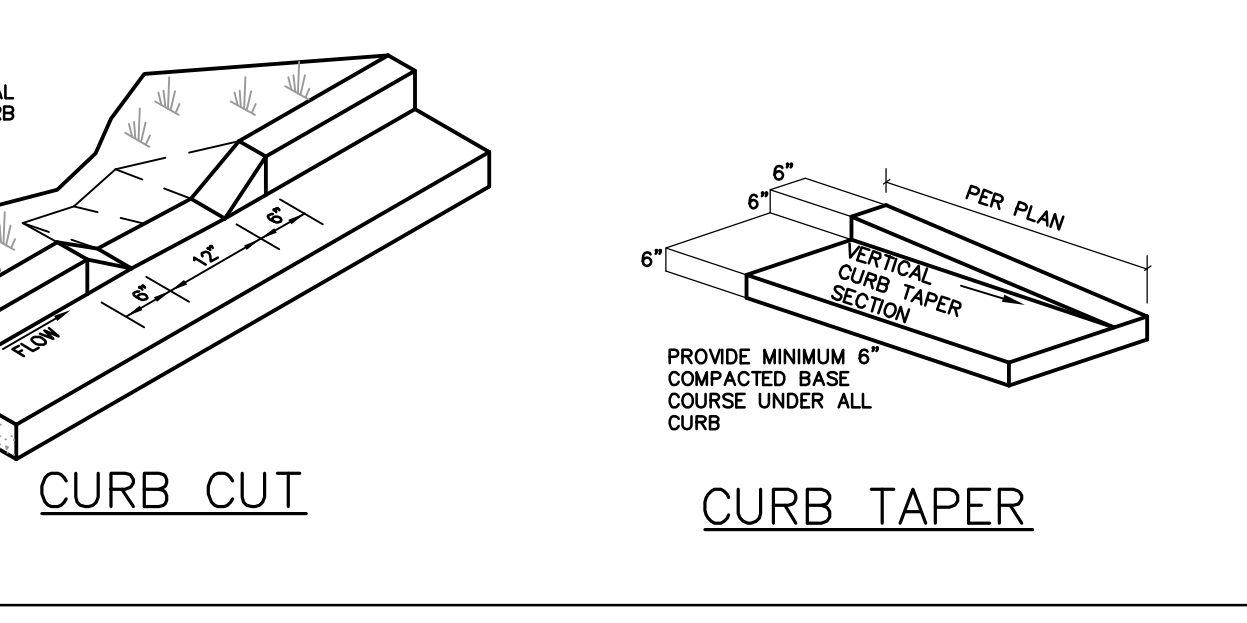
K BOLDER WALL



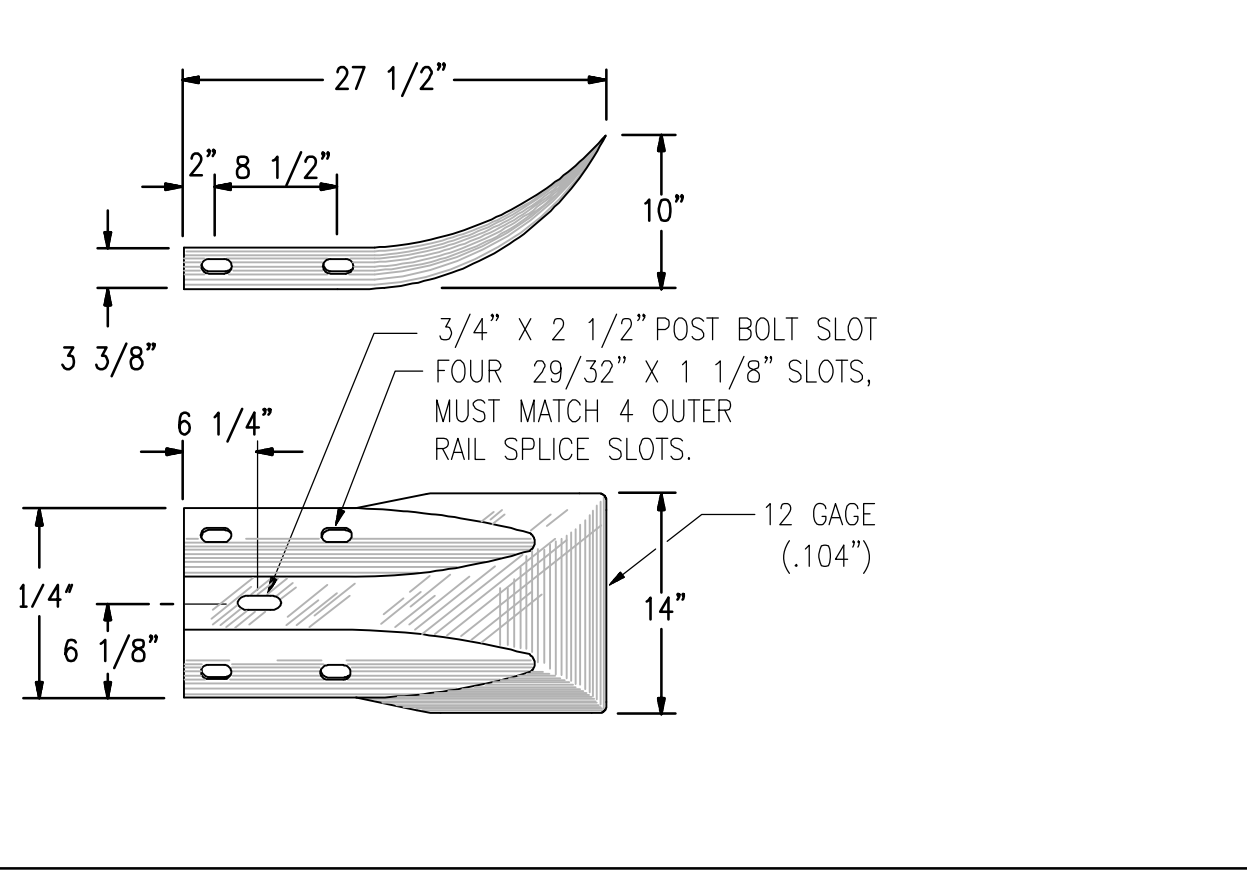
L POND SECTION (RAIN GARDEN)



M GUARDRAIL FLARED END SECTION



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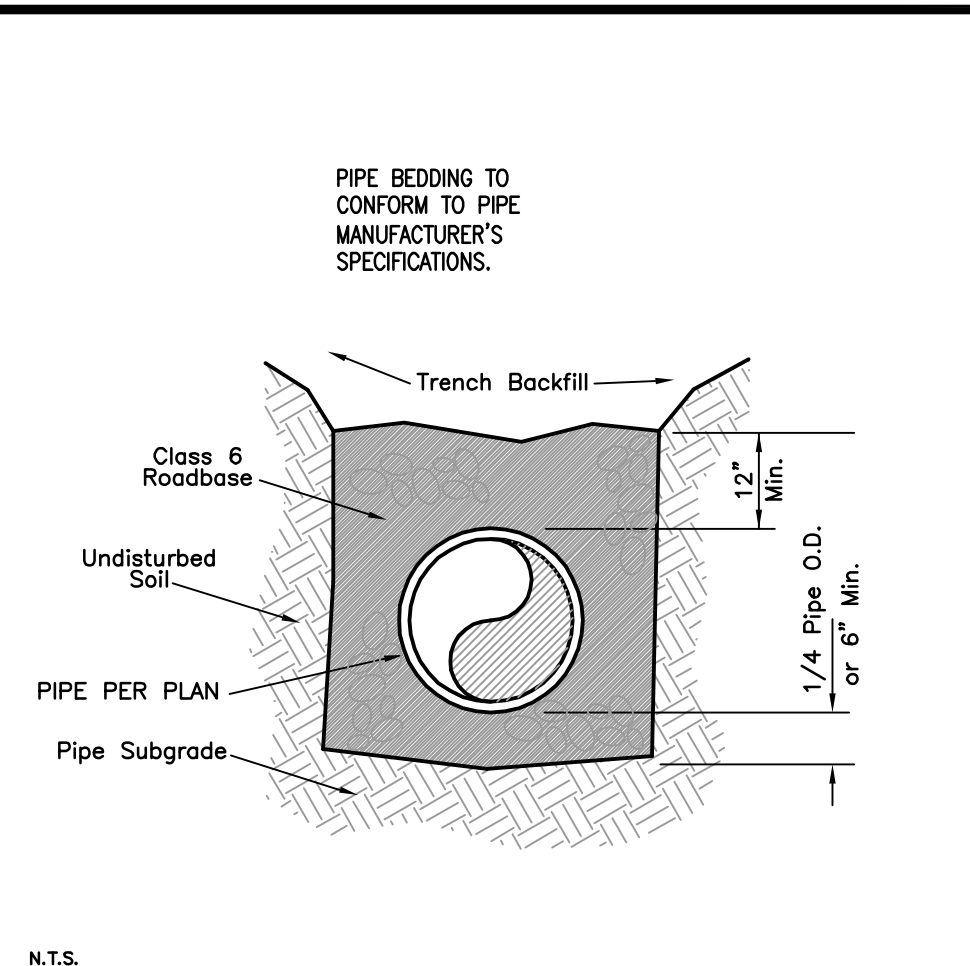


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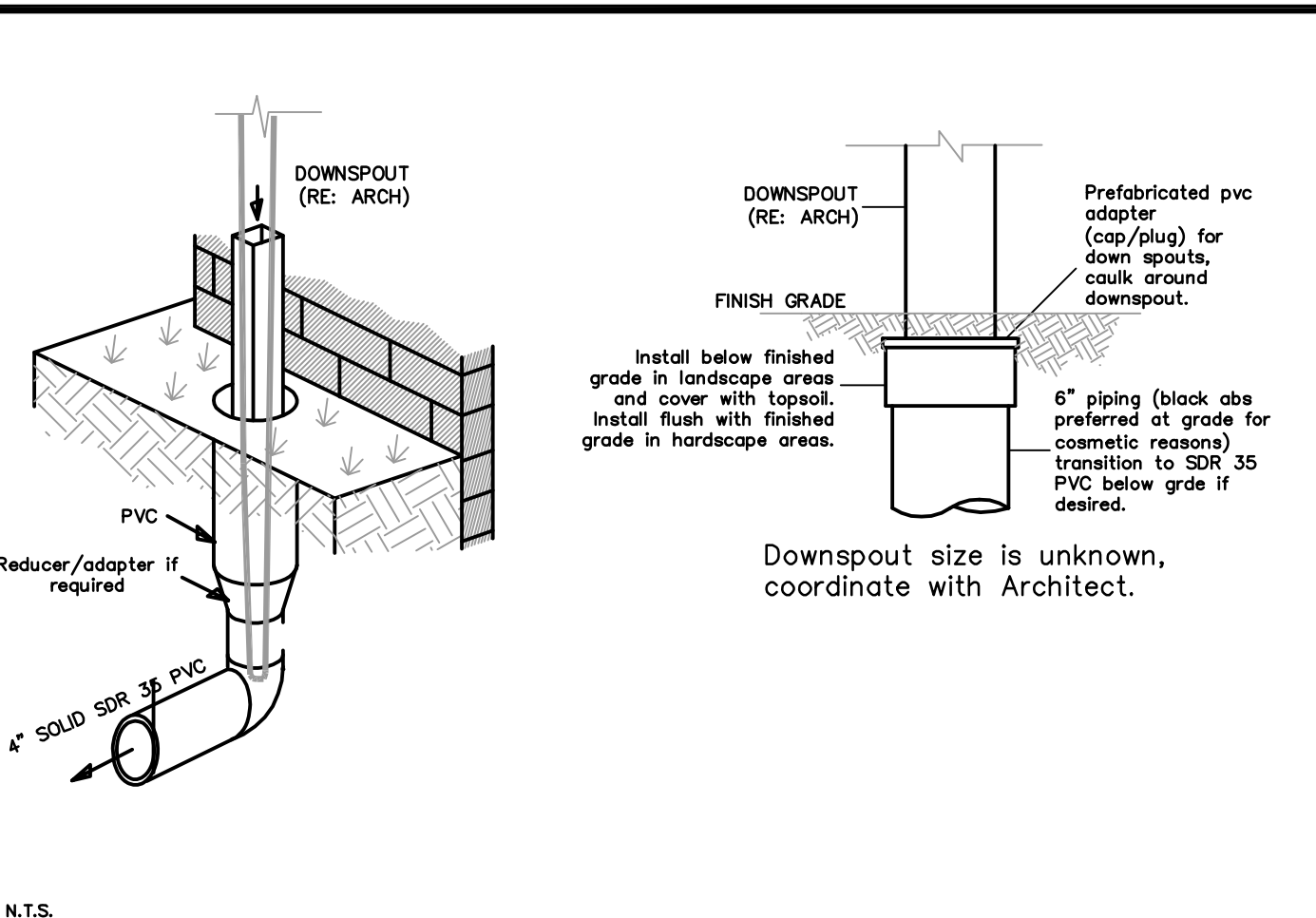
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05/12/2023	PRELIMINARY PLAN	MCW	
12/12/2023	FINAL SITE PLAN	MCW	
01/25/2024	RESPONSE TO COMMENTS	MCW	
05/14/2024	FINAL SITE SUBMITTAL	MCW	
05/31/2024	RESPONSE TO COMMENTS	MCW	

DESIGNED	DRAWN	CHECKED	JOB NO.	DATE
MCW	MCW	MCW_TSL		05/11/2023

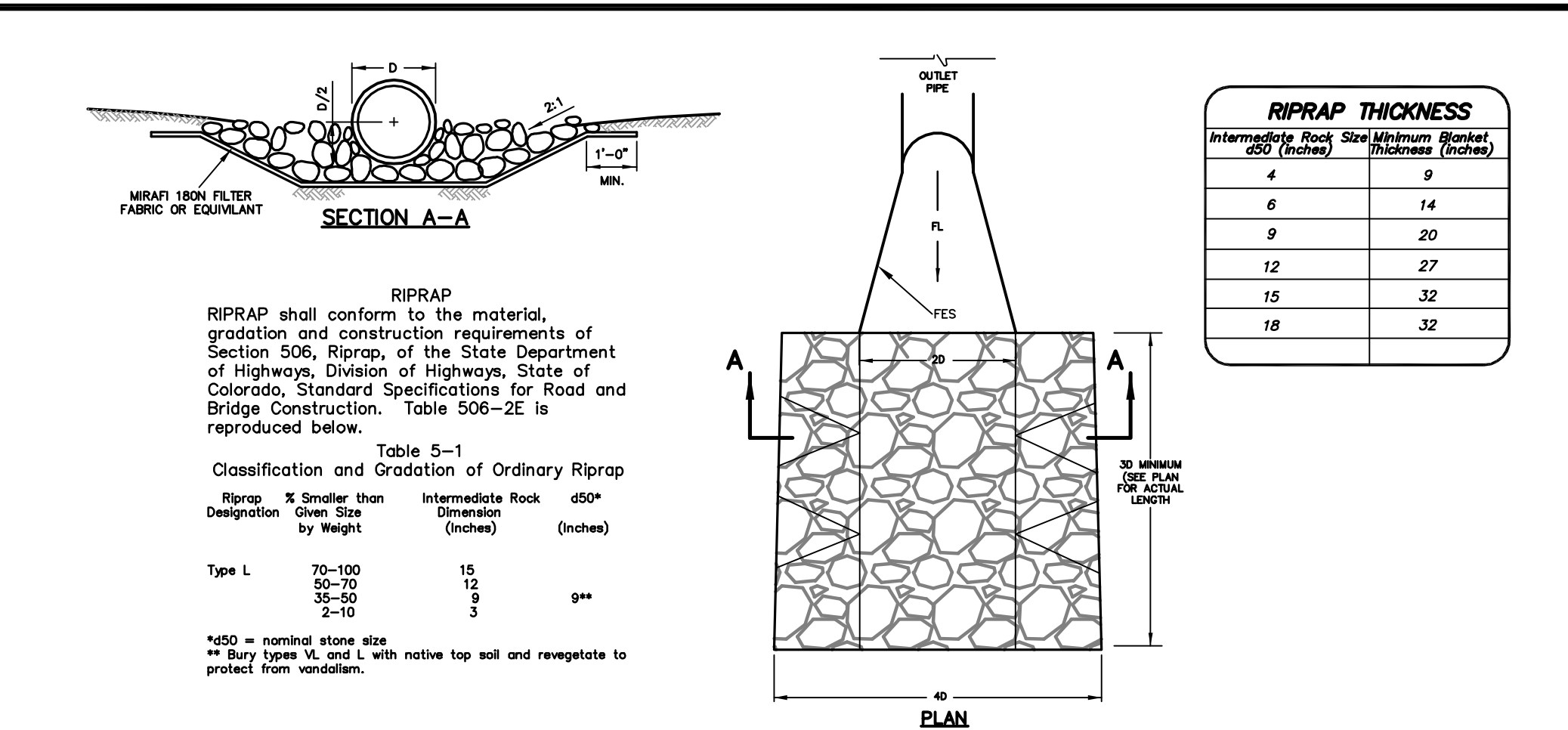
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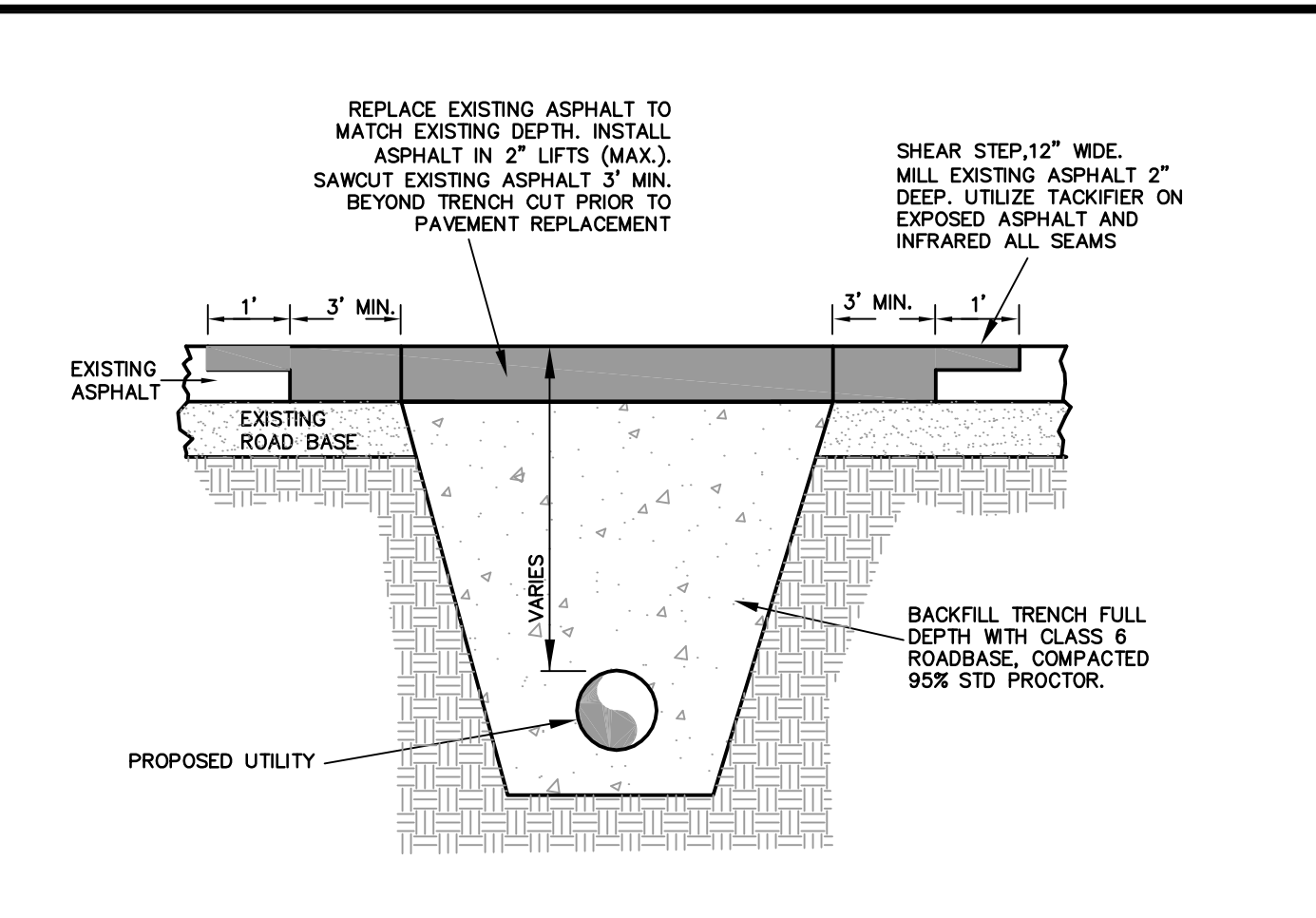
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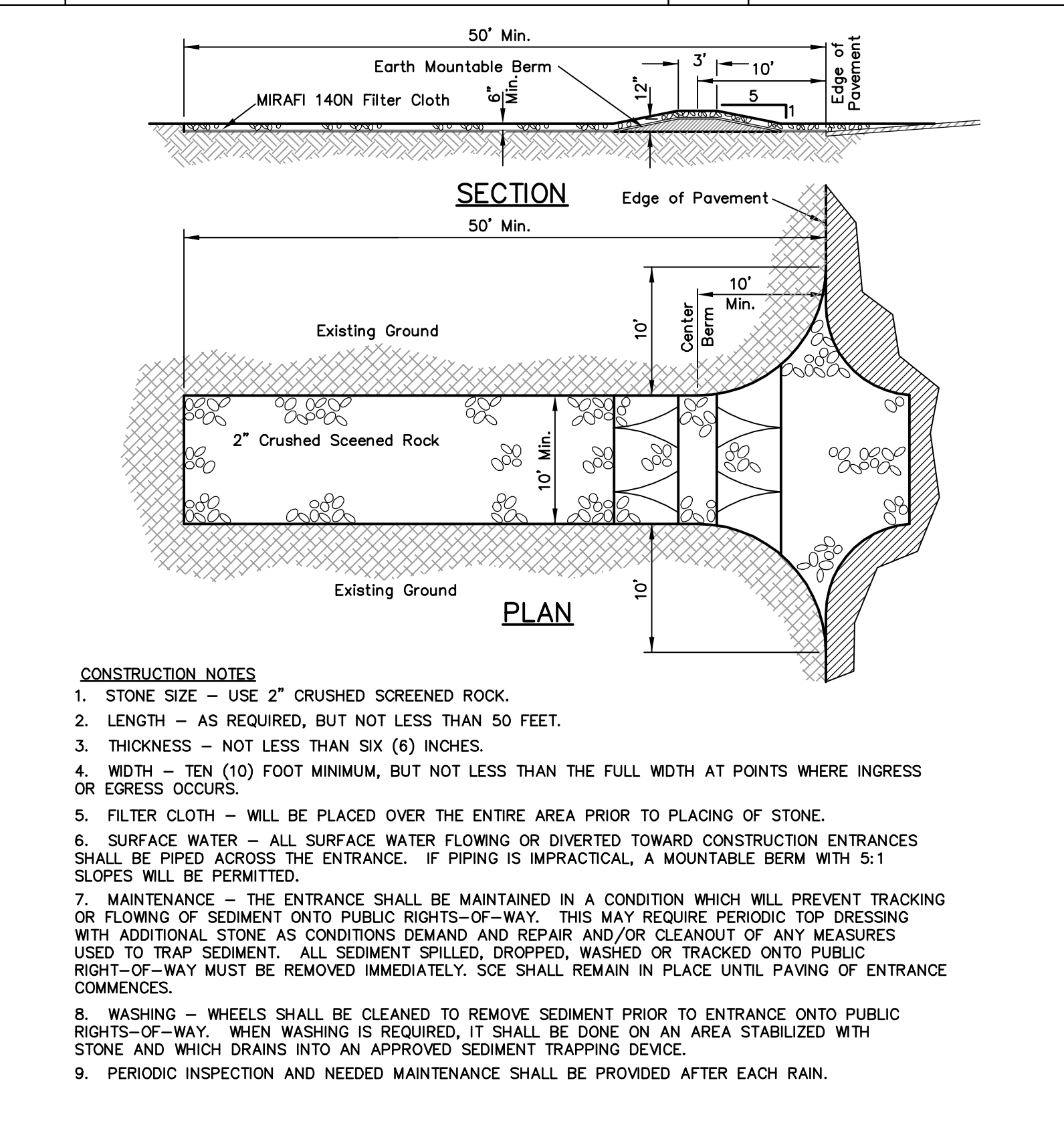
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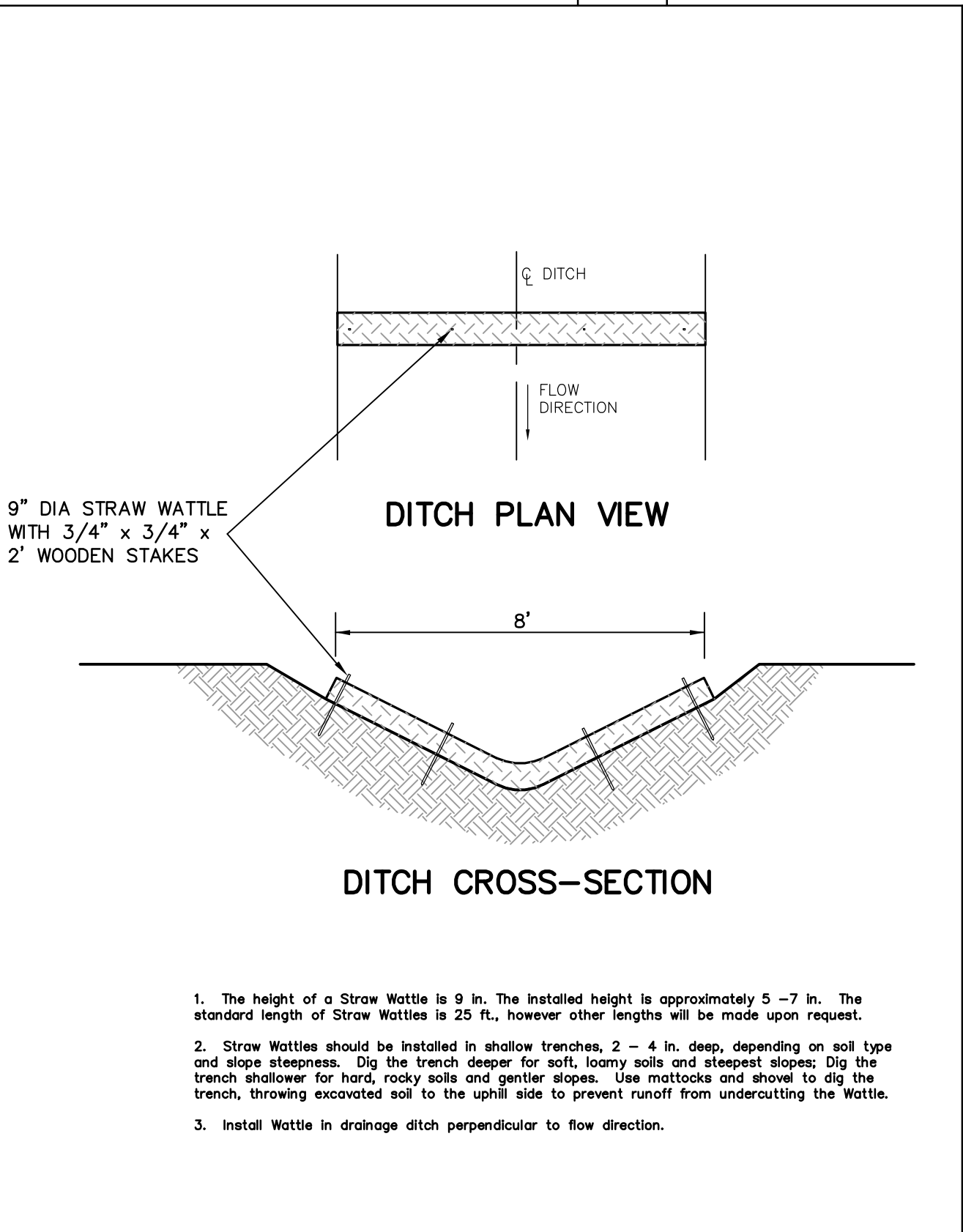
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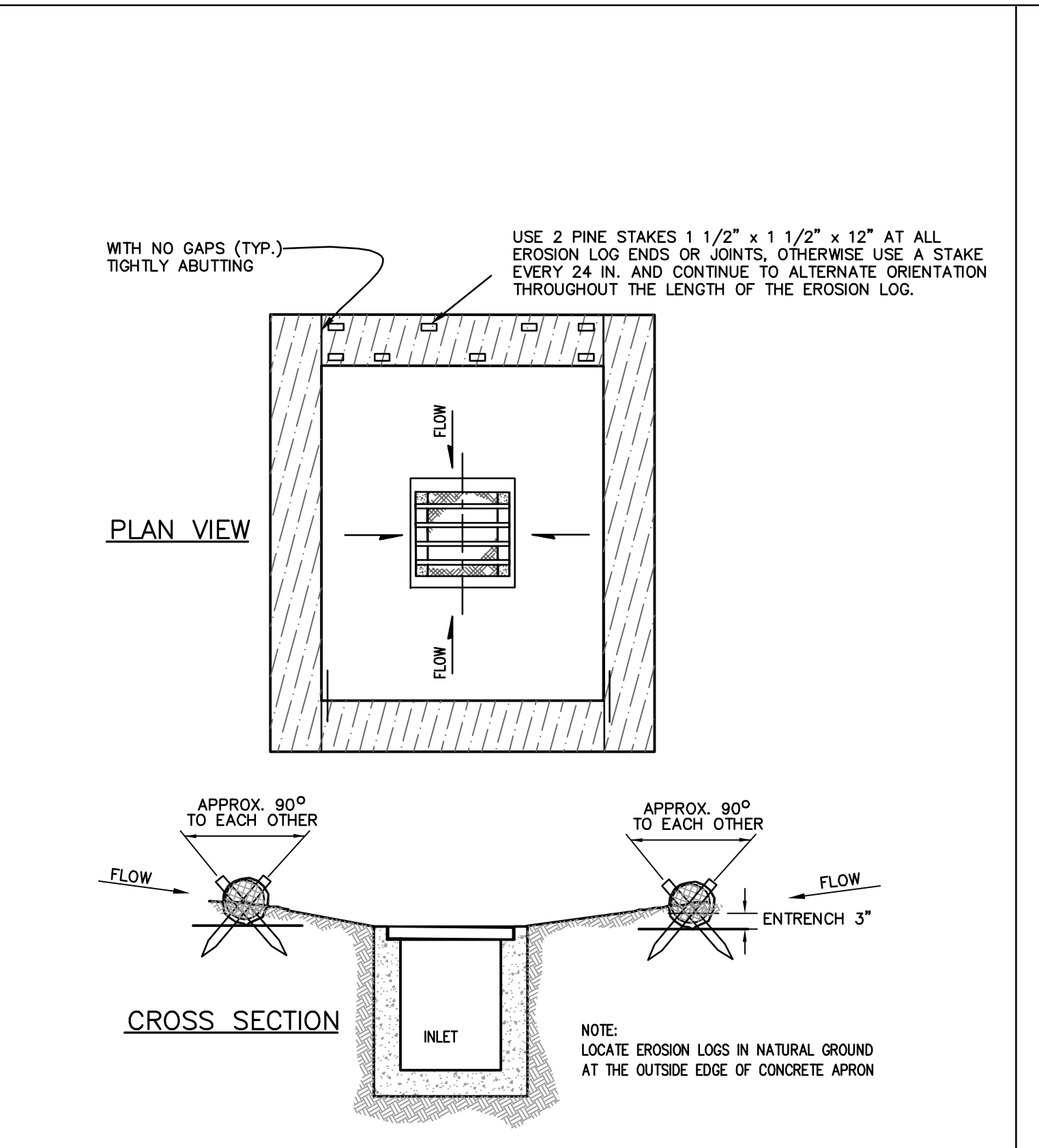
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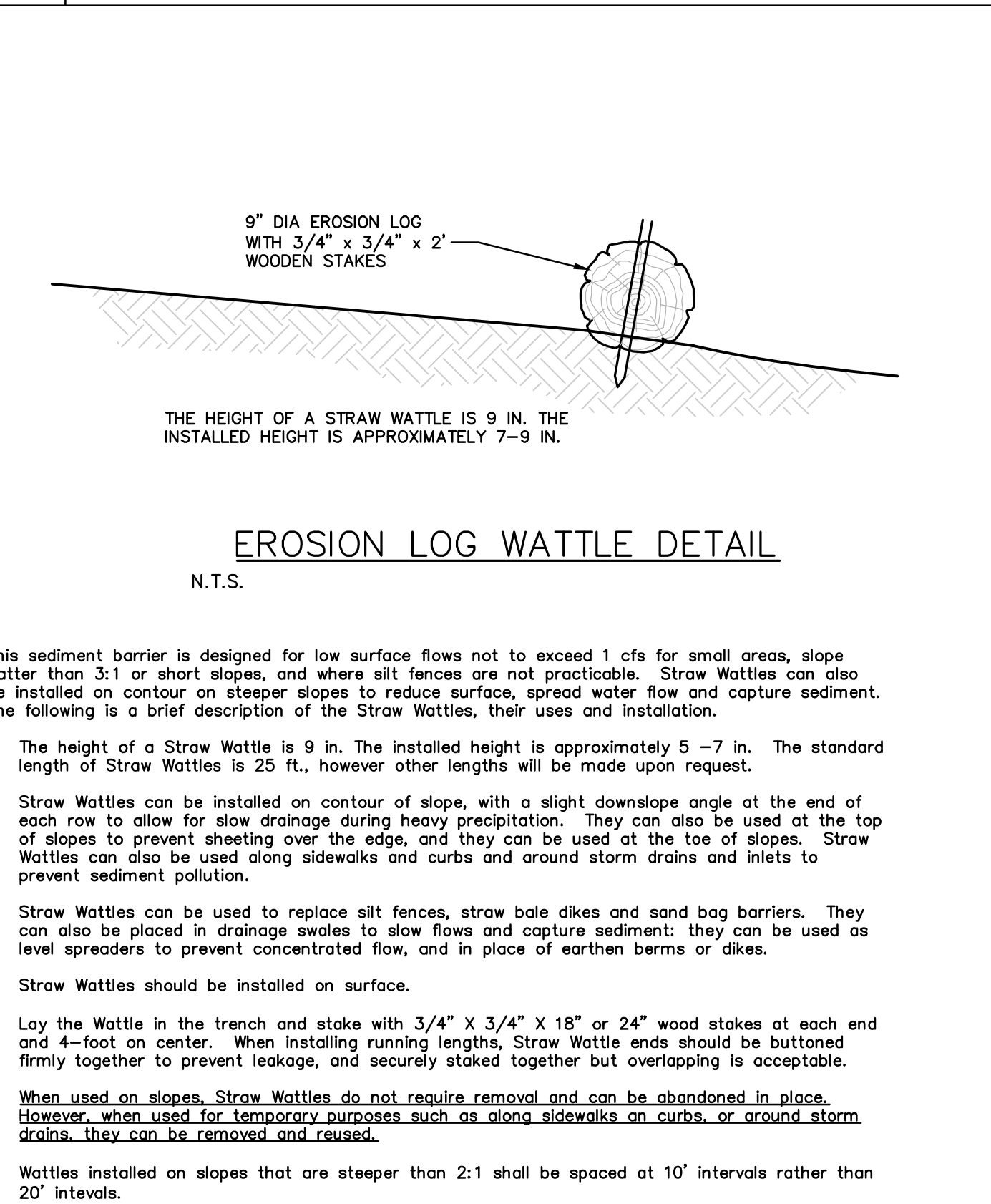
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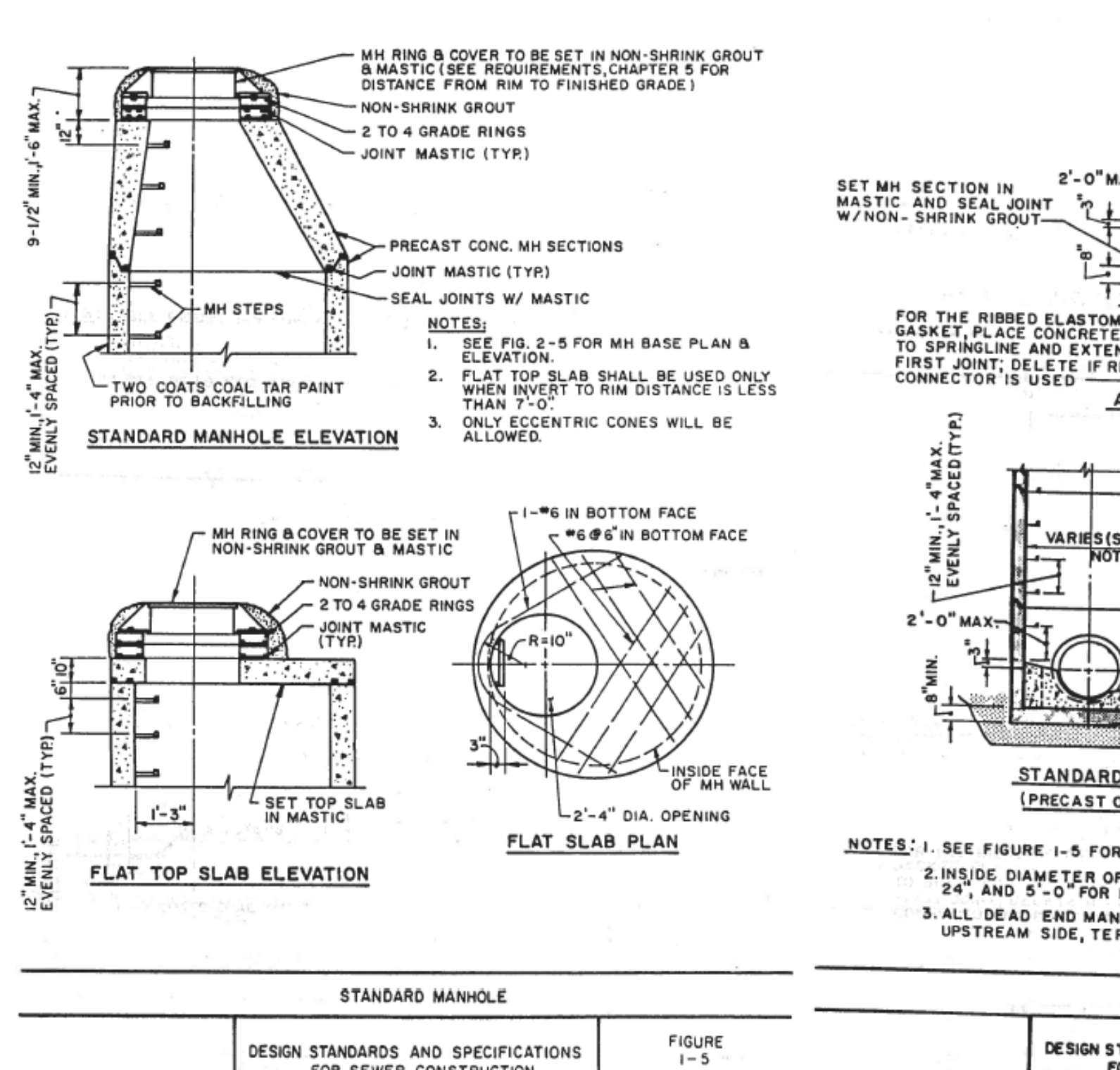
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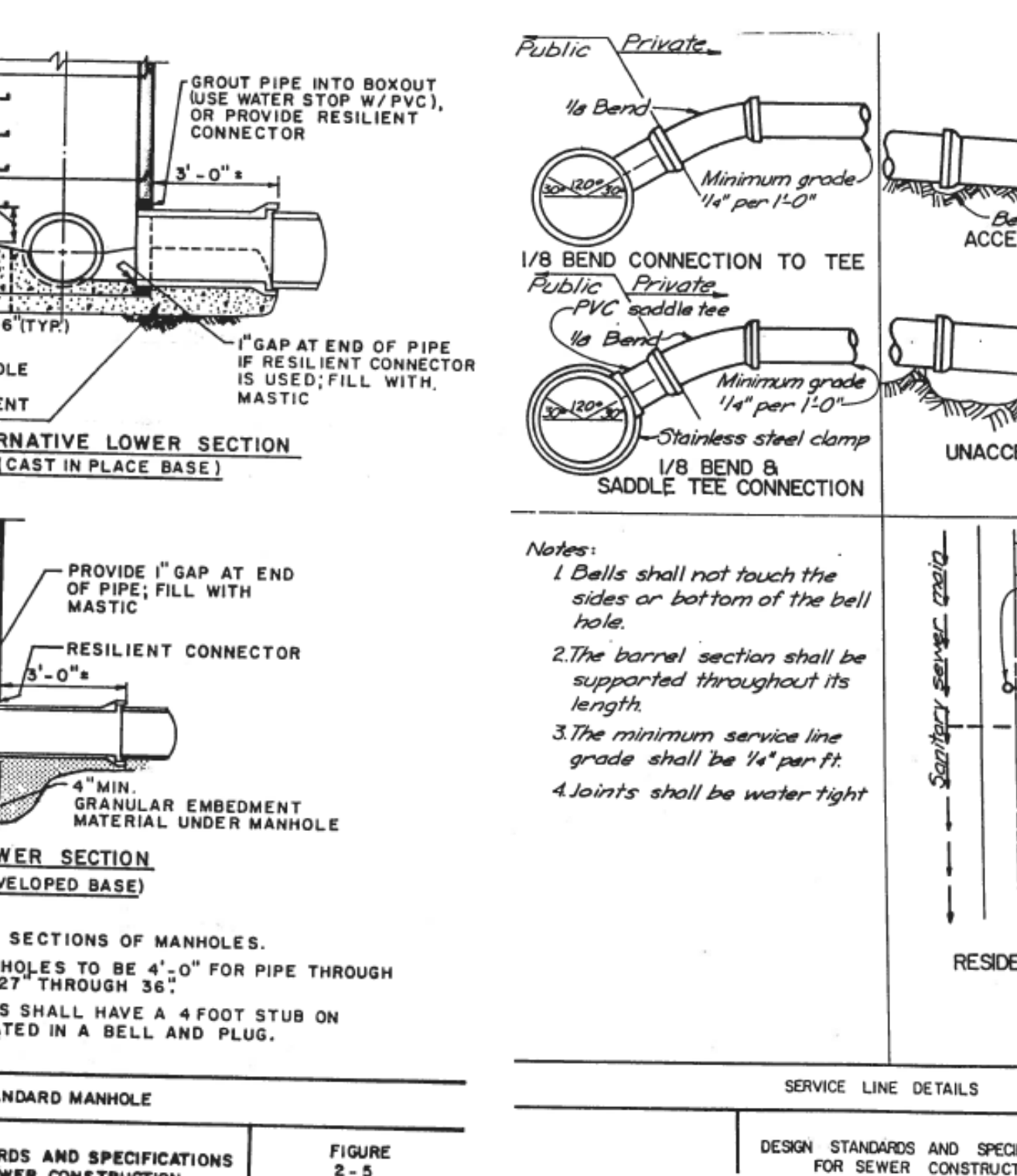
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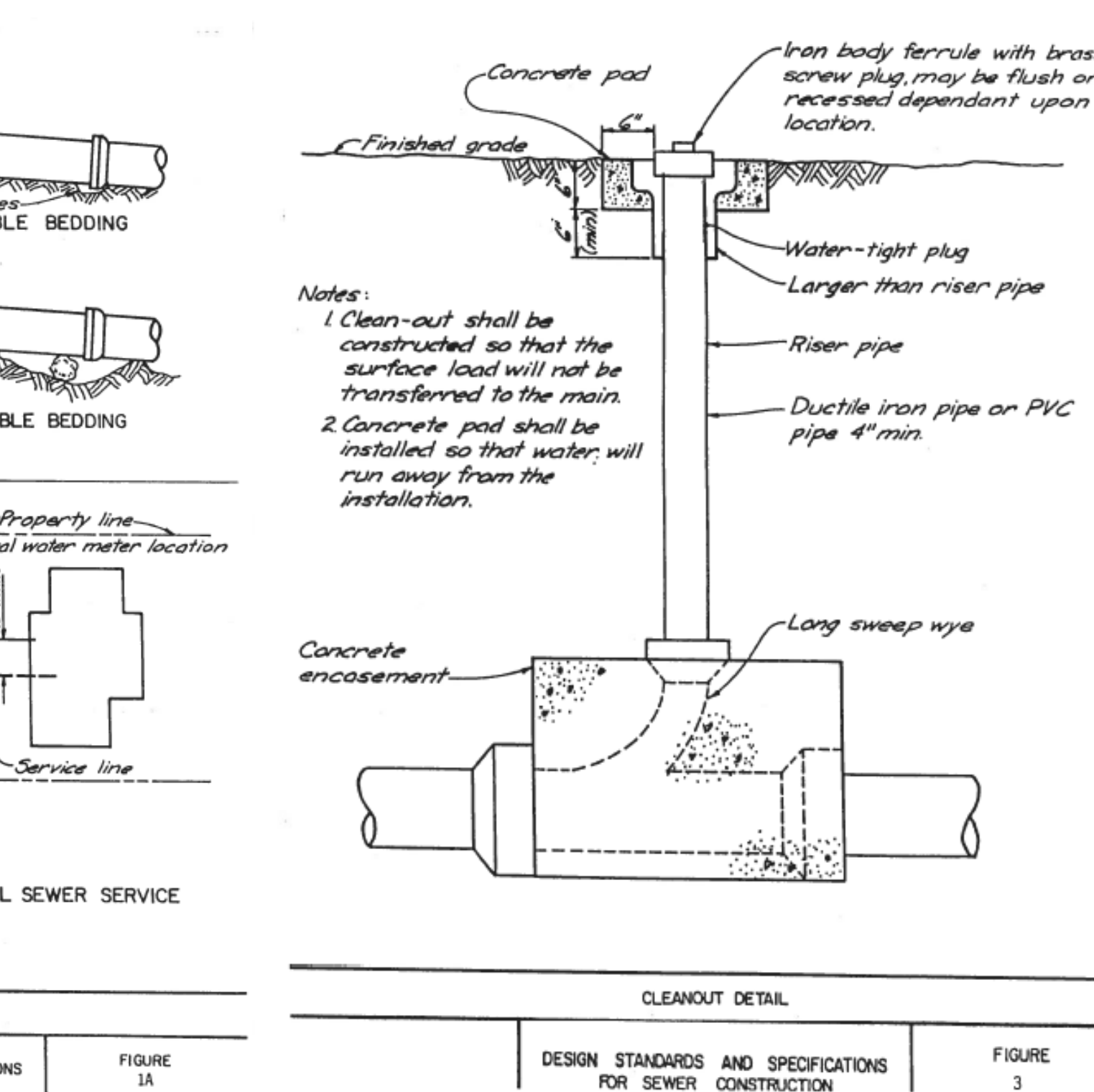
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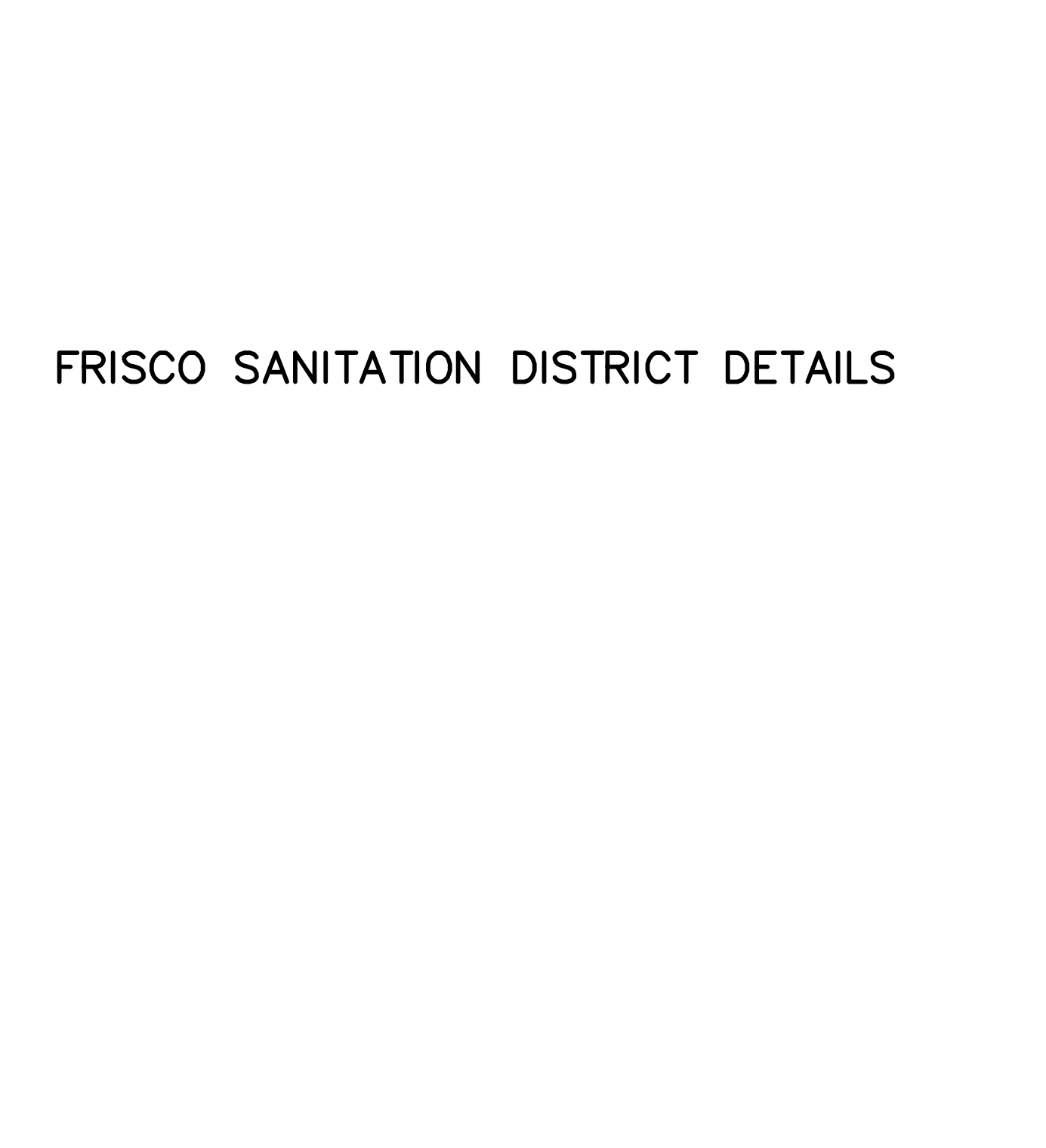
DESIGN STANDARDS AND SPECIFICATIONS FOR SEWER CONSTRUCTION FIGURE 1-5



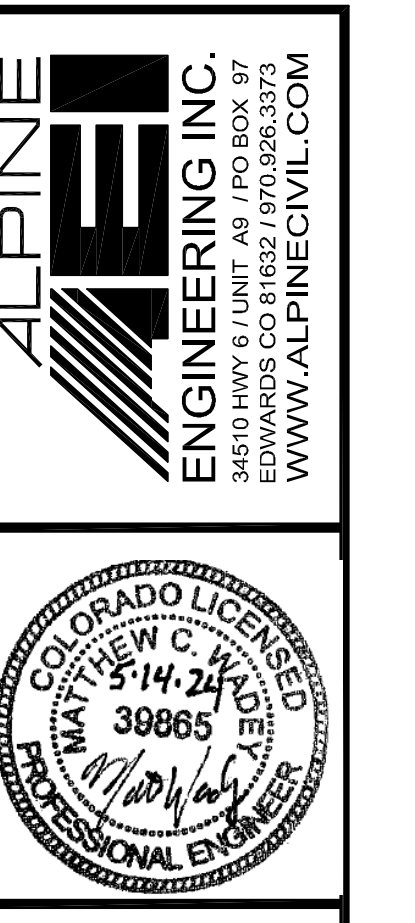
DESIGN STANDARDS AND SPECIFICATIONS FOR SEWER CONSTRUCTION FIGURE 2-5



DESIGN STANDARDS AND SPECIFICATIONS FOR SEWER CONSTRUCTION FIGURE 1A



DESIGN STANDARDS AND SPECIFICATIONS FOR SEWER CONSTRUCTION FIGURE 3



101 WEST MAIN STREET  
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DETAILS

NO.	DATE	REVISIONS	BY
05/12/2023	PRELIMINARY PLAN	MCW	
12/12/2023	FINAL SITE PLAN	MCW	
01/25/2024	RESPONSE TO COMMENTS		
05/14/2024	FINAL SITE SUBMITTAL		
05/31/2024	RESPONSE TO COMMENTS		

DESIGNED	DATE
MCW	05/11/2023

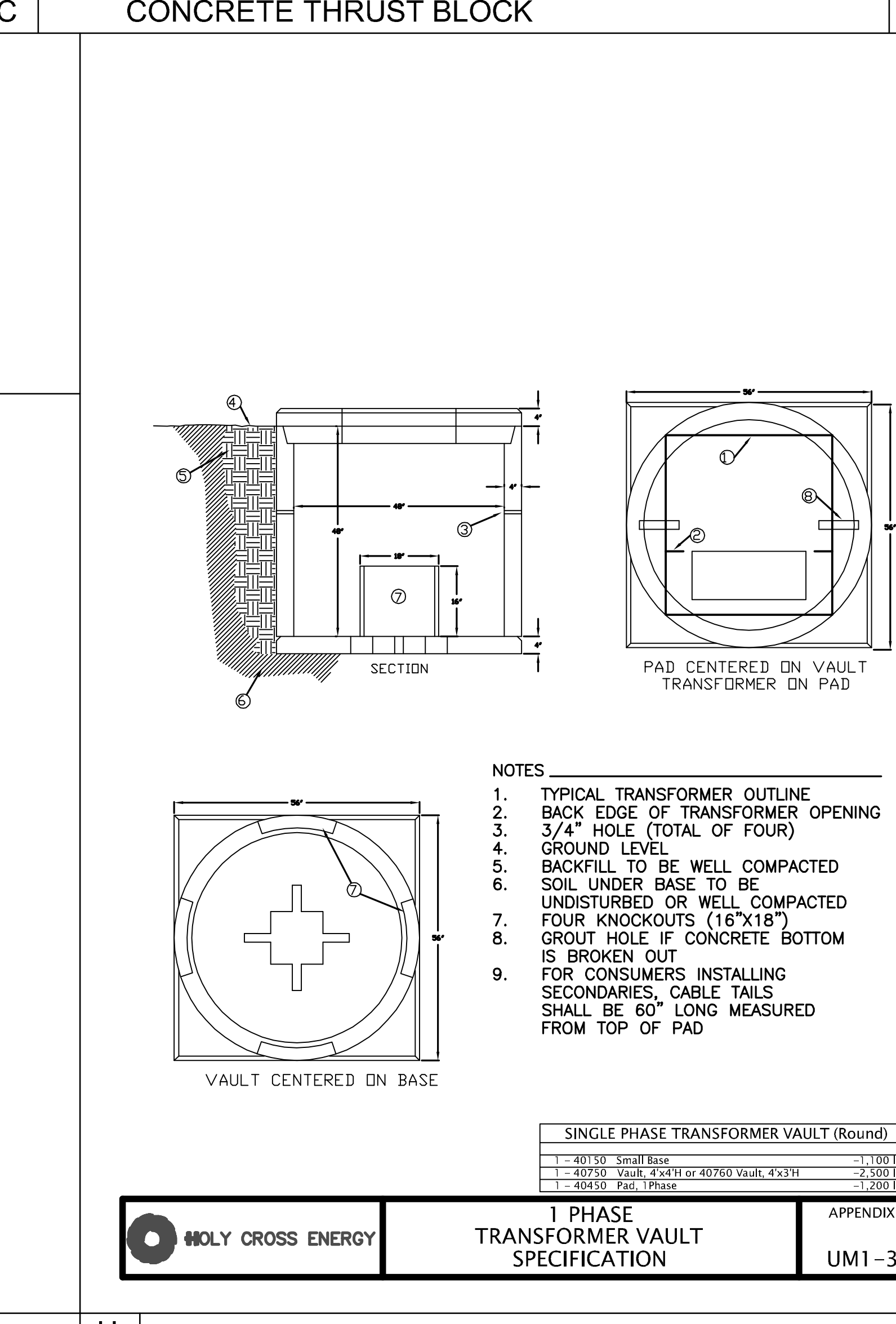
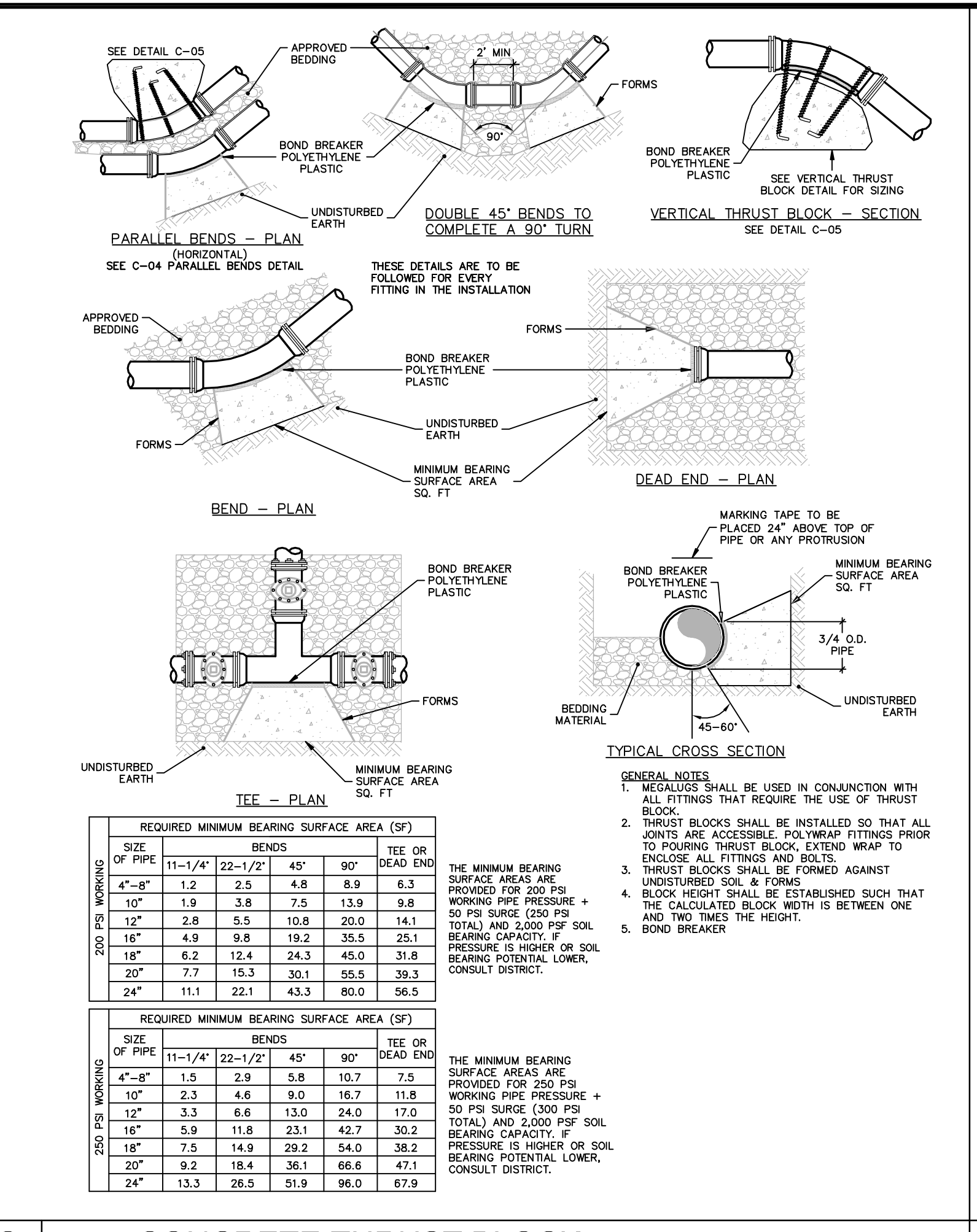
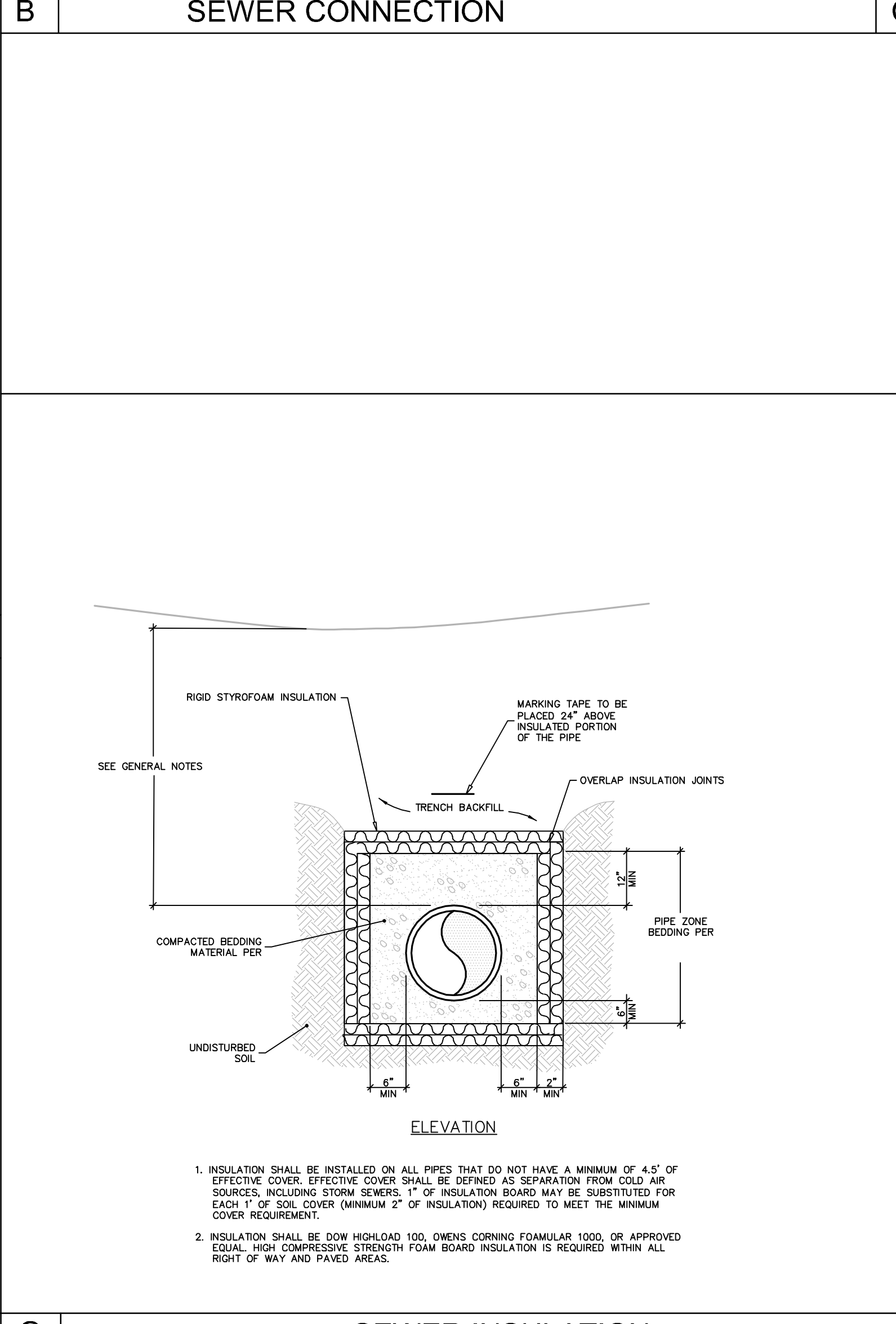
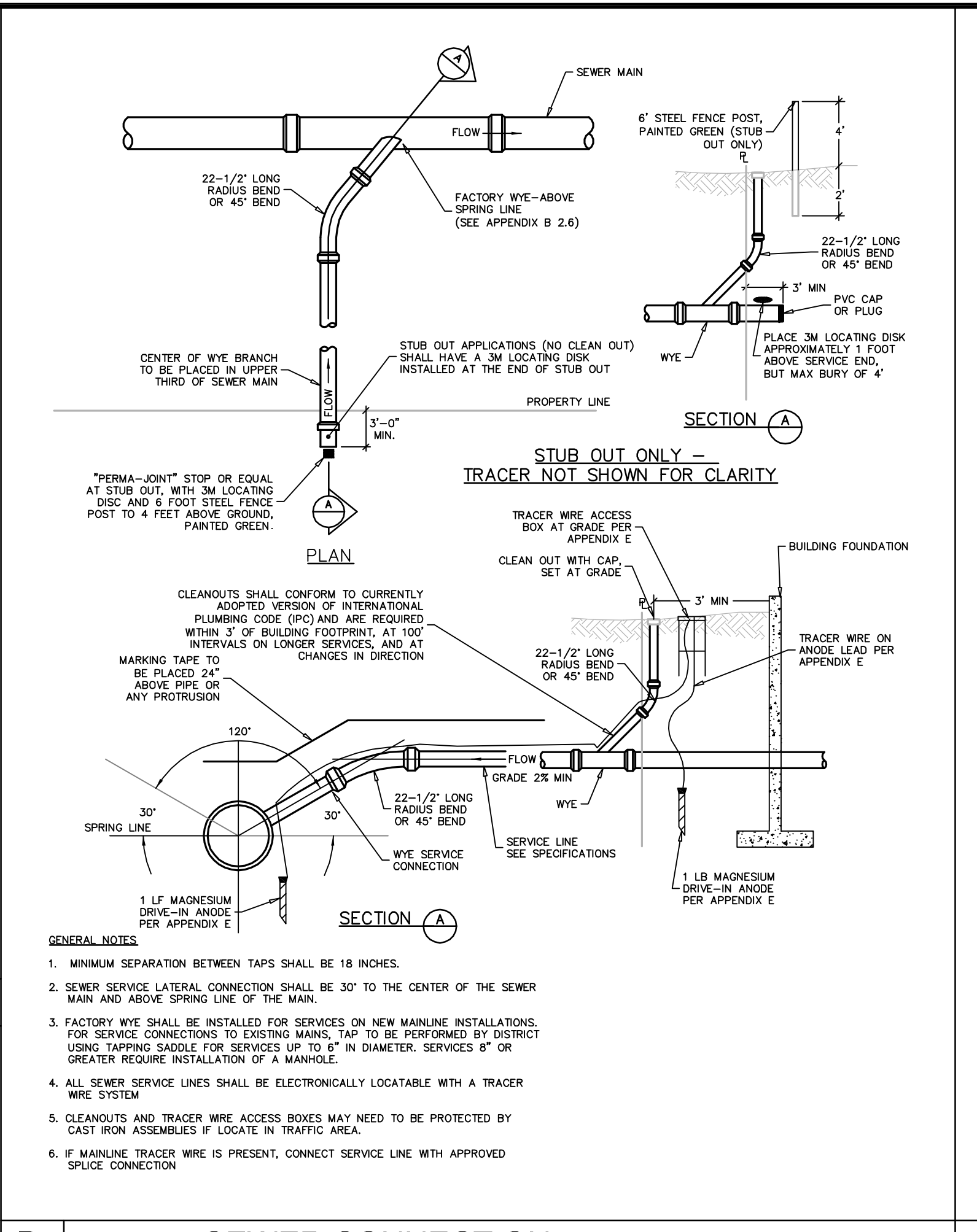
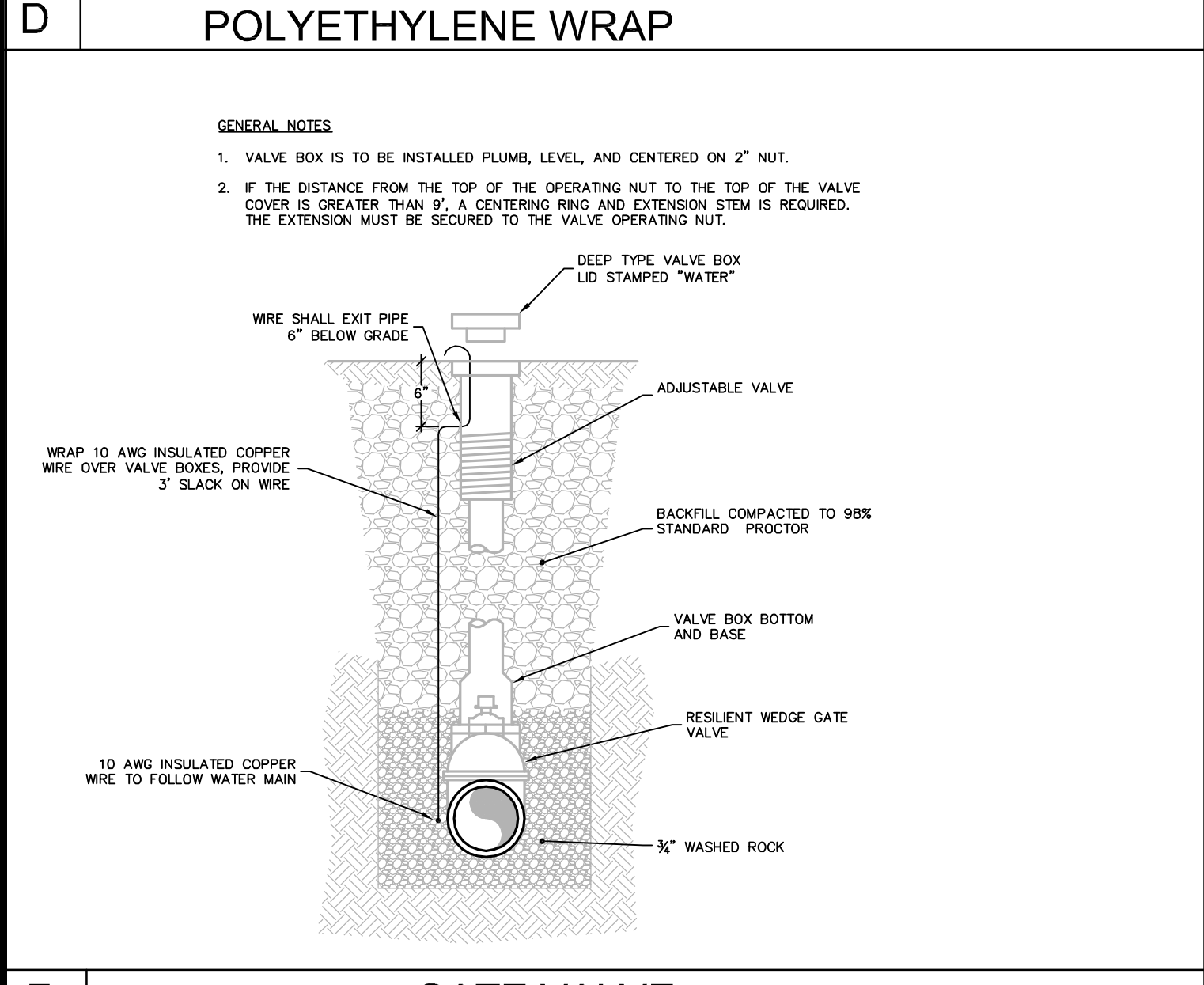
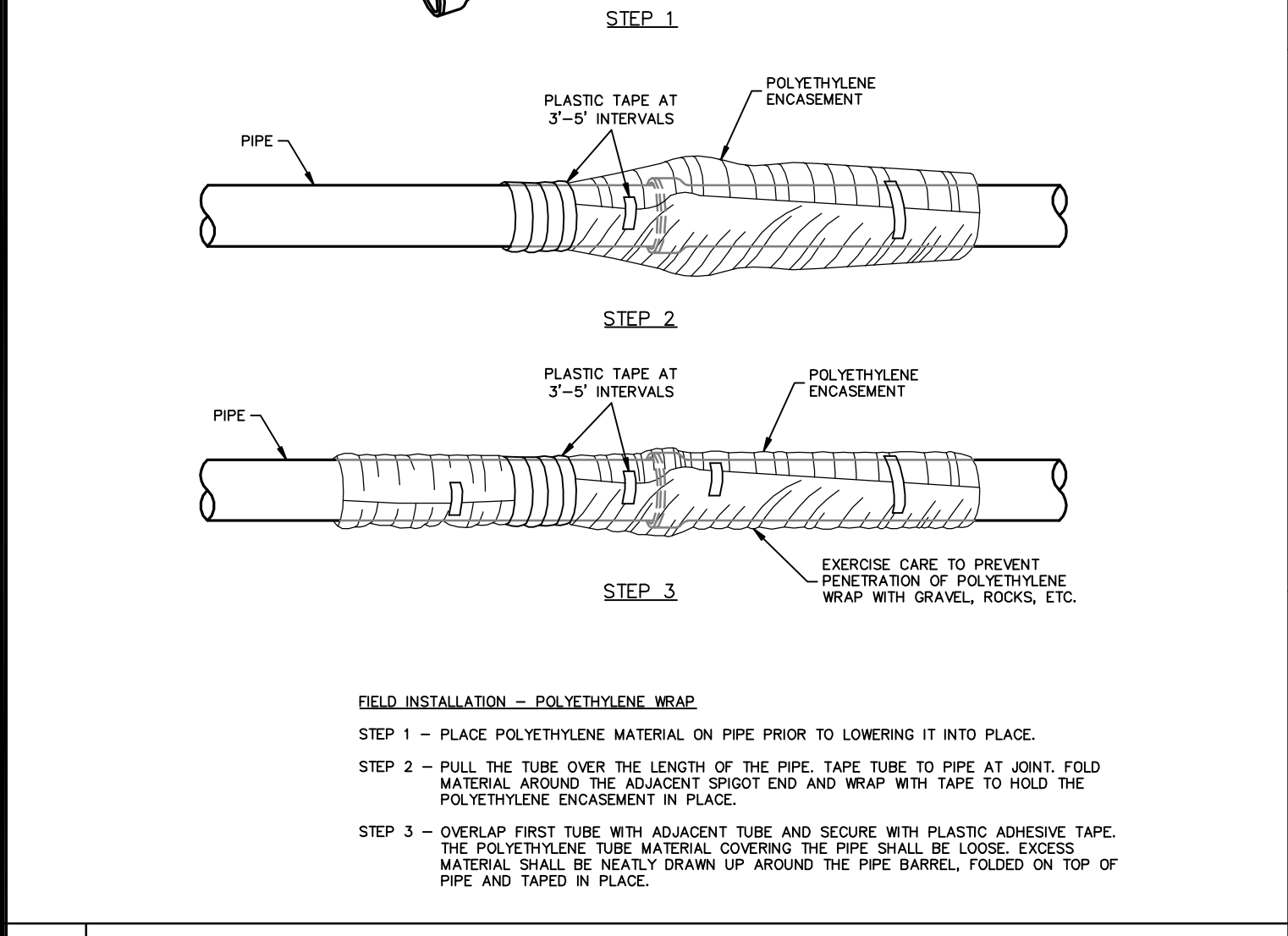
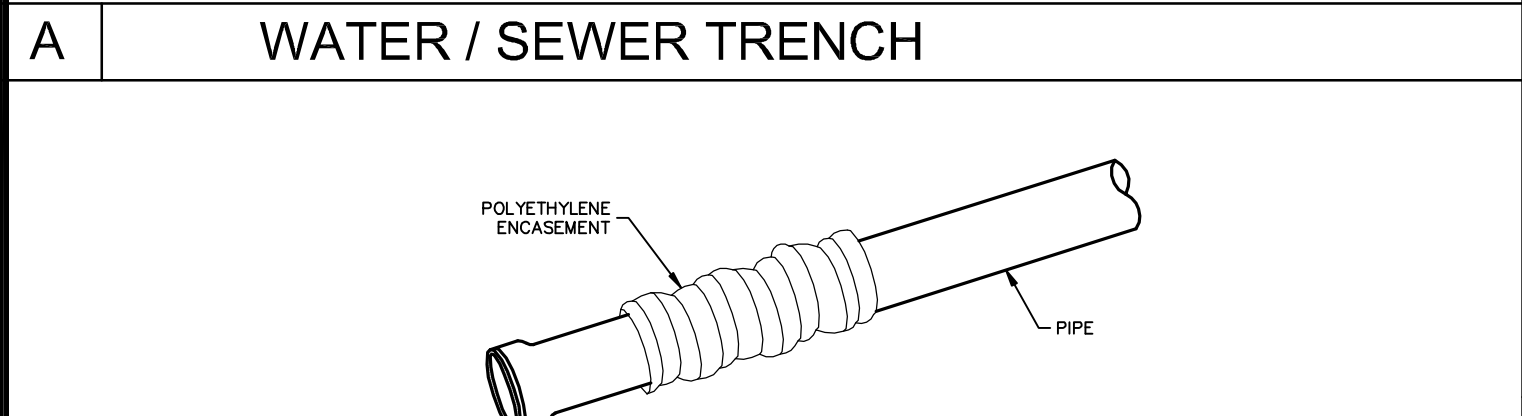
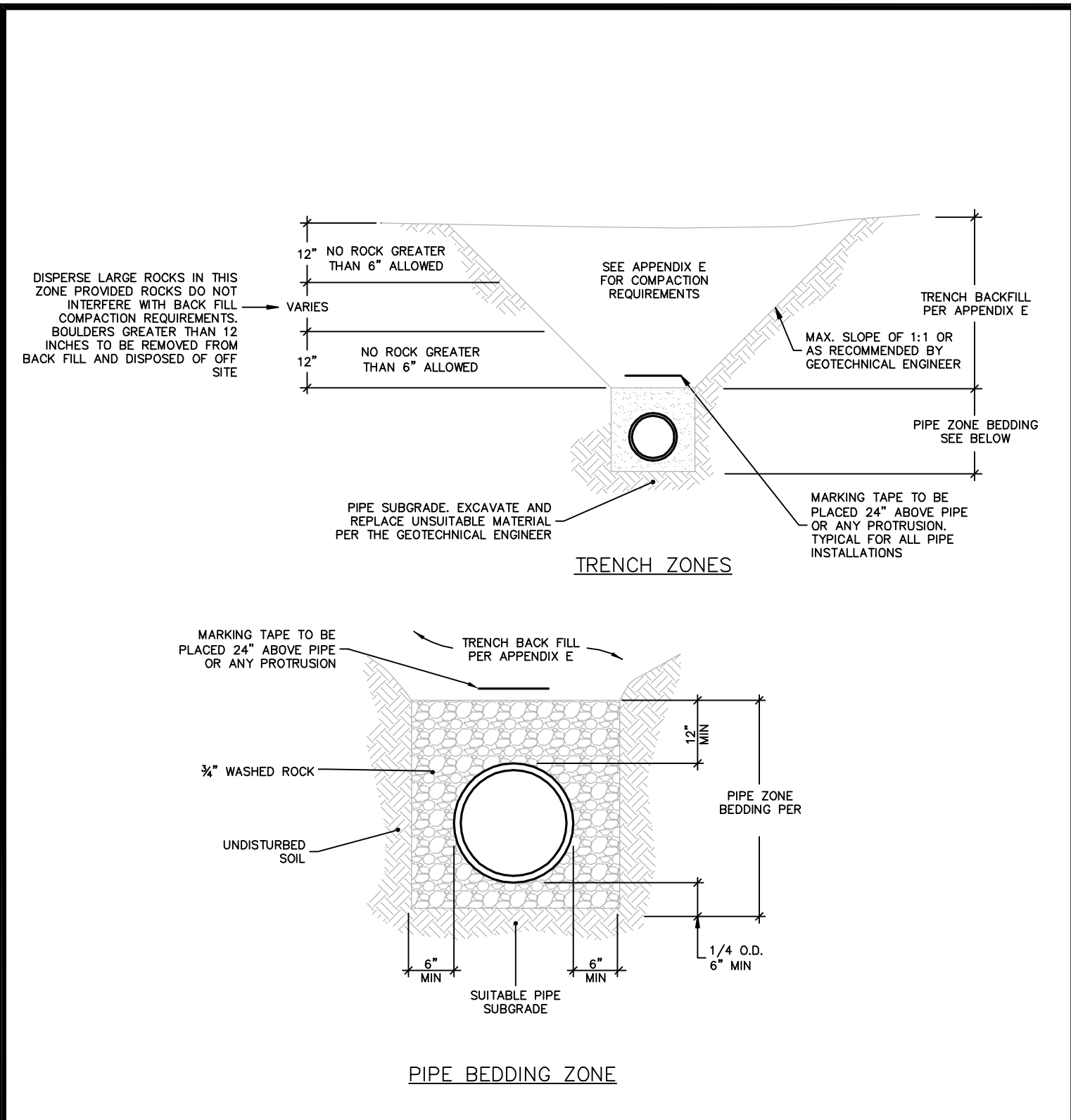
  

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ALPINE ENGINEERING INC.

34510 HWY 6 UNIT A9 P.O. BOX 97 EDWARDS CO BR257 / 703.296.3073 WWW.ALPIENGINEERING.COM

101 WEST MAIN STREET

FRISCO, CO

DETAILS

DESIGNED MCW

DRAWN MCW

CHECKED MCW\_TSL

JOB NO.

DATE 05/11/2023

SHEET C6.2

**GENERAL NOTES**

1. THE CONTRACTOR AND OWNER'S REPRESENTATIVE SHALL CONTACT THE LANDSCAPE ARCHITECT FOR A PRE-CONSTRUCTION MEETING PRIOR TO START OF ANY WORK SHOWN ON THESE PLANS.
2. THESE PLANS SHALL NOT BE UTILIZED FOR CONSTRUCTION OR PERMITTING UNLESS STATED FOR SUCH USE IN THE TITLE BLOCK.
3. DRAWINGS ARE INTENDED TO BE PRINTED ON 24" X 36" PAPER. PRINTING THESE DRAWINGS AT A DIFFERENT SIZE WILL IMPACT THE SCALE. VERIFY THE GRAPHIC SCALE BEFORE REFERENCING ANY MEASUREMENTS ON THESE SHEETS. THE RECIPIENT OF THESE DRAWINGS SHALL BE RESPONSIBLE FOR ANY ERRORS RESULTING FROM INCORRECT PRINTING, COPYING, OR ANY OTHER CHANGES THAT ALTER THE SCALE OF THE DRAWINGS.
4. VERIFY ALL PLAN DIMENSIONS PRIOR TO START OF CONSTRUCTION. NOTIFY THE OWNER'S REPRESENTATIVE TO ADDRESS ANY QUESTIONS OR CLARIFY ANY DISCREPANCIES.
5. WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.
6. REVISIONS TO THESE DOCUMENTS, REVIEW AND CONSIDERATIONS OF SUBSTITUTIONS, OWNER DIRECTED CHANGES, AND/OR RFI RESPONSES WHICH REQUIRE PROVIDING ADDITIONAL DETAIL AFTER APPROVAL OF THE PERMIT SET MAY REQUIRE APPROVAL OF AN ADDITIONAL SERVICES REQUEST BY THE CLIENT.
7. SUBMIT A CHANGE ORDER FOR APPROVAL FOR ANY CHANGES TO WORK SCOPE RESULTING FROM FIELD CONDITIONS OR DIRECTION BY OWNER'S REPRESENTATIVE WHICH REQUIRE ADDITIONAL COST TO THE OWNER PRIOR TO PERFORMANCE OF WORK.
8. THE CONTRACTOR SHALL PROVIDE A STAKED LAYOUT OF ALL SITE IMPROVEMENTS FOR INSPECTION BY THE OWNER'S REPRESENTATIVE AND MAKE MODIFICATIONS AS REQUIRED. ALL LAYOUT INFORMATION IS AVAILABLE IN DIGITAL FORMAT FOR USE BY THE CONTRACTOR.
9. IF A GEOTECHNICAL SOILS REPORT IS NOT AVAILABLE AT THE TIME OF CONSTRUCTION, NORRIS DESIGN RECOMMENDS A REPORT BE AUTHORIZED BY THE OWNER AND THAT ALL RECOMMENDATIONS OF THE REPORT ARE FOLLOWED DURING CONSTRUCTION. THE CONTRACTOR SHALL USE THESE CONTRACT DOCUMENTS AS A BASIS FOR THE BID. IF THE OWNER ELECTS TO PROVIDE A GEOTECHNICAL REPORT, THE CONTRACTOR SHALL REVIEW THE REPORT AND SUBMIT AN APPROPRIATE CHANGE ORDER TO THE OWNER'S REPRESENTATIVE IF ADDITIONAL COSTS ARE REQUESTED.
10. CONTRACTOR SHALL CONFIRM THAT SITE CONDITIONS ARE SIMILAR TO THE PLANS, WITHIN TOLERANCES STATED IN THE CONTRACT DOCUMENTS, AND SATISFACTORY TO THE CONTRACTOR PRIOR TO START OF WORK. SHOULD SITE CONDITIONS BE DIFFERENT THAN REPRESENTED ON THE PLANS OR UNSATISFACTORY TO THE CONTRACTOR, THE CONTRACTOR SHALL CONTACT THE OWNER'S REPRESENTATIVE FOR CLARIFICATION AND FURTHER DIRECTION.
11. CONTRACTOR IS RESPONSIBLE TO PAY FOR, AND OBTAIN, ANY REQUIRED APPLICATIONS, PERMITTING, LICENSES, INSPECTIONS AND METERS ASSOCIATED WITH WORK.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO ANY VIOLATIONS OR NON-CONFORMANCE WITH THE PLANS, SPECIFICATIONS, CONTRACT DOCUMENTS, JURISDICTIONAL CODES, AND REGULATORY AGENCIES.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF ALL UTILITY LOCATES PRIOR TO ANY EXCAVATION. REFER TO ENGINEERING UTILITY PLANS FOR ALL PROPOSED UTILITY LOCATIONS AND DETAILS. NOTIFY OWNER'S REPRESENTATIVE IF EXISTING OR PROPOSED UTILITIES INTERFERE WITH THE ABILITY TO PERFORM WORK.
14. UNLESS IDENTIFIED ON THE PLANS FOR DEMOLITION OR REMOVAL, THE CONTRACTOR IS RESPONSIBLE FOR THE COST TO REPAIR UTILITIES, ADJACENT OR EXISTING LANDSCAPE, ADJACENT OR EXISTING PAVING, OR ANY PUBLIC AND PRIVATE PROPERTY THAT IS DAMAGED BY THE CONTRACTOR OR THEIR SUBCONTRACTOR'S OPERATIONS DURING INSTALLATION, ESTABLISHMENT OR DURING THE SPECIFIED MAINTENANCE PERIOD. ALL DAMAGES SHALL BE REPAIRED TO PRE-CONSTRUCTION CONDITIONS AS DETERMINED BY THE OWNER'S

- REPRESENTATIVE. CONTRACTOR SHALL BE RESPONSIBLE FOR LOGGING ANY DAMAGES PRIOR TO START OF CONSTRUCTION AND DURING THE CONTRACT PERIOD.
15. ALL WORK SHALL BE CONFINED TO THE AREA WITHIN THE CONSTRUCTION LIMITS AS SHOWN ON THE PLANS. ANY AREAS OR IMPROVEMENTS DISTURBED OUTSIDE THESE LIMITS SHALL BE RETURNED TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE. IN THE EVENT THE CONTRACTOR REQUIRES A MODIFICATION TO THE CONSTRUCTION LIMITS, WRITTEN PERMISSION MUST BE OBTAINED FROM THE OWNER'S REPRESENTATIVE PRIOR TO ANY DISTURBANCE OUTSIDE OF THE LIMITS OF WORK.
  16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY OF THEIR TRENCHES OR EXCAVATIONS THAT SETTLE.
  17. THE CONTRACTOR SHALL BE RESPONSIBLE TO PREPARE AND SUBMIT A TRAFFIC CONTROL PLAN TO THE APPROPRIATE JURISDICTIONAL AGENCIES AND THE OWNER'S REPRESENTATIVE IF THEIR WORK AND OPERATIONS AFFECT OR IMPACT THE PUBLIC RIGHTS-OF-WAY. OBTAIN APPROVAL PRIOR TO ANY WORK WHICH AFFECTS OR IMPACTS THE PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THIS REQUIREMENT DURING THE CONTRACT PERIOD.
  18. SIGHT TRIANGLES AND SIGHT LINES SHALL REMAIN UNOBTSTRUCTED BY EQUIPMENT, CONSTRUCTION MATERIALS, PLANT MATERIAL OR ANY OTHER VISUAL OBSTACLE DURING THE CONTRACT PERIOD AND AT MATURITY OF PLANTS PER LOCAL JURISDICTIONAL REQUIREMENTS.
  19. NO PLANT MATERIAL OTHER THAN GROUND COVER IS ALLOWED TO BE PLANTED ADJACENT TO FIRE HYDRANTS AS STIPULATED BY JURISDICTIONAL REQUIREMENTS.
  20. COORDINATE SITE ACCESS, STAGING, STORAGE AND CLEANOUT AREAS WITH OWNER'S REPRESENTATIVE.
  21. CONTRACTOR IS RESPONSIBLE FOR PROVIDING TEMPORARY SAFETY FENCING AND BARRIERS AROUND ALL IMPROVEMENTS SUCH AS WALLS, PLAY STRUCTURES, EXCAVATIONS, ETC. ASSOCIATED WITH THEIR WORK UNTIL SUCH FACILITIES ARE COMPLETELY INSTALLED PER THE PLANS, SPECIFICATIONS AND MANUFACTURER'S RECOMMENDATIONS.
  22. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF THEIR MATERIAL STOCK PILES AND WORK FROM VANDALISM, EROSION OR UNINTENDED DISTURBANCE DURING THE CONSTRUCTION PERIOD AND UNTIL FINAL ACCEPTANCE IS ISSUED.
  23. THE CONTRACTOR SHALL KNOW, UNDERSTAND AND ABIDE BY ANY STORM WATER POLLUTION PREVENTION PLAN (SWPPP) ASSOCIATED WITH THE SITE. IF A STORM WATER POLLUTION PREVENTION PLAN IS NOT PROVIDED BY THE OWNER'S REPRESENTATIVE, REQUEST A COPY BEFORE PERFORMANCE OF ANY SITE WORK.
  24. MAINTAIN ANY STORM WATER MANAGEMENT FACILITIES THAT EXIST ON SITE FOR FULL FUNCTIONALITY. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ANY NEW STORM WATER MANAGEMENT FACILITIES THAT ARE IDENTIFIED IN THE SCOPE OF WORK TO FULL FUNCTIONALITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER FOR FAILURE TO MAINTAIN STORM WATER MANAGEMENT FACILITIES DURING THE CONTRACT PERIOD.
  25. THE CONTRACTOR SHALL PREVENT SEDIMENT, DEBRIS AND ALL OTHER POLLUTANTS FROM EXITING THE SITE OR ENTERING THE STORM SEWER SYSTEM DURING ALL DEMOLITION OR CONSTRUCTION OPERATIONS THAT ARE PART OF THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THESE REQUIREMENTS DURING THEIR CONTRACTED COURSE OF WORK.
  26. THE CONTRACTOR SHALL BE RESPONSIBLE TO PREVENT ANY IMPACTS TO ADJACENT WATERWAYS, WETLANDS, OR OTHER ENVIRONMENTALLY SENSITIVE AREAS RESULTING FROM WORK DONE AS PART OF THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THESE STANDARDS DURING THEIR CONTRACTED COURSE OF WORK.

27. THE CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS SHALL INSURE THAT ALL LOADS OF CONSTRUCTION MATERIAL IMPORTED TO OR EXPORTED FROM THE PROJECT SITE SHALL BE PROPERLY COVERED TO PREVENT LOSS OF MATERIAL DURING TRANSPORT. TRANSPORTATION METHODS ON PUBLIC RIGHT-OF-WAYS SHALL CONFORM TO JURISDICTIONAL REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THESE REQUIREMENTS.
28. THE CLEANING OF EQUIPMENT IS PROHIBITED AT THE JOB SITE UNLESS AUTHORIZED BY THE OWNER'S REPRESENTATIVE IN A DESIGNATED AREA. THE DISCHARGE OF WATER, WASTE CONCRETE, POLLUTANTS, OR OTHER MATERIALS SHALL ONLY OCCUR IN AREAS DESIGNED FOR SUCH USE AND APPROVED BY THE OWNER'S REPRESENTATIVE.
29. THE CLEANING OF CONCRETE EQUIPMENT IS PROHIBITED AT THE JOB SITE EXCEPT IN DESIGNATED CONCRETE WASHOUT AREAS. THE DISCHARGE OF WATER CONTAINING WASTE CONCRETE IN THE STORM SEWER IS PROHIBITED.
30. THE USE OF REBAR, STEEL STAKES, OR STEEL FENCE POSTS TO STAKE DOWN STRAW OR HAY BALES OR TO SUPPORT SILT FENCING USED AS AN EROSION CONTROL MEASURE IS PROHIBITED.
31. OPEN SPACE SWALES: IF SWALES ARE EXISTING ON SITE AND ARE NOT INTENDED TO BE MODIFIED AS PART OF THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE CONVEYANCE OF WATER WITHIN THE SWALES DURING THE CONTRACT PERIOD. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DIVERSION OR PUMPING OF WATER IF REQUIRED TO COMPLETE WORK. ANY SWALES DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED/RESTORED TO THEIR ORIGINAL CONDITION. IF THE SWALE NEEDS TO BE DISTURBED OR MODIFIED FOR ANY REASON, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE FOR APPROVAL PRIOR TO DISTURBANCE.
32. DETENTION AND WATER QUALITY PONDS: IF DETENTION PONDS AND WATER QUALITY PONDS ARE EXISTING ON SITE AND ARE NOT INTENDED TO BE MODIFIED AS PART OF THE PLANS, THE CONTRACTOR SHALL MINIMIZE DISTURBANCE TO THE PONDS, DRAINAGE STRUCTURES AND SPILLWAYS DURING CONSTRUCTION. ALL PONDS, DRAINAGE STRUCTURES AND SPILLWAYS SHALL BE MAINTAINED IN OPERABLE CONDITIONS AT ALL TIMES. ANY POND OR SPILLWAY AREAS DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED/RESTORED TO THEIR ORIGINAL CONDITION. IF THE POND NEEDS TO BE DISTURBED OR MODIFIED FOR ANY REASON, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE FOR APPROVAL PRIOR TO DISTURBANCE.
33. MAINTENANCE ACCESS BENCHES: IF MAINTENANCE BENCHES OR ACCESS ROADS EXIST ON SITE AND ARE NOT INTENDED TO BE MODIFIED AS PART OF THE PLANS, THE CONTRACTOR SHALL MINIMIZE DISTURBANCE TO THE BENCHES OR ACCESS ROADS DURING CONSTRUCTION. ANY BENCHES OR ACCESS ROADS DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED/RESTORED TO THEIR ORIGINAL CONDITION. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL EXISTING BENCHES AND ACCESS ROADS DURING THE CONSTRUCTION PERIOD. IF ACCESS NEEDS TO BE BLOCKED FOR ANY REASON, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE FOR APPROVAL PRIOR TO INTERRUPTION OF ACCESS.
34. LOCAL, STATE AND FEDERAL JURISDICTIONAL REQUIREMENTS, RESTRICTIONS OR PROCEDURES SHALL SUPERSEDE THESE PLANS, NOTES AND SPECIFICATIONS WHEN MORE STRINGENT. NOTIFY THE OWNER'S REPRESENTATIVE IF CONFLICTS OCCUR.

**SITE CALCULATIONS**

LOT: 36,248 SF  
 LOT COVERAGE AREA: 21,641 SF  
 BUILDING AREA: 14,086 SF

**SNOW STORAGE REQUIRMENTS**

TOTAL PAVED SURFACE	REQ. (50 SF STORAGE / 350 SF TOTAL PAVED SURFACE - 50% REDUCTION)	PROVIDED
7,375 SF	1,844 SF	1,874 SF

**COMMUNITY AMENITY REQUIREMENTS**

1. DEVELOPMENT WITH OVER 10,000 SQUARE FEET OF GROSS FLOOR AREA SHALL PROVIDE COMMUNITY SPACES, INCLUDING BUT NOT LIMITED TO, PUBLIC BENCHES, WATER FEATURES, PUBLIC KIOSK/GAZEBO, PUBLIC PATIO/SEATING AREAS, PUBLIC PLAZAS, OR PUBLIC ART.
  - 1.1. MINIMUM OF 1 COMMUNITY SPACE SHALL BE PROVIDED FOR ANY DEVELOPMENT WITH BETWEEN 10,000 AND 25,000 SQUARE FEET OF GROSS FLOOR AREA.
  - 1.2. MINIMUM OF 3 COMMUNITY SPACES SHALL BE PROVIDED FOR ANY DEVELOPMENT WITH OVER 25,000 SQUARE FEET OF GROSS FLOOR AREA.
  - 1.3. 3 COMMUNITY SPACE PROVIDED
2. ALL MIXED-USE AND NON-RESIDENTIAL DEVELOPMENT SHALL PROVIDE BICYCLE PARKING FACILITIES, IN AN APPROPRIATE LOCATION, WITH BICYCLE SPACES IN THE AMOUNT OF NOT LESS THAN 20 PERCENT OF THE TOTAL NUMBER OF PARKING SPACES REQUIRED FOR THE PROJECT, WITH A MINIMUM OF FIVE BICYCLE SPACES.
  - 2.1. 8 SPACES HAVE BEEN PROVIDED, 20% OF THE PARKING REQUIRED

**SITE LANDSCAPE REQUIREMENTS**

- REQUIREMENTS**
- 1 TREE PER 1,000 SF OF DEVELOPMENT LOT AREA (37 TREES)
  - 1 SHRUB PER 1,500 SF OF DEVELOPMENT LOT AREA (25 SHRUBS)
  - MINIMUM SPECIES DIVERSITY: 50% OF ANY ONE TREE PER 20-39 TREES

MAXIMUM LAWN AREA: 1,333 SF (10% OF UNDEVELOPED AREA OF THE SITE)  
 NO LAWN AREA PROVIDED.

**NOTES:**  
 LANDSCAPE TABLE DOES NOT INCLUDE PLANTING FOR RIGHT OF WAY IMPROVEMENTS ALONG MAIN STREET.

BREAK DOWN	REQ.	EXISTING	PROVIDED	TOTAL
TREES	37	23	19	42
DECIDUOUS		4	11	15
2" CAL (REDUCED)	50%	4	4	8
1.5" CAL (REDUCED)	50%	0	7	7
MULTI-STEM			3	3
EVERGREEN		19	8	27
10 HT.	25%	19	0	19
8' HT.	25%	0	2	2
6' HT.	50%	0	6	6
SHRUBS	25	0	25	25
DECIDUOUS	N/A	0	24	24
EVERGREEN	N/A	0	1	1
SHRUBS IN ROW	18	0	18	18
DECIDUOUS	N/A	0	16	16
EVERGREEN	N/A	0	4	4

**SUBSTITUTIONS AND AMENDMENTS**

1. FOR EVERY 1,000 SQUARE FEET OF PROJECT LOT AREA OR FRACTION THEREOF, A MINIMUM OF ONE TREE MUST BE PLANTED ON THE SITE. ONE SHRUB SHALL BE REQUIRED FOR EVERY 1,500 SQUARE FEET OF PROJECT LOT AREA OR FRACTION THEREOF.
2. EXISTING HEALTHY TREES (INCLUDING LODGE POLE PINE) MAY BE SUBSTITUTED FOR UP TO 50 PERCENT OF THE TREE LANDSCAPING REQUIRED BY THIS ARTICLE, ON A TREE-FOR-TREE BASIS, IN CASES WHERE THE EXISTING TREES MEET OR EXCEED THE STANDARDS FOR REQUIRED VEGETATION AND MINIMUM PLANT SIZES.
  - 2.1. 23 EXISTING TREES ON SITE ARE BEING COUNTED TOWARDS LANDSCAPE REQUIREMENTS.
3. ADDITIONAL LANDSCAPING, NOT TO EXCEED 20 PERCENT OF THE LANDSCAPING OTHERWISE REQUIRED MAY BE REQUIRED IN AREAS INCLUDING BUT NOT LIMITED TO:
  - 3.1. AT THE EDGES (PERIMETER) OF A PARKING LOT AND BETWEEN PARKING LOTS.
  - 3.2. BETWEEN BUILDING DEVELOPMENT AND RECREATIONAL TRAILS OR OPEN SPACE.
  - 3.3. BETWEEN MULTI-FAMILY OR COMMERCIAL DEVELOPMENT AND A ROAD OR OTHER PUBLIC SPACES.
  - 3.4. BETWEEN COMMERCIAL AND RESIDENTIAL AREAS.
  - 3.5. ON SLOPES OF OVER 15 PERCENT.
- 3.6. SITE DESIGN INCLUDES 8 TREES (20%) BETWEEN THE PROPOSED DEVELOPMENT AND THE ADJACENT RESIDENTIAL USE. PROJECT EXCEEDS MINIMUM REQUIREMENTS.

**BIKE PARKING REQUIREMENTS**

REQUIRED PARKING	REQUIRED BIKE SPACES (20%)	PROVIDED
43	9	9

**SHEET LIST**

- L-001 LANDSCAPE NOTES
- L-002 LANDSCAPE SCHEDULE
- L-003 LANDSCAPE SCHEDULE
- L-101 LANDSCAPE PLAN
- L-501 LANDSCAPE DETAILS
- L-502 LANDSCAPE DETAILS
- L-503 LANDSCAPE DETAILS
- L-504 LANDSCAPE DETAILS

**LANDSCAPE NOTES**

1. THE CONTRACTOR SHALL FOLLOW THE LANDSCAPE PLANS AND SPECIFICATIONS AS CLOSELY AS POSSIBLE. ANY SUBSTITUTION OR ALTERATION SHALL NOT BE ALLOWED WITHOUT APPROVAL OF THE OWNER'S REPRESENTATIVE. OVERALL PLANT QUANTITY AND QUALITY SHALL BE CONSISTENT WITH THE PLANS.
2. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PLANT QUANTITIES. GRAPHIC QUANTITIES TAKES PRECEDENCE OVER WRITTEN QUANTITIES.
3. THE OWNER'S REPRESENTATIVE RESERVES THE RIGHT TO INSPECT AND TAG ALL PLANT MATERIAL PRIOR TO SHIPPING TO THE SITE. IN ALL CASES, THE OWNER'S REPRESENTATIVE MAY REJECT PLANT MATERIAL AT THE SITE IF MATERIAL IS DAMAGED, DISEASED, OR DECLINING IN HEALTH AT THE TIME OF ONSITE INSPECTIONS OR IF THE PLANT MATERIAL DOES NOT MEET THE MINIMUM SPECIFIED STANDARD IDENTIFIED ON THE PLANS AND IN THE SPECIFICATIONS. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER'S REPRESENTATIVE FOR INSPECTION AND APPROVAL OF ALL MATERIALS AND PRODUCTS PRIOR TO INSTALLATION.
4. THE OWNER'S REPRESENTATIVE MAY ELECT TO UPSIZE PLANT MATERIAL AT THEIR DISCRETION BASED ON SELECTION, AVAILABILITY, OR TO ENHANCE SPECIFIC AREAS OF THE PROJECT. THE CONTRACTOR SHALL VERIFY PLANT MATERIAL SIZES WITH OWNER'S REPRESENTATIVE PRIOR TO PURCHASING, SHIPPING OR STOCKING OF PLANT MATERIALS. SUBMIT CHANGE ORDER REQUEST TO OWNER'S REPRESENTATIVE FOR APPROVAL IF ADDITIONAL COST IS REQUESTED BY THE CONTRACTOR PRIOR TO INSTALLATION. RE-STOCKING CHARGES WILL NOT BE APPROVED IF THE CONTRACTOR FAILS TO SUBMIT A REQUEST FOR MATERIAL CHANGES.
5. THE CONTRACTOR SHALL WARRANTY ALL CONTRACTED WORK AND MATERIALS FOR A PERIOD OF ONE YEAR AFTER SUBSTANTIAL COMPLETION HAS BEEN ISSUED BY THE OWNER'S REPRESENTATIVE FOR THE ENTIRE PROJECT UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS OR SPECIFICATIONS.
6. REFER TO IRRIGATION PLANS FOR LIMITS AND TYPES OF IRRIGATION DESIGNED FOR THE LANDSCAPE. IN NO CASE SHALL IRRIGATION BE EMITTED WITHIN THE MINIMUM DISTANCE FROM BUILDING OR WALL FOUNDATIONS AS STIPULATED IN THE GEOTECHNICAL REPORT. ALL IRRIGATION DISTRIBUTION LINES, HEADS AND EMITTERS SHALL BE KEPT OUTSIDE THE MINIMUM DISTANCE AWAY FROM ALL BUILDING AND WALL FOUNDATIONS AS STIPULATED IN THE GEOTECHNICAL REPORT.
7. LANDSCAPE MATERIAL LOCATIONS SHALL HAVE PRECEDENCE OVER IRRIGATION MAINLINE AND LATERAL LOCATIONS. COORDINATE INSTALLATION OF IRRIGATION EQUIPMENT SO THAT IT DOES NOT INTERFERE WITH THE PLANTING OF TREES OR OTHER LANDSCAPE MATERIAL.
8. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING POSITIVE DRAINAGE EXISTS IN ALL LANDSCAPE AREAS. SURFACE DRAINAGE ON LANDSCAPE AREAS SHALL NOT FLOW TOWARD STRUCTURES AND FOUNDATIONS. MAINTAIN SLOPE AWAY FROM FOUNDATIONS PER THE GEOTECHNICAL REPORT RECOMMENDATIONS. ALL LANDSCAPE AREAS BETWEEN WALKS AND CURBS SHALL DRAIN FREELY TO THE CURB UNLESS OTHERWISE IDENTIFIED ON THE GRADING PLAN. IN NO CASE SHALL THE GRADE, TURF THATCH, OR OTHER LANDSCAPE MATERIALS DAM WATER AGAINST WALKS. MINIMUM SLOPES ON LANDSCAPE AREAS SHALL BE 2%; MAXIMUM SLOPE SHALL BE 25% UNLESS SPECIFICALLY IDENTIFIED ON THE PLANS OR APPROVED BY THE OWNER'S REPRESENTATIVE.
9. PRIOR TO INSTALLATION OF PLANT MATERIALS, AREAS THAT HAVE BEEN COMPACTED OR DISTURBED BY CONSTRUCTION ACTIVITY SHALL BE THOROUGHLY LOOSENEED TO A DEPTH OF 8" - 12" AND AMENDED PER SPECIFICATIONS.
10. ALL LANDSCAPED AREAS ARE TO RECEIVE ORGANIC SOIL PREPARATION AT 3.5 cu.yrds/1,000sf OR AS NOTED IN THE TECHNICAL SPECIFICATIONS.
11. TREES SHALL NOT BE LOCATED IN DRAINAGE SWALES, DRAINAGE AREAS, OR UTILITY EASEMENTS. CONTACT OWNER'S REPRESENTATIVE FOR RELOCATION OF PLANTS IN QUESTIONABLE AREAS PRIOR TO INSTALLATION.
12. THE CENTER OF EVERGREEN TREES SHALL NOT BE PLACED CLOSER THAN 8' AND THE CENTER OF ORNAMENTAL TREES CLOSER THAN 6' FROM A SIDEWALK, STREET OR DRIVE LANE. EVERGREEN TREES SHALL NOT BE LOCATED ANY CLOSER THAN 15' FROM IRRIGATION ROTOR HEADS. NOTIFY OWNER'S REPRESENTATIVE IF TREE LOCATIONS CONFLICT WITH THESE STANDARDS FOR FURTHER DIRECTION.
13. ALL EVERGREEN TREES SHALL BE FULLY BRANCHED TO THE GROUND AND SHALL NOT EXHIBIT SIGNS OF ACCELERATED GROWTH AS DETERMINED BY THE OWNER'S REPRESENTATIVE.
14. ALL TREES ARE TO BE STAKED AND GUYED PER DETAILS FOR A PERIOD OF 3 YEARS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING STAKES AT THE END OF 3 YEARS FROM ACCEPTANCE OF LANDSCAPE INSTALLATION BY THE OWNER'S REPRESENTATIVE. OBTAIN APPROVAL BY OWNER'S REPRESENTATIVE PRIOR TO REMOVAL.
15. ALL TREES INSTALLED ABOVE RETAINING WALLS UTILIZING GEO-GRID MUST BE HAND DUG TO PROTECT GEO-GRID. IF GEO-GRID MUST BE CUT TO INSTALL TREES, APPROVAL MUST BE GIVEN BY OWNER'S REPRESENTATIVE PRIOR TO DOING WORK.
16. ALL TREES IN SEED OR TURF AREAS SHALL RECEIVE MULCH RINGS. OBTAIN APPROVAL FROM OWNER'S REPRESENTATIVE FOR ANY TREES THAT WILL NOT BE MULCHED FOR EXCESSIVE MOISTURE REASONS.
17. SHRUB, GROUND COVER AND PERENNIAL BEDS ARE TO BE CONTAINED BY PLASTIC EDGER AS CALLED OUT IN THE MATERIAL SCHEDULE AND SPECIFICATIONS. EDGER IS NOT REQUIRED WHEN ADJACENT TO CURBS, WALLS, WALKS OR SOLID FENCES WITHIN 3" OF PRE-MULCHED FINAL GRADE. EDGER SHALL NOT BE REQUIRED TO SEPARATE MULCH TYPES UNLESS SPECIFIED ON THE PLANS.
18. ALL SHRUB BEDS ARE TO BE MULCHED WITH MIN. 3" DEPTH, SPECIFIED LANDSCAPE MULCH OVER SPECIFIED GEOTEXTILE WEED CONTROL FABRIC. ALL GROUND COVER AND PERENNIAL FLOWER BEDS SHALL BE MULCHED WITH 3" DEPTH SPECIFIED LANDSCAPE MULCH. NO WEED CONTROL FABRIC IS REQUIRED IN GROUND COVER OR PERENNIAL AREAS.
19. AT SEED AREA BOUNDARIES ADJACENT TO EXISTING NATIVE AREAS, OVERLAP ABUTTING NATIVE AREAS BY THE FULL WIDTH OF THE SEEDER.
20. EXISTING TURF AREAS THAT ARE DISTURBED DURING CONSTRUCTION, ESTABLISHMENT AND THE MAINTENANCE PERIOD SHALL BE RESTORED WITH NEW SOD TO MATCH EXISTING TURF SPECIES. DISTURBED NATIVE AREAS WHICH ARE TO REMAIN SHALL BE OVER SEEDED AND RESTORED WITH SPECIFIED SEED MIX.
21. CONTRACTOR SHALL OVER SEED ALL MAINTENANCE OR SERVICE ACCESS BENCHES AND ROADS WITH SPECIFIED SEED MIX UNLESS OTHERWISE NOTED ON THE PLANS.
22. ALL SEEDED SLOPES EXCEEDING 25% IN GRADE (4:1) SHALL RECEIVE EROSION CONTROL BLANKETS. PRIOR TO INSTALLATION, NOTIFY OWNER'S REPRESENTATIVE FOR APPROVAL OF LOCATION AND ANY ADDITIONAL COST IF A CHANGE ORDER IS NECESSARY.
23. WHEN COMPLETE, ALL GRADES SHALL BE WITHIN +/- 1/8" OF FINISHED GRADES AS SHOWN ON THE PLANS.
24. SOFT SURFACE TRAILS NEXT TO MANICURED TURF OR SHRUB BEDS SHALL BE CONTAINED WITH PLASTIC EDGER AS SPECIFIED IN THE MATERIAL SCHEDULE AND SPECIFICATIONS.

CHECKED BY: ARMIT  
DRAWN BY: EGIAR

OWNER:  
 NHP FOUNDATION

122 EAST 42ND STREET  
 SUITE 4900  
 NEW YORK, NY 10168

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SHEET TITLE:  
**LANDSCAPE NOTES**

PLANT SCHEDULE

SYMBOL	CODE	QTY	BOTANICAL NAME	COMMON NAME	ROOT	SIZE	FIELD6
<u>DECIDUOUS TREES</u>							
	PO AC	4	POPULUS TREMULOIDES	QUAKING ASPEN	B & B	1.5" CAL	MULTI-STEM
	PO TE	30	POPULUS TREMULOIDES	QUAKING ASPEN	B & B	2" CAL.	
<u>EVERGREEN TREES</u>							
	PI PU	9	PICEA PUNGENS	COLORADO SPRUCE	B & B	8' HT.	
	PI AR	10	PINUS ARISTATA	BRISTLEcone PINE	B & B	6' HT.	
<u>ORNAMENTAL TREES</u>							
	MA SS	3	MALUS X 'SPRING SNOW'	SPRING SNOW CRAB APPLE	B & B	1.5" CAL	

SYMBOL	CODE	QTY	BOTANICAL NAME	COMMON NAME	ROOT	SIZE
<u>DECIDUOUS SHRUBS</u>						
	AM AL	4	AMELANCHIER ALNIFOLIA	SASKATOON SERVICEBERRY	CONT.	#5
	PH MO	12	PHYSOCARPUS MONOGYNUS	MOUNTAIN NINEBARK	CONT.	#5
	PR CH	7	PRUNUS X VIRGINIANA 'P002'	SUCKER PUNCH CANADA CHOKECHERRY	CONT.	#5
	RO WO	6	ROSA WOODSII	MOUNTAIN ROSE	CONT.	#5
	SY MI	9	SYRINGA PATULA 'MISS KIM'	MISS KIM LILAC	CONT.	#5
<u>EVERGREEN SHRUBS</u>						
	CO CO	5	COTONEASTER DAMMERI 'CORAL BEAUTY'	CORAL BEAUTY COTONEASTER	CONT.	#5
<u>ORNAMENTAL GRASSES</u>						
	CA KA	36	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	KARL FOERSTER FEATHER REED GRASS	CONT.	#1
	SP HE	30	SPOROBOLUS HETEROLEPIS	PRAIRIE DROPSEED	CONT.	#1

SHORT DRY GRASS SEED MIXTURE

COMMON NAME	% OF TOTAL	LBS. PER 1,000 S.F.
HARD FESCUE, VNS	30%	0.6 LBS.
CREEPING RED FESCUE, VNS	30%	0.6 LBS.
SHEEP FESCUE, VNS	25%	0.5 LBS.
CANADA BLUEGRASS, RUBENS	10%	0.2 LBS.
CANBY BLUGRASS, CANBAR	5%	0.1 LBS.
TOTAL	100%	2.0 LBS.

- SLOPES OVER 3:1 SHALL BE HAYED AND TACKIFIED OR NETTED
- SPREAD SEED AT A RATE OF 2 LBS PER 1,000 SF

CHECKED BY: ARMIT  
DRAWN BY: EGAR

101 WEST MAIN  
101 WEST MAIN  
FRISCO, COLORADO  
SKETCH PLAN

OWNER:  
NHP FOUNDATION  
122 EAST 42ND STREET  
SUITE 4900  
NEW YORK, NY 10168

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DATE:  
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05/31/24 MSP

SHEET TITLE:  
LANDSCAPE NOTES

**GENERAL NOTES**

1. THE CONTRACTOR AND OWNER'S REPRESENTATIVE SHALL CONTACT THE LANDSCAPE ARCHITECT FOR A PRE-CONSTRUCTION MEETING PRIOR TO START OF ANY WORK SHOWN ON THESE PLANS.
2. THESE PLANS SHALL NOT BE UTILIZED FOR CONSTRUCTION OR PERMITTING UNLESS STATED FOR SUCH USE IN THE TITLE BLOCK.
3. DRAWINGS ARE INTENDED TO BE PRINTED ON 24" X 36" PAPER. PRINTING THESE DRAWINGS AT A DIFFERENT SIZE WILL IMPACT THE SCALE. VERIFY THE GRAPHIC SCALE BEFORE REFERENCING ANY MEASUREMENTS ON THESE SHEETS. THE RECIPIENT OF THESE DRAWINGS SHALL BE RESPONSIBLE FOR ANY ERRORS RESULTING FROM INCORRECT PRINTING, COPYING, OR ANY OTHER CHANGES THAT ALTER THE SCALE OF THE DRAWINGS.
4. VERIFY ALL PLAN DIMENSIONS PRIOR TO START OF CONSTRUCTION. NOTIFY THE OWNER'S REPRESENTATIVE TO ADDRESS ANY QUESTIONS OR CLARIFY ANY DISCREPANCIES.
5. WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.
6. REVISIONS TO THESE DOCUMENTS, REVIEW AND CONSIDERATIONS OF SUBSTITUTIONS, OWNER DIRECTED CHANGES, AND/OR RFI RESPONSES WHICH REQUIRE PROVIDING ADDITIONAL DETAIL AFTER APPROVAL OF THE PERMIT SET MAY REQUIRE APPROVAL OF AN ADDITIONAL SERVICES REQUEST BY THE CLIENT.
7. SUBMIT A CHANGE ORDER FOR APPROVAL FOR ANY CHANGES TO WORK SCOPE RESULTING FROM FIELD CONDITIONS OR DIRECTION BY OWNER'S REPRESENTATIVE WHICH REQUIRE ADDITIONAL COST TO THE OWNER PRIOR TO PERFORMANCE OF WORK.
8. THE CONTRACTOR SHALL PROVIDE A STAKED LAYOUT OF ALL SITE IMPROVEMENTS FOR INSPECTION BY THE OWNER'S REPRESENTATIVE AND MAKE MODIFICATIONS AS REQUIRED. ALL LAYOUT INFORMATION IS AVAILABLE IN DIGITAL FORMAT FOR USE BY THE CONTRACTOR.
9. IF A GEOTECHNICAL SOILS REPORT IS NOT AVAILABLE AT THE TIME OF CONSTRUCTION, NORRIS DESIGN RECOMMENDS A REPORT BE AUTHORIZED BY THE OWNER AND THAT ALL RECOMMENDATIONS OF THE REPORT ARE FOLLOWED DURING CONSTRUCTION. THE CONTRACTOR SHALL USE THESE CONTRACT DOCUMENTS AS A BASIS FOR THE BID. IF THE OWNER ELECTS TO PROVIDE A GEOTECHNICAL REPORT, THE CONTRACTOR SHALL REVIEW THE REPORT AND SUBMIT AN APPROPRIATE CHANGE ORDER TO THE OWNER'S REPRESENTATIVE IF ADDITIONAL COSTS ARE REQUESTED.
10. CONTRACTOR SHALL CONFIRM THAT SITE CONDITIONS ARE SIMILAR TO THE PLANS, WITHIN TOLERANCES STATED IN THE CONTRACT DOCUMENTS, AND SATISFACTORY TO THE CONTRACTOR PRIOR TO START OF WORK. SHOULD SITE CONDITIONS BE DIFFERENT THAN REPRESENTED ON THE PLANS OR UNSATISFACTORY TO THE CONTRACTOR, THE CONTRACTOR SHALL CONTACT THE OWNER'S REPRESENTATIVE FOR CLARIFICATION AND FURTHER DIRECTION.
11. CONTRACTOR IS RESPONSIBLE TO PAY FOR, AND OBTAIN, ANY REQUIRED APPLICATIONS, PERMITTING, LICENSES, INSPECTIONS AND METERS ASSOCIATED WITH WORK.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO ANY VIOLATIONS OR NON-CONFORMANCE WITH THE PLANS, SPECIFICATIONS, CONTRACT DOCUMENTS, JURISDICTIONAL CODES, AND REGULATORY AGENCIES.
13. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF ALL UTILITY LOCATES PRIOR TO ANY EXCAVATION. REFER TO ENGINEERING UTILITY PLANS FOR ALL PROPOSED UTILITY LOCATIONS AND DETAILS. NOTIFY OWNER'S REPRESENTATIVE IF EXISTING OR PROPOSED UTILITIES INTERFERE WITH THE ABILITY TO PERFORM WORK.
14. UNLESS IDENTIFIED ON THE PLANS FOR DEMOLITION OR REMOVAL, THE CONTRACTOR IS RESPONSIBLE FOR THE COST TO REPAIR UTILITIES, ADJACENT OR EXISTING LANDSCAPE, ADJACENT OR EXISTING PAVING, OR ANY PUBLIC AND PRIVATE PROPERTY THAT IS DAMAGED BY THE CONTRACTOR OR THEIR SUBCONTRACTOR'S OPERATIONS DURING INSTALLATION, ESTABLISHMENT OR DURING THE SPECIFIED MAINTENANCE PERIOD. ALL DAMAGES SHALL BE REPAIRED TO PRE-CONSTRUCTION CONDITIONS AS DETERMINED BY THE OWNER'S

- REPRESENTATIVE. CONTRACTOR SHALL BE RESPONSIBLE FOR LOGGING ANY DAMAGES PRIOR TO START OF CONSTRUCTION AND DURING THE CONTRACT PERIOD.
15. ALL WORK SHALL BE CONFINED TO THE AREA WITHIN THE CONSTRUCTION LIMITS AS SHOWN ON THE PLANS. ANY AREAS OR IMPROVEMENTS DISTURBED OUTSIDE THESE LIMITS SHALL BE RETURNED TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE. IN THE EVENT THE CONTRACTOR REQUIRES A MODIFICATION TO THE CONSTRUCTION LIMITS, WRITTEN PERMISSION MUST BE OBTAINED FROM THE OWNER'S REPRESENTATIVE PRIOR TO ANY DISTURBANCE OUTSIDE OF THE LIMITS OF WORK.
  16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REPAIR OF ANY OF THEIR TRENCHES OR EXCAVATIONS THAT SETTLE.
  17. THE CONTRACTOR SHALL BE RESPONSIBLE TO PREPARE AND SUBMIT A TRAFFIC CONTROL PLAN TO THE APPROPRIATE JURISDICTIONAL AGENCIES AND THE OWNER'S REPRESENTATIVE IF THEIR WORK AND OPERATIONS AFFECT OR IMPACT THE PUBLIC RIGHTS-OF-WAY. OBTAIN APPROVAL PRIOR TO ANY WORK WHICH AFFECTS OR IMPACTS THE PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THIS REQUIREMENT DURING THE CONTRACT PERIOD.
  18. SIGHT TRIANGLES AND SIGHT LINES SHALL REMAIN UNOBTSTRUCTED BY EQUIPMENT, CONSTRUCTION MATERIALS, PLANT MATERIAL OR ANY OTHER VISUAL OBSTACLE DURING THE CONTRACT PERIOD AND AT MATURITY OF PLANTS PER LOCAL JURISDICTIONAL REQUIREMENTS.
  19. NO PLANT MATERIAL OTHER THAN GROUND COVER IS ALLOWED TO BE PLANTED ADJACENT TO FIRE HYDRANTS AS STIPULATED BY JURISDICTIONAL REQUIREMENTS.
  20. COORDINATE SITE ACCESS, STAGING, STORAGE AND CLEANOUT AREAS WITH OWNER'S REPRESENTATIVE.
  21. CONTRACTOR IS RESPONSIBLE FOR PROVIDING TEMPORARY SAFETY FENCING AND BARRIERS AROUND ALL IMPROVEMENTS SUCH AS WALLS, PLAY STRUCTURES, EXCAVATIONS, ETC. ASSOCIATED WITH THEIR WORK UNTIL SUCH FACILITIES ARE COMPLETELY INSTALLED PER THE PLANS, SPECIFICATIONS AND MANUFACTURER'S RECOMMENDATIONS.
  22. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTION OF THEIR MATERIAL STOCK PILES AND WORK FROM VANDALISM, EROSION OR UNINTENDED DISTURBANCE DURING THE CONSTRUCTION PERIOD AND UNTIL FINAL ACCEPTANCE IS ISSUED.
  23. THE CONTRACTOR SHALL KNOW, UNDERSTAND AND ABIDE BY ANY STORM WATER POLLUTION PREVENTION PLAN (SWPPP) ASSOCIATED WITH THE SITE. IF A STORM WATER POLLUTION PREVENTION PLAN IS NOT PROVIDED BY THE OWNER'S REPRESENTATIVE, REQUEST A COPY BEFORE PERFORMANCE OF ANY SITE WORK.
  24. MAINTAIN ANY STORM WATER MANAGEMENT FACILITIES THAT EXIST ON SITE FOR FULL FUNCTIONALITY. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ANY NEW STORM WATER MANAGEMENT FACILITIES THAT ARE IDENTIFIED IN THE SCOPE OF WORK TO FULL FUNCTIONALITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER FOR FAILURE TO MAINTAIN STORM WATER MANAGEMENT FACILITIES DURING THE CONTRACT PERIOD.
  25. THE CONTRACTOR SHALL PREVENT SEDIMENT, DEBRIS AND ALL OTHER POLLUTANTS FROM EXITING THE SITE OR ENTERING THE STORM SEWER SYSTEM DURING ALL DEMOLITION OR CONSTRUCTION OPERATIONS THAT ARE PART OF THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THESE REQUIREMENTS DURING THEIR CONTRACTED COURSE OF WORK.
  26. THE CONTRACTOR SHALL BE RESPONSIBLE TO PREVENT ANY IMPACTS TO ADJACENT WATERWAYS, WETLANDS, OR OTHER ENVIRONMENTALLY SENSITIVE AREAS RESULTING FROM WORK DONE AS PART OF THIS PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THESE STANDARDS DURING THEIR CONTRACTED COURSE OF WORK.

27. THE CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS SHALL INSURE THAT ALL LOADS OF CONSTRUCTION MATERIAL IMPORTED TO OR EXPORTED FROM THE PROJECT SITE SHALL BE PROPERLY COVERED TO PREVENT LOSS OF MATERIAL DURING TRANSPORT. TRANSPORTATION METHODS ON PUBLIC RIGHT-OF-WAYS SHALL CONFORM TO JURISDICTIONAL REQUIREMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY FINES OR PENALTIES ASSESSED TO THE OWNER RELATING TO THESE REQUIREMENTS.
28. THE CLEANING OF EQUIPMENT IS PROHIBITED AT THE JOB SITE UNLESS AUTHORIZED BY THE OWNER'S REPRESENTATIVE IN A DESIGNATED AREA. THE DISCHARGE OF WATER, WASTE CONCRETE, POLLUTANTS, OR OTHER MATERIALS SHALL ONLY OCCUR IN AREAS DESIGNED FOR SUCH USE AND APPROVED BY THE OWNER'S REPRESENTATIVE.
29. THE CLEANING OF CONCRETE EQUIPMENT IS PROHIBITED AT THE JOB SITE EXCEPT IN DESIGNATED CONCRETE WASHOUT AREAS. THE DISCHARGE OF WATER CONTAINING WASTE CONCRETE IN THE STORM SEWER IS PROHIBITED.
30. THE USE OF REBAR, STEEL STAKES, OR STEEL FENCE POSTS TO STAKE DOWN STRAW OR HAY BALES OR TO SUPPORT SILT FENCING USED AS AN EROSION CONTROL MEASURE IS PROHIBITED.
31. OPEN SPACE SWALES: IF SWALES ARE EXISTING ON SITE AND ARE NOT INTENDED TO BE MODIFIED AS PART OF THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN THE CONVEYANCE OF WATER WITHIN THE SWALES DURING THE CONTRACT PERIOD. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DIVERSION OR PUMPING OF WATER IF REQUIRED TO COMPLETE WORK. ANY SWALES DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED/RESTORED TO THEIR ORIGINAL CONDITION. IF THE SWALE NEEDS TO BE DISTURBED OR MODIFIED FOR ANY REASON, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE FOR APPROVAL PRIOR TO DISTURBANCE.
32. DETENTION AND WATER QUALITY PONDS: IF DETENTION PONDS AND WATER QUALITY PONDS ARE EXISTING ON SITE AND ARE NOT INTENDED TO BE MODIFIED AS PART OF THE PLANS, THE CONTRACTOR SHALL MINIMIZE DISTURBANCE TO THE PONDS, DRAINAGE STRUCTURES AND SPILLWAYS DURING CONSTRUCTION. ALL PONDS, DRAINAGE STRUCTURES AND SPILLWAYS SHALL BE MAINTAINED IN OPERABLE CONDITIONS AT ALL TIMES. ANY POND OR SPILLWAY AREAS DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED/RESTORED TO THEIR ORIGINAL CONDITION. IF THE POND NEEDS TO BE DISTURBED OR MODIFIED FOR ANY REASON, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE FOR APPROVAL PRIOR TO DISTURBANCE.
33. MAINTENANCE ACCESS BENCHES: IF MAINTENANCE BENCHES OR ACCESS ROADS EXIST ON SITE AND ARE NOT INTENDED TO BE MODIFIED AS PART OF THE PLANS, THE CONTRACTOR SHALL MINIMIZE DISTURBANCE TO THE BENCHES OR ACCESS ROADS DURING CONSTRUCTION. ANY BENCHES OR ACCESS ROADS DISTURBED BY THE CONTRACTOR SHALL BE REPAIRED/RESTORED TO THEIR ORIGINAL CONDITION. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL EXISTING BENCHES AND ACCESS ROADS DURING THE CONSTRUCTION PERIOD. IF ACCESS NEEDS TO BE BLOCKED FOR ANY REASON, THE CONTRACTOR SHALL NOTIFY THE OWNER'S REPRESENTATIVE FOR APPROVAL PRIOR TO INTERRUPTION OF ACCESS.
34. LOCAL, STATE AND FEDERAL JURISDICTIONAL REQUIREMENTS, RESTRICTIONS OR PROCEDURES SHALL SUPERSEDE THESE PLANS, NOTES AND SPECIFICATIONS WHEN MORE STRINGENT. NOTIFY THE OWNER'S REPRESENTATIVE IF CONFLICTS OCCUR.

**SITE CALCULATIONS**

LOT:	36,248 SF
LOT COVERAGE AREA:	21,641 SF
BUILDING AREA:	14,086 SF

**SNOW STORAGE REQUIRMENTS**

TOTAL PAVED SURFACE	REQ. (50 SF STORAGE / 350 SF TOTAL PAVED SURFACE - 50% REDUCTION)	PROVIDED
7,375 SF	1,844 SF	1,874 SF

**COMMUNITY AMENITY REQUIREMENTS**

1. DEVELOPMENT WITH OVER 10,000 SQUARE FEET OF GROSS FLOOR AREA SHALL PROVIDE COMMUNITY SPACES, INCLUDING BUT NOT LIMITED TO, PUBLIC BENCHES, WATER FEATURES, PUBLIC KIOSK/GAZEBO, PUBLIC PATIO/SEATING AREAS, PUBLIC PLAZAS, OR PUBLIC ART.
  - 1.1. MINIMUM OF 1 COMMUNITY SPACE SHALL BE PROVIDED FOR ANY DEVELOPMENT WITH BETWEEN 10,000 AND 25,000 SQUARE FEET OF GROSS FLOOR AREA.
  - 1.2. MINIMUM OF 3 COMMUNITY SPACES SHALL BE PROVIDED FOR ANY DEVELOPMENT WITH OVER 25,000 SQUARE FEET OF GROSS FLOOR AREA.
  - 1.3. 3 COMMUNITY SPACE PROVIDED
2. ALL MIXED-USE AND NON-RESIDENTIAL DEVELOPMENT SHALL PROVIDE BICYCLE PARKING FACILITIES, IN AN APPROPRIATE LOCATION, WITH BICYCLE SPACES IN THE AMOUNT OF NOT LESS THAN 20 PERCENT OF THE TOTAL NUMBER OF PARKING SPACES REQUIRED FOR THE PROJECT, WITH A MINIMUM OF FIVE BICYCLE SPACES.
  - 2.1. 8 SPACES HAVE BEEN PROVIDED, 20% OF THE PARKING REQUIRED

**SITE LANDSCAPE REQUIREMENTS**

**REQUIREMENTS**

- 1 TREE PER 1,000 SF OF DEVELOPMENT LOT AREA (37 TREES)
- 1 SHRUB PER 1,500 SF OF DEVELOPMENT LOT AREA (25 SHRUBS)
- MINIMUM SPECIES DIVERSITY: 50% OF ANY ONE TREE PER 20-39 TREES

MAXIMUM LAWN AREA: 1,333 SF (10% OF UNDEVELOPED AREA OF THE SITE)  
 NO LAWN AREA PROVIDED.

**NOTES:**

LANDSCAPE TABLE DOES NOT INCLUDE PLANTING FOR RIGHT OF WAY IMPROVEMENTS ALONG MAIN STREET.

BREAK DOWN	REQ.	EXISTING	PROVIDED	TOTAL
TREES	37	23	19	42
DECIDUOUS		4	11	15
2" CAL (REDUCED)	50%	4	4	8
1.5" CAL (REDUCED)	50%	0	7	7
MULTI-STEM			3	3
EVERGREEN		19	8	27
10 HT.	25%	19	0	19
8' HT.	25%	0	2	2
6' HT.	50%	0	6	6
SHRUBS	25	0	25	25
DECIDUOUS	N/A	0	24	24
EVERGREEN	N/A	0	1	1
SHRUBS IN ROW	18	0	18	18
DECIDUOUS	N/A	0	16	16
EVERGREEN	N/A	0	4	4

**SUBSTITUTIONS AND AMENDMENTS**

1. FOR EVERY 1,000 SQUARE FEET OF PROJECT LOT AREA OR FRACTION THEREOF, A MINIMUM OF ONE TREE MUST BE PLANTED ON THE SITE. ONE SHRUB SHALL BE REQUIRED FOR EVERY 1,500 SQUARE FEET OF PROJECT LOT AREA OR FRACTION THEREOF.
  - 2.1. 23 EXISTING TREES ON SITE ARE BEING COUNTED TOWARDS LANDSCAPE REQUIREMENTS.
2. EXISTING HEALTHY TREES (INCLUDING LODGE POLE PINE) MAY BE SUBSTITUTED FOR UP TO 50 PERCENT OF THE TREE LANDSCAPING REQUIRED BY THIS ARTICLE, ON A TREE-FOR-TREE BASIS, IN CASES WHERE THE EXISTING TREES MEET OR EXCEED THE STANDARDS FOR REQUIRED VEGETATION AND MINIMUM PLANT SIZES.
  - 2.1. 23 EXISTING TREES ON SITE ARE BEING COUNTED TOWARDS LANDSCAPE REQUIREMENTS.
3. ADDITIONAL LANDSCAPING, NOT TO EXCEED 20 PERCENT OF THE LANDSCAPING OTHERWISE REQUIRED MAY BE REQUIRED IN AREAS INCLUDING BUT NOT LIMITED TO:
  - 3.1. AT THE EDGES (PERIMETER) OF A PARKING LOT AND BETWEEN PARKING LOTS.
  - 3.2. BETWEEN BUILDING DEVELOPMENT AND RECREATIONAL TRAILS OR OPEN SPACE.
  - 3.3. BETWEEN MULTI-FAMILY OR COMMERCIAL DEVELOPMENT AND A ROAD OR OTHER PUBLIC SPACES.
  - 3.4. BETWEEN COMMERCIAL AND RESIDENTIAL AREAS.
  - 3.5. ON SLOPES OF OVER 15 PERCENT.
- 3.6. SITE DESIGN INCLUDES 8 TREES (20%) BETWEEN THE PROPOSED DEVELOPMENT AND THE ADJACENT RESIDENTIAL USE. PROJECT EXCEEDS MINIMUM REQUIREMENTS.

**BIKE PARKING REQUIREMENTS**

REQUIRED PARKING	REQUIRED BIKE SPACES (20%)	PROVIDED
43	9	9

**SHEET LIST**

- L-001 LANDSCAPE NOTES
- L-002 LANDSCAPE SCHEDULE
- L-003 LANDSCAPE SCHEDULE
- L-101 LANDSCAPE PLAN
- L-501 LANDSCAPE DETAILS
- L-502 LANDSCAPE DETAILS
- L-503 LANDSCAPE DETAILS
- L-504 LANDSCAPE DETAILS

**LANDSCAPE NOTES**

1. THE CONTRACTOR SHALL FOLLOW THE LANDSCAPE PLANS AND SPECIFICATIONS AS CLOSELY AS POSSIBLE. ANY SUBSTITUTION OR ALTERATION SHALL NOT BE ALLOWED WITHOUT APPROVAL OF THE OWNER'S REPRESENTATIVE. OVERALL PLANT QUANTITY AND QUALITY SHALL BE CONSISTENT WITH THE PLANS.
2. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING ALL PLANT QUANTITIES. GRAPHIC QUANTITIES TAKES PRECEDENCE OVER WRITTEN QUANTITIES.
3. THE OWNER'S REPRESENTATIVE RESERVES THE RIGHT TO INSPECT AND TAG ALL PLANT MATERIAL PRIOR TO SHIPPING TO THE SITE. IN ALL CASES, THE OWNER'S REPRESENTATIVE MAY REJECT PLANT MATERIAL AT THE SITE IF MATERIAL IS DAMAGED, DISEASED, OR DECLINING IN HEALTH AT THE TIME OF ONSITE INSPECTIONS OR IF THE PLANT MATERIAL DOES NOT MEET THE MINIMUM SPECIFIED STANDARD IDENTIFIED ON THE PLANS AND IN THE SPECIFICATIONS. THE CONTRACTOR SHALL COORDINATE WITH THE OWNER'S REPRESENTATIVE FOR INSPECTION AND APPROVAL OF ALL MATERIALS AND PRODUCTS PRIOR TO INSTALLATION.
4. THE OWNER'S REPRESENTATIVE MAY ELECT TO UPSIZE PLANT MATERIAL AT THEIR DISCRETION BASED ON SELECTION, AVAILABILITY, OR TO ENHANCE SPECIFIC AREAS OF THE PROJECT. THE CONTRACTOR SHALL VERIFY PLANT MATERIAL SIZES WITH OWNER'S REPRESENTATIVE PRIOR TO PURCHASING, SHIPPING OR STOCKING OF PLANT MATERIALS. SUBMIT CHANGE ORDER REQUEST TO OWNER'S REPRESENTATIVE FOR APPROVAL IF ADDITIONAL COST IS REQUESTED BY THE CONTRACTOR PRIOR TO INSTALLATION. RE-STOCKING CHARGES WILL NOT BE APPROVED IF THE CONTRACTOR FAILS TO SUBMIT A REQUEST FOR MATERIAL CHANGES.
5. THE CONTRACTOR SHALL WARRANTY ALL CONTRACTED WORK AND MATERIALS FOR A PERIOD OF ONE YEAR AFTER SUBSTANTIAL COMPLETION HAS BEEN ISSUED BY THE OWNER'S REPRESENTATIVE FOR THE ENTIRE PROJECT UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS OR SPECIFICATIONS.
6. REFER TO IRRIGATION PLANS FOR LIMITS AND TYPES OF IRRIGATION DESIGNED FOR THE LANDSCAPE. IN NO CASE SHALL IRRIGATION BE EMITTED WITHIN THE MINIMUM DISTANCE FROM BUILDING OR WALL FOUNDATIONS AS STIPULATED IN THE GEOTECHNICAL REPORT. ALL IRRIGATION DISTRIBUTION LINES, HEADS AND EMITTERS SHALL BE KEPT OUTSIDE THE MINIMUM DISTANCE AWAY FROM ALL BUILDING AND WALL FOUNDATIONS AS STIPULATED IN THE GEOTECHNICAL REPORT.
7. LANDSCAPE MATERIAL LOCATIONS SHALL HAVE PRECEDENCE OVER IRRIGATION MAINLINE AND LATERAL LOCATIONS. COORDINATE INSTALLATION OF IRRIGATION EQUIPMENT SO THAT IT DOES NOT INTERFERE WITH THE PLANTING OF TREES OR OTHER LANDSCAPE MATERIAL.
8. THE LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING POSITIVE DRAINAGE EXISTS IN ALL LANDSCAPE AREAS. SURFACE DRAINAGE ON LANDSCAPE AREAS SHALL NOT FLOW TOWARD STRUCTURES AND FOUNDATIONS. MAINTAIN SLOPE AWAY FROM FOUNDATIONS PER THE GEOTECHNICAL REPORT RECOMMENDATIONS. ALL LANDSCAPE AREAS BETWEEN WALKS AND CURBS SHALL DRAIN FREELY TO THE CURB UNLESS OTHERWISE IDENTIFIED ON THE GRADING PLAN. IN NO CASE SHALL THE GRADE, TURF THATCH, OR OTHER LANDSCAPE MATERIALS DAM WATER AGAINST WALKS. MINIMUM SLOPES ON LANDSCAPE AREAS SHALL BE 2%; MAXIMUM SLOPE SHALL BE 25% UNLESS SPECIFICALLY IDENTIFIED ON THE PLANS OR APPROVED BY THE OWNER'S REPRESENTATIVE.
9. PRIOR TO INSTALLATION OF PLANT MATERIALS, AREAS THAT HAVE BEEN COMPACTED OR DISTURBED BY CONSTRUCTION ACTIVITY SHALL BE THOROUGHLY LOOSENED TO A DEPTH OF 8" - 12" AND AMENDED PER SPECIFICATIONS.
10. ALL LANDSCAPED AREAS ARE TO RECEIVE ORGANIC SOIL PREPARATION AT 3.5 cu.yrds/1,000sf OR AS NOTED IN THE TECHNICAL SPECIFICATIONS.
11. TREES SHALL NOT BE LOCATED IN DRAINAGE SWALES, DRAINAGE AREAS, OR UTILITY EASEMENTS. CONTACT OWNER'S REPRESENTATIVE FOR RELOCATION OF PLANTS IN QUESTIONABLE AREAS PRIOR TO INSTALLATION.
12. THE CENTER OF EVERGREEN TREES SHALL NOT BE PLACED CLOSER THAN 8' AND THE CENTER OF ORNAMENTAL TREES CLOSER THAN 6' FROM A SIDEWALK, STREET OR DRIVE LANE. EVERGREEN TREES SHALL NOT BE LOCATED ANY CLOSER THAN 15' FROM IRRIGATION ROTOR HEADS. NOTIFY OWNER'S REPRESENTATIVE IF TREE LOCATIONS CONFLICT WITH THESE STANDARDS FOR FURTHER DIRECTION.
13. ALL EVERGREEN TREES SHALL BE FULLY BRANCHED TO THE GROUND AND SHALL NOT EXHIBIT SIGNS OF ACCELERATED GROWTH AS DETERMINED BY THE OWNER'S REPRESENTATIVE.
14. ALL TREES ARE TO BE STAKED AND GUYED PER DETAILS FOR A PERIOD OF 3 YEARS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING STAKES AT THE END OF 3 YEARS FROM ACCEPTANCE OF LANDSCAPE INSTALLATION BY THE OWNER'S REPRESENTATIVE. OBTAIN APPROVAL BY OWNER'S REPRESENTATIVE PRIOR TO REMOVAL.
15. ALL TREES INSTALLED ABOVE RETAINING WALLS UTILIZING GEO-GRID MUST BE HAND DUG TO PROTECT GEO-GRID. IF GEO-GRID MUST BE CUT TO INSTALL TREES, APPROVAL MUST BE GIVEN BY OWNER'S REPRESENTATIVE PRIOR TO DOING WORK.
16. ALL TREES IN SEED OR TURF AREAS SHALL RECEIVE MULCH RINGS. OBTAIN APPROVAL FROM OWNER'S REPRESENTATIVE FOR ANY TREES THAT WILL NOT BE MULCHED FOR EXCESSIVE MOISTURE REASONS.
17. SHRUB, GROUNDCOVER AND PERENNIAL BEDS ARE TO BE CONTAINED BY PLASTIC EDGER AS CALLED OUT IN THE MATERIAL SCHEDULE AND SPECIFICATIONS. EDGER IS NOT REQUIRED WHEN ADJACENT TO CURBS, WALLS, WALKS OR SOLID FENCES WITHIN 3' OF PRE-MULCHED FINAL GRADE. EDGER SHALL NOT BE REQUIRED TO SEPARATE MULCH TYPES UNLESS SPECIFIED ON THE PLANS.
18. ALL SHRUB BEDS ARE TO BE MULCHED WITH MIN. 3" DEPTH, SPECIFIED LANDSCAPE MULCH OVER SPECIFIED GEOTEXTILE WEED CONTROL FABRIC. ALL GROUND COVER AND PERENNIAL FLOWER BEDS SHALL BE MULCHED WITH 3" DEPTH SPECIFIED LANDSCAPE MULCH. NO WEED CONTROL FABRIC IS REQUIRED IN GROUNDCOVER OR PERENNIAL AREAS.
19. AT SEED AREA BOUNDARIES ADJACENT TO EXISTING NATIVE AREAS, OVERLAP ABUTTING NATIVE AREAS BY THE FULL WIDTH OF THE SEEDER.
20. EXISTING TURF AREAS THAT ARE DISTURBED DURING CONSTRUCTION, ESTABLISHMENT AND THE MAINTENANCE PERIOD SHALL BE RESTORED WITH NEW SOD TO MATCH EXISTING TURF SPECIES. DISTURBED NATIVE AREAS WHICH ARE TO REMAIN SHALL BE OVER SEEDED AND RESTORED WITH SPECIFIED SEED MIX.
21. CONTRACTOR SHALL OVER SEED ALL MAINTENANCE OR SERVICE ACCESS BENCHES AND ROADS WITH SPECIFIED SEED MIX UNLESS OTHERWISE NOTED ON THE PLANS.
22. ALL SEEDED SLOPES EXCEEDING 25% IN GRADE (4:1) SHALL RECEIVE EROSION CONTROL BLANKETS. PRIOR TO INSTALLATION, NOTIFY OWNER'S REPRESENTATIVE FOR APPROVAL OF LOCATION AND ANY ADDITIONAL COST IF A CHANGE ORDER IS NECESSARY.
23. WHEN COMPLETE, ALL GRADES SHALL BE WITHIN +/- 1/8" OF FINISHED GRADES AS SHOWN ON THE PLANS.
24. SOFT SURFACE TRAILS NEXT TO MANICURED TURF OR SHRUB BEDS SHALL BE CONTAINED WITH PLASTIC EDGER AS SPECIFIED IN THE MATERIAL SCHEDULE AND SPECIFICATIONS.

25. WHEN PLANTER POTS ARE SHOWN ON PLANS, CONTRACTOR SHALL INCLUDE THE FOLLOWING: PLANTER MIX, ANNUAL FLOWER PLANTING PROGRAM (INCLUDES 2 PLANTINGS FOR THE 1ST YEAR (SPRING AND FALL) AND WINTER HAND-WATERING AS NEEDED. UNLESS OTHERWISE SPECIFIED, CONTRACTOR TO PROVIDE ANNUAL PLANTING SELECTION FOR REVIEW BY OWNER. IRRIGATION FOR PLANTERS TO BE ON SEPARATE ZONE(S). CONTRACTOR TO COORDINATE PLACEMENT OF NECESSARY SLEEVING PRIOR TO PLACEMENT OF PLANT.
26. PRIOR TO THE PLACEMENT OF MULCH AND WEED FABRIC, A GRANULAR, PRE-EMERGENT, WEED CONTROL AGENT SHALL BE ADDED TO ALL PLANTING BEDS IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTION, EXCEPT AROUND ORNAMENTAL GRASSES.
27. THE CONTRACTOR IS EXPECTED TO KNOW AND UNDERSTAND THE CITY AND COUNTY SPECIFICATIONS FOR LANDSCAPE AND IRRIGATION. IN CASES OF DISCREPANCIES THE HIGHER OF THE TWO STANDARDS SHALL HAVE PRECEDENCE.
28. THE DEVELOPER, HIS SUCCESSORS AND ASSIGNS SHALL BE RESPONSIBLE FOR THE INSTALLATION, MAINTENANCE AND REPLACEMENT OF ALL IMPROVEMENTS SHOWN OR INDICATED ON THE APPROVED LANDSCAPE PLAN ON FILE IN THE PLANNING DEPARTMENT.

CHECKED BY: ARMIT  
 DRAWN BY: EGAR

101 WEST MAIN  
 101 WEST MAIN  
 FRISCO, COLORADO  
 SKETCH PLAN

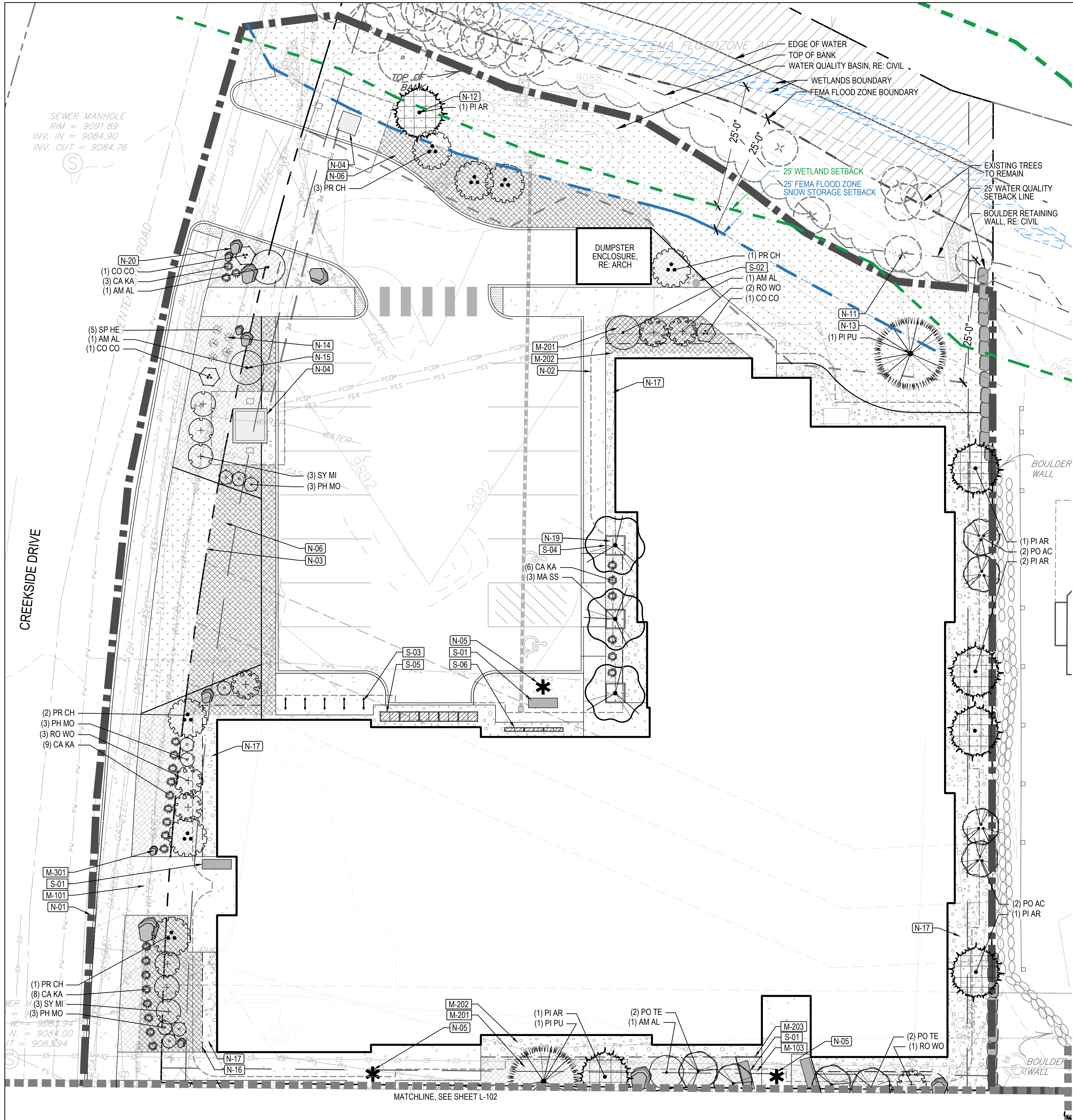
OWNER:  
 NHP FOUNDATION  
 122 EAST 42ND STREET  
 SUITE 4900  
 NEW YORK, NY 10168



DATE:  
 12/11/23 MSP  
 05/13/24 MSP  
 05/31/24 MSP

SHEET TITLE:  
**LANDSCAPE NOTES**

L-001



**PLANT SCHEDULE**

CODE	COMMON NAME
<b>DECIDUOUS TREES</b>	
PO AC	QUAKING ASPEN
PO TE	QUAKING ASPEN
<b>EVERGREEN TREES</b>	
PI PU	COLORADO SPRUCE
PI AR	BRISTLECONE PINE
<b>ORNAMENTAL TREES</b>	
MA SS	SPRING SNOW CRAB APPLE
<b>DECIDUOUS SHRUBS</b>	
AM AL	SASKATOON SERVICEBERRY
PH MO	MOUNTAIN NINEBARK
PR CH	SUCKER PUNCH CANADA CHOKECHERRY
RO WO	MOUNTAIN ROSE
SY MI	MISS KIM LILAC
<b>EVERGREEN SHRUBS</b>	
CO CO	CORAL BEAUTY COTONEASTER
<b>ORNAMENTAL GRASSES</b>	
CA KA	KARL FOERSTER FEATHER REED GRASS
SP HE	PRAIRIE DROPSIDE

**NOTES**

- RESEED ALL DISTURBANCE WITH SPECIFIED SEED MIX UNLESS OTHERWISE DESIGNATED.
- THESE PLANS SHALL NOT BE UTILIZED FOR CONSTRUCTION OR PERMITTING UNLESS STATED FOR SUCH USE IN THE TITLE BLOCK.
- EROSION CONTROL BLANKETS SHALL BE USED ON DISTURBED SLOPES STEEPER THAN 3:1.
- PLANT SYMBOLS ARE SHOWN AT APPROXIMATELY MATURE SIZE.
- ALL PLANT SYMBOLS SHOWN IN SNOW STORAGE AREAS SHALL BE PLANTS TOLERANT OF SNOW LOAD.
- ALL PLANTINGS SHALL BE PLANTED TO AVOID CONFLICTS WITH SIGHT TRIANGLES AND EXISTING AND PROPOSED UTILITIES. NOTIFY LANDSCAPE ARCHITECT OF CONFLICTS.
- FINAL PLANT LOCATION SHALL BE FIELD VERIFIED.
- PRESERVE AND PROTECT EXISTING TREES AS POSSIBLE.

**IRRIGATION:**

- ALL INSTALLED LANDSCAPE SHALL BE IRRIGATED.
- ALL TURF, NATIVE REVEGETATION, PERENNIAL AND ANNUAL PLANTINGS SHALL BE SPRAY IRRIGATED.
- ALL TREES AND SHRUB AREAS SHALL BE DRIP IRRIGATED.
- ALL MAINLINES SHALL HAVE CONDUIT UNDER CONCRETE SURFACES.
- IRRIGATION SHALL COMPLY WITH TOWN STANDARDS.

**AMENITY SCHEDULE**

SYMBOL	DESCRIPTION
[Symbol]	S-01 BENCH
[Symbol]	S-02 DOG WASTE STATION
[Symbol]	S-03 BIKE RACK
[Symbol]	S-04 LARGE SQUARE PLANTER
[Symbol]	S-05 MEDIUM RECTANGLE PLANTER
[Symbol]	S-06 SMALL RECTANGLE PLANTER

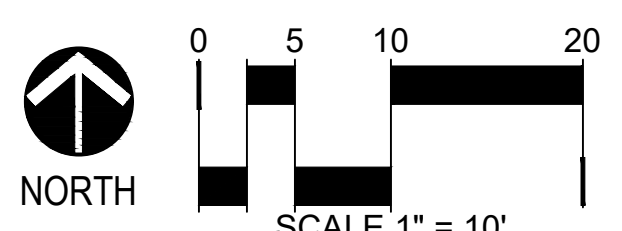
**MATERIAL SCHEDULE**

SYMBOL	DESCRIPTION
[Symbol]	M-101 STANDARD CONCRETE
[Symbol]	M-102 PAVING BRICK
[Symbol]	M-103 CRUSHER FINES
[Symbol]	M-201 LANDSCAPE MULCH
[Symbol]	M-202 ROCK MULCH
[Symbol]	M-203 PLASTIC EDGER
[Symbol]	M-301 BOULDERS
[Symbol]	M-302 SEATING SLAB
[Symbol]	M-303 DRY STACK WALL

**KEY NOTES**

SYMBOL	DESCRIPTION	AMOUNT
[Symbol]	N-06 SNOW STORAGE	
	PAVED SURFACES:	7,375 SF
	REQUIRED SNOW STORAGE:	1,844 SF
	PROVIDED SNOW STORAGE:	1,874 SF

CODE	DESCRIPTION	DETAIL
N-01	LIMIT OF WORK	
N-02	5' BUILDING SETBACK	
N-03	PROPERTY LINE	
N-04	EXISTING AND PROPOSED UTILITIES, REFER TO CIVIL	
N-05	COMMUNITY SPACE	1/L-501
N-06	SNOW STORAGE	2/L-501
N-11	TREE PROTECTION	3/L-501
N-12	TREE PLANTING	4/L-501
N-13	TREE PLANTING ON A SLOPE	
N-14	ORNAMENTAL GRASS AND PERENNIAL PLANTING	
N-15	SHRUB PLANTING	
N-16	SPADE CUT EDGER	
N-17	DRIP LINE	
N-19	TYPICAL POT PLANTING	
N-20	BOULDER OUTCROP	
N-21	EXISTING BOULDER OUTCROP TO REMAIN	
N-22	FRISCO STREET LIGHT	
N-23	BENCH TREE PLANTING	



CHECKED BY: ARMIT  
DRAWN BY: EGIAR

MATCHLINE, SEE SHEET L-102

MATCHLINE, SEE SHEET L-103





**PLANT SCHEDULE**

CODE	COMMON NAME
<b>DECIDUOUS TREES</b>	
PO AC	QUAKING ASPEN
PO TE	QUAKING ASPEN
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CA KA	KARL FOERSTER FEATHER REED GRASS
SP HE	PRAIRIE DROPSIDE

**NOTES**

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**AMENITY SCHEDULE**

SYMBOL	DESCRIPTION
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[S-03]	BIKE RACK
[S-04]	LARGE SQUARE PLANTER
[S-05]	MEDIUM RECTANGLE PLANTER
[S-06]	SMALL RECTANGLE PLANTER

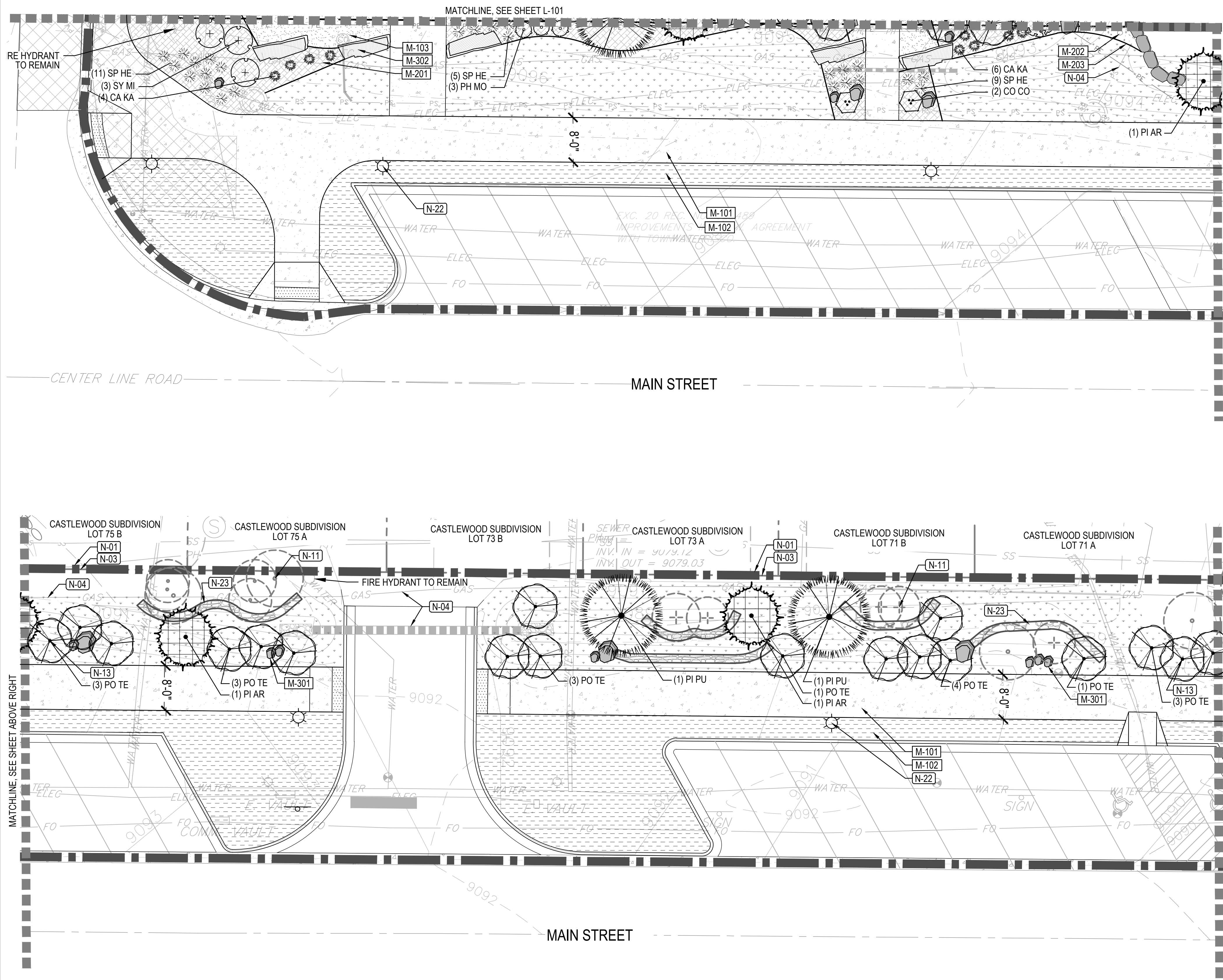
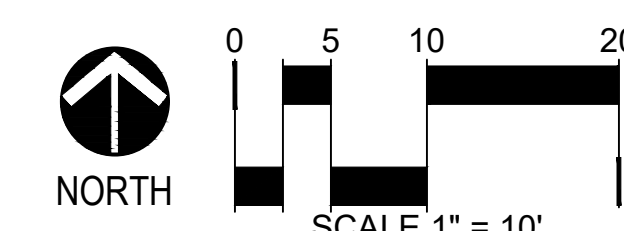
**MATERIAL SCHEDULE**

SYMBOL	DESCRIPTION
[M-101]	STANDARD CONCRETE
[M-102]	PAVING BRICK
[M-103]	CRUSHER FINES
[M-201]	LANDSCAPE MULCH
[M-202]	ROCK MULCH
[M-203]	PLASTIC EDGER
[M-301]	BOULDERS
[M-302]	SEATING SLAB
[M-303]	DRY STACK WALL

**KEY NOTES**

SYMBOL	DESCRIPTION	DETAIL
[N-06]	SNOW STORAGE	
	PAVED SURFACES:	7,375 SF
	REQUIRED SNOW STORAGE:	1,844 SF
	PROVIDED SNOW STORAGE:	1,874 SF

CODE	DESCRIPTION	DETAIL
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N-02	5' BUILDING SETBACK	
N-03	PROPERTY LINE	
N-04	EXISTING AND PROPOSED UTILITIES, REFER TO CIVIL COMMUNITY SPACE	
N-05	COMMUNITY SPACE	
N-06	SNOW STORAGE	
N-11	TREE PROTECTION	1/L-501
N-12	TREE PLANTING	2/L-501
N-13	TREE PLANTING ON A SLOPE	3/L-501
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N-19	TYPICAL POT PLANTING	6/L-502
N-20	BOULDER OUTCROP	7/L-502
N-21	EXISTING BOULDER OUTCROP TO REMAIN	
N-22	FRISCO STREET LIGHT	1-2/L-503
N-23	BENCH TREE PLANTING	1/L-504



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DRAWN BY:

**PLANT SCHEDULE**

CODE	COMMON NAME
<b>DECIDUOUS TREES</b>	
PO AC	QUAKING ASPEN
PO TE	QUAKING ASPEN
<b>EVERGREEN TREES</b>	
PI PU	COLORADO SPRUCE
PI AR	BRISTLECONE PINE
<b>ORNAMENTAL TREES</b>	
MA SS	SPRING SNOW CRAB APPLE
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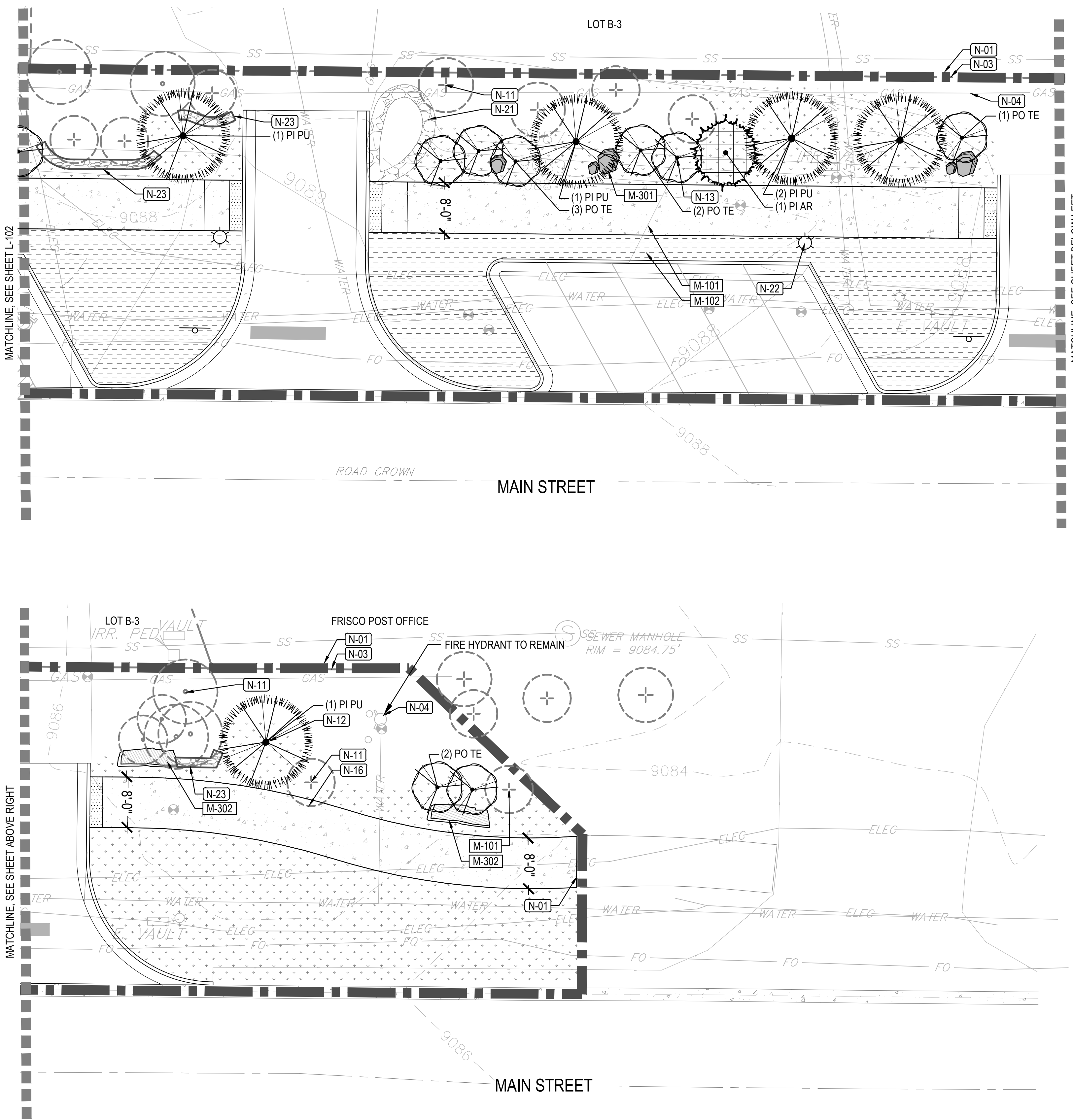
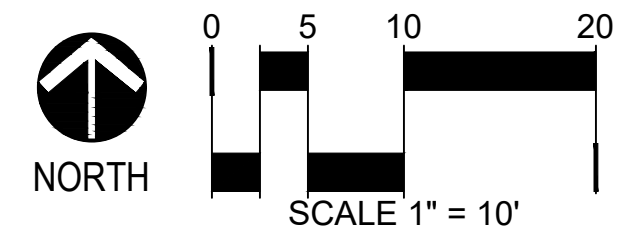
**MATERIAL SCHEDULE**

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[Symbol]	M-103 CRUSHER FINES
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**KEY NOTES**

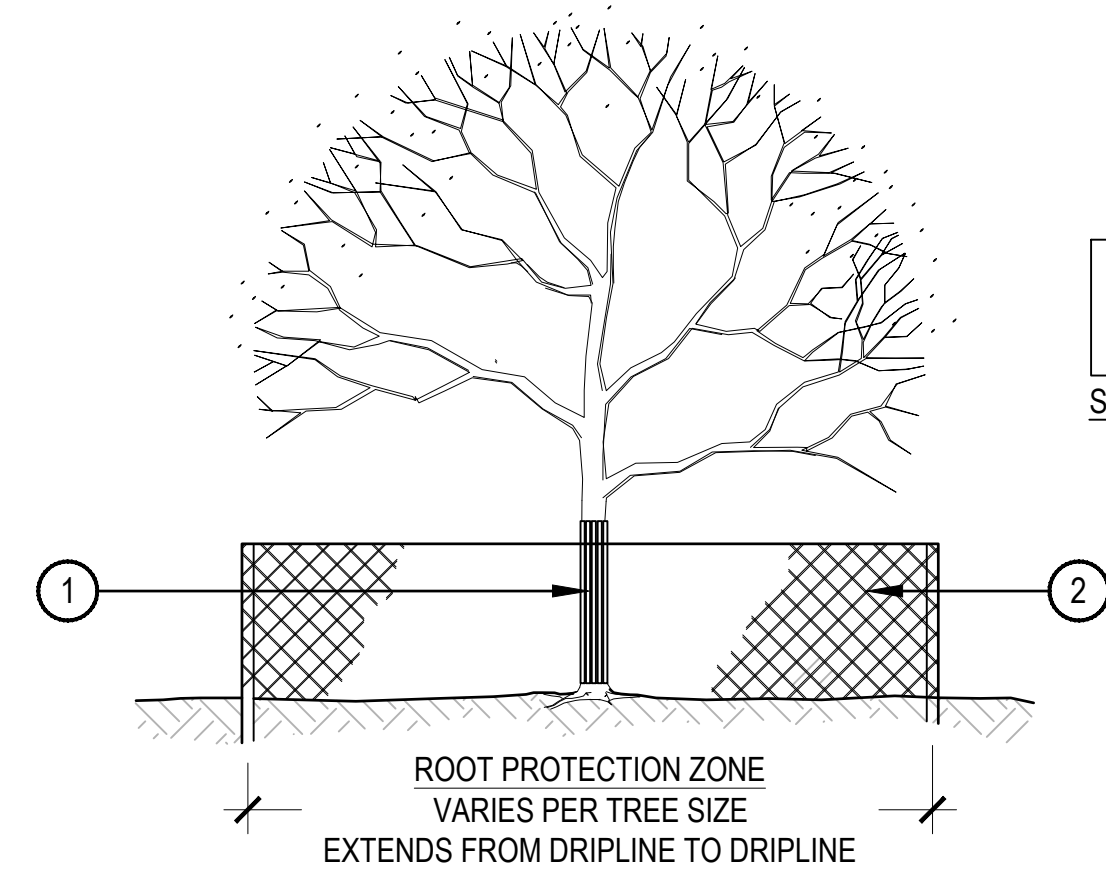
SYMBOL	DESCRIPTION	DETAIL
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CODE	DESCRIPTION	DETAIL
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N-03	PROPERTY LINE	
N-04	EXISTING AND PROPOSED UTILITIES, REFER TO CIVIL	
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N-17	DRIP LINE	3/L-502
N-19	TYPICAL POT PLANTING	6/L-502
N-20	BOULDER OUTCROP	7/L-502
N-21	EXISTING BOULDER OUTCROP TO REMAIN	
N-22	FRISCO STREET LIGHT	1-2/L-503
N-23	BENCH TREE PLANTING	1/L-504



**NOTES:**

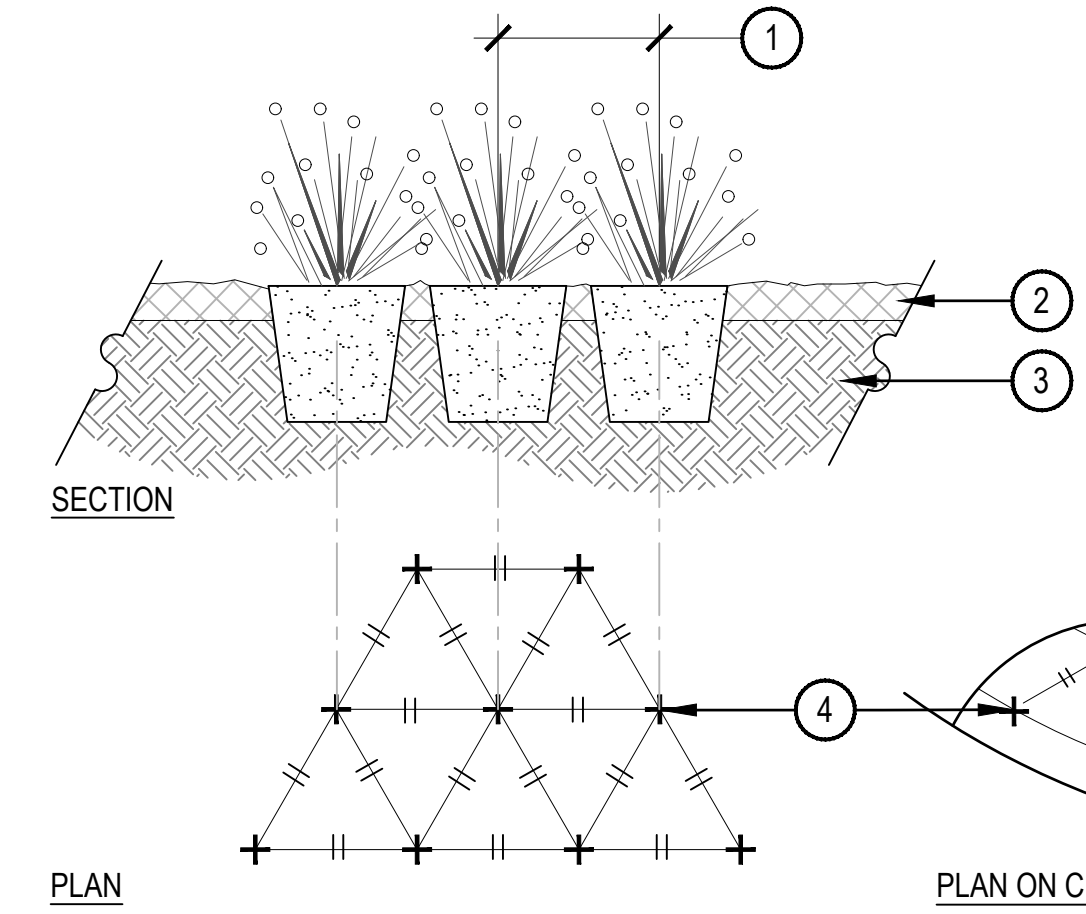
- TREES TO BE PROTECTED AND PRESERVED SHALL BE IDENTIFIED ON THE TRUNK WITH WHITE SURVEY TAPE. GROUPING OF MORE THAN ONE TREE MAY OCCUR.
- TO PREVENT ROOT SMOTHERING, SOIL STOCKPILES, SUPPLIES, EQUIPMENT OR ANY OTHER MATERIAL SHALL NOT BE PLACED OR STORED WITHIN THE DRIP LINE OR WITHIN 15 FEET OF A TREE TRUNK, WHICHEVER IS GREATER.
- FENCING MATERIAL SHALL BE SET AT THE DRIP LINE OR 15 FEET FROM TREE TRUNK, WHICHEVER IS GREATER, AND MAINTAINED IN AN UPRIGHT POSITION THROUGHOUT THE DURATION OF CONSTRUCTION ACTIVITIES.
- FENCING MATERIAL SHALL BE BRIGHT, CONTRASTING COLOR, DURABLE, AND A MINIMUM OF FOUR FEET IN HEIGHT. TREE ROOTS SHALL NOT BE CUT UNLESS CUTTING IS UNAVOIDABLE.
- WHEN ROOT CUTTING IS UNAVOIDABLE, A CLEAN SHARP CUT SHALL BE MADE TO AVOID SHREDDING OR SMASHING. ROOT CUTS SHOULD BE MADE BACK TO A LATERAL ROOT. ROOTS SHALL BE CUT NO MORE THAN 1/3 OF THE RADIUS FROM DRIPLINE TO TRUNK. WHENEVER POSSIBLE, ROOTS SHOULD BE CUT BETWEEN LATE FALL AND BUD OPENING, DURING DORMANCY PERIOD. ROOT STIMULATOR SHALL BE APPLIED TO CUT ROOTS. EXPOSED ROOTS SHALL BE COVERED IMMEDIATELY TO PREVENT DEHYDRATION. ROOTS SHALL BE COVERED WITH SOIL OR BURLAP AND KEPT MOIST. WATERING OF PROTECTED TREES IN WHICH ROOTS WERE CUT SHALL BE PROVIDED BY THE CONTRACTOR.
- WHEN ROOT CUTTING IS UNAVOIDABLE, A CLEAN SHARP CUT SHALL BE MADE TO AVOID SHREDDING OR SMASHING. ROOT CUTS SHOULD BE MADE BACK TO A LATERAL ROOT. WHENEVER POSSIBLE, ROOTS SHOULD BE CUT BETWEEN LATE FALL AND BUD OPENING, DURING DORMANCY PERIOD. EXPOSED ROOTS SHALL BE COVERED IMMEDIATELY TO PREVENT DEHYDRATION. ROOTS SHALL BE COVERED WITH SOIL OR BURLAP AND KEPT MOIST. WATERING OF PROTECTED TREES IN WHICH ROOTS WERE CUT SHALL BE PROVIDED BY THE CONTRACTOR.
- ANY GRADE CHANGES (SUCH AS THE REMOVAL OF TOPSOIL OR ADDITION OF FILL MATERIAL) WITHIN THE DRIP LINE SHOULD BE AVOIDED FOR EXISTING TREES TO REMAIN. RETAINING WALLS AND TREE WELLS ARE ACCEPTABLE ONLY WHEN CONSTRUCTED PRIOR TO GRADE CHANGE.



SIGN

ROOT PROTECTION ZONE  
 VARIES PER TREE SIZE  
 EXTENDS FROM DRIPLINE TO DRIPLINE

- TRUNK PROTECTION** - 1" BOARDS NO LESS THAN 5' LONG OR TO REACH FIRST SCAFFOLD BRANCH. WIRE TO HOLD BOARDS IN PLACE, NO NAILS PERMITTED. INCLUDE WRAPPING OF BURLAP UNDER BOARDS.
- BRANCH PROTECTION** - PROTECT LOWER BRANCHES OF TREE CANOPY. PROVIDE CONSTRUCTION FENCING OR EQUAL AT DRIPLINE MINIMUM.
- PLACE SIGNS EVERY 50', PLACE SIGNS WHERE VISIBLE, ATTACH TO FENCING.



NOTES:

- WHEN PLANTED ON A CURVE, ORIENT ROWS TO FOLLOW THE LONG AXIS OF AREAS WHERE PLANTS ARE MASSED.

- REFER TO PLANT SCHEDULE FOR PLANT ON CENTER SPACING
- SPECIFIED MULCH, REFER TO MATERIAL SCHEDULE, SHEET L-XXX
- AMENDED PLANTING BED TILLED TO A DEPTH OF 10", BACKFILL WITH PLANT MIX PER LANDSCAPE SPECIFICATIONS
- CENTER OF PLANT

**1 TREE PROTECTION**

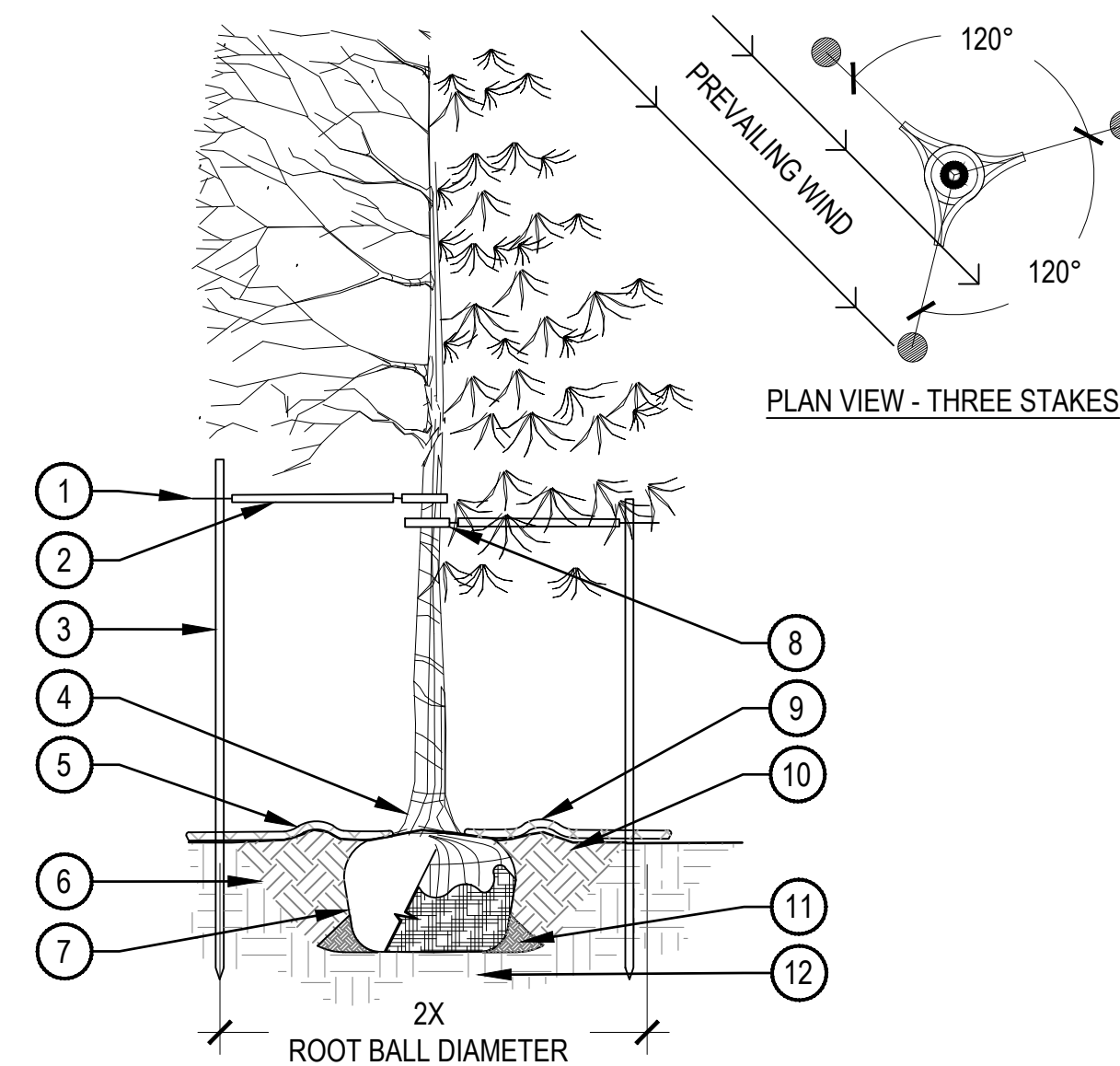
SCALE: 1/8" = 1'-0"

**PRUNING NOTES:**

- ALL PRUNING SHALL COMPLY WITH ANSI A300 STANDARDS.
- DO NOT HEAVILY PRUNE TREE AT PLANTING. PRUNE ONLY CROSSOVER LIMBS, CO-DOMINANT LEADERS AND BROKEN BRANCHES. SOME INTERIOR TWIGS AND LATERAL BRANCHES MAY BE PRUNED. HOWEVER, DO NOT REMOVE THE TERMINAL BUDS OF BRANCHES THAT EXTEND TO THE EDGE OF THE CROWN.

**STAKING NOTES:**

- STAKE TREES PER DIAGRAM. AFTER A MINIMUM OF (3) THREE YEARS CONFIRM TREE IS ESTABLISHED. CHECK FOR ROOTBALL STABILITY. APPLY HAND PRESSURE TO TRUNK OF TREE, WHEN ROOTBALL DOES NOT MOVE, REMOVE STAKING.
  - 2" CALIPER SIZE AND UNDER DECIDUOUS AND ASPEN TREES - MINIMUM 2 STAKES - ONE ON N.W. SIDE, ONE ON S.W. SIDE (OR PREVAILING WIND SIDE AND 180° FROM THAT SIDE).
  - EVERGREEN TREES - 3 STAKES PER DIAGRAM.
  - 3" CALIPER SIZE AND LARGER - 3 STAKES PER DIAGRAM.
- WIRE OR CABLE SHALL BE MINIMUM 12 GAUGE, TIGHTEN WIRE OR CABLE ONLY ENOUGH TO KEEP FROM SLIPPING. ALLOW FOR SOME TRUNK MOVEMENT. NYLON STRAPS SHALL BE LONG ENOUGH TO ACCOMMODATE 1-1/2" OF GROWTH AND BUFFER ALL BRANCHES FROM WIRE.
- ADJUST STAKING, STRAPS AND GUY WIRES ANNUALLY.
- TREATED WOOD POST PREFERRED. METAL T STAKES WITH PLASTIC SAFETY CAPS ACCEPTABLE WITH APPROVAL FROM OWNER.



PLAN VIEW - THREE STAKES

- GALVANIZED WIRE
- PLACE MINIMUM 1/2" PVC PIPE AROUND EACH WIRE, EXPOSED WIRE SHALL BE MAXIMUM 2" EACH SIDE
- INSTALL STAKING PER SPECIFICATIONS
- PLANT TREE SO THAT FIRST ORDER MAJOR ROOT IS 1"-2" ABOVE FINAL GRADE
- 2'-0" RADIUS MULCH RING, CENTERED ON TRUNK, 3" DEPTH, ON TOP OF WEED FABRIC, DO NOT PLACE MULCH IN CONTACT WITH TREE TRUNK, FINISHED GRADE REFERENCES TOP OF MULCH
- 1:1 SLOPE ON SIDES OF PLANTING HOLE
- REMOVE ALL TWINE, ROPE, BURLAP AND WIRE FROM THE ENTIRE ROOTBALL AND TRUNK
- GROMMETED NYLON STRAPS
- 4'-6" HIGH WATER SAUCER IN NON-TURF AREAS
- BACKFILL AROUND ROOTBALL WITH PLANT MIX, PLANT MIX SHALL CONSIST OF EQUAL PARTS TOPSOIL, COMPOST, EXCAVATED SOIL, PLUS MYCORRHIZAL INOCULANT PER SPECIFICATIONS
- PLACE SOIL AROUND ROOT BALL FIRMLY, DO NOT COMPACT OR TAMP, SETTLE SOIL WITH WATER TO FILL ALL AIR POCKETS
- PLACE ROOT BALL ON UNDISTURBED SOIL TO PREVENT SETTLEMENT

**2 TREE PLANTING DETAIL**

SCALE: 3/16" = 1'-0"

**NOTES:**

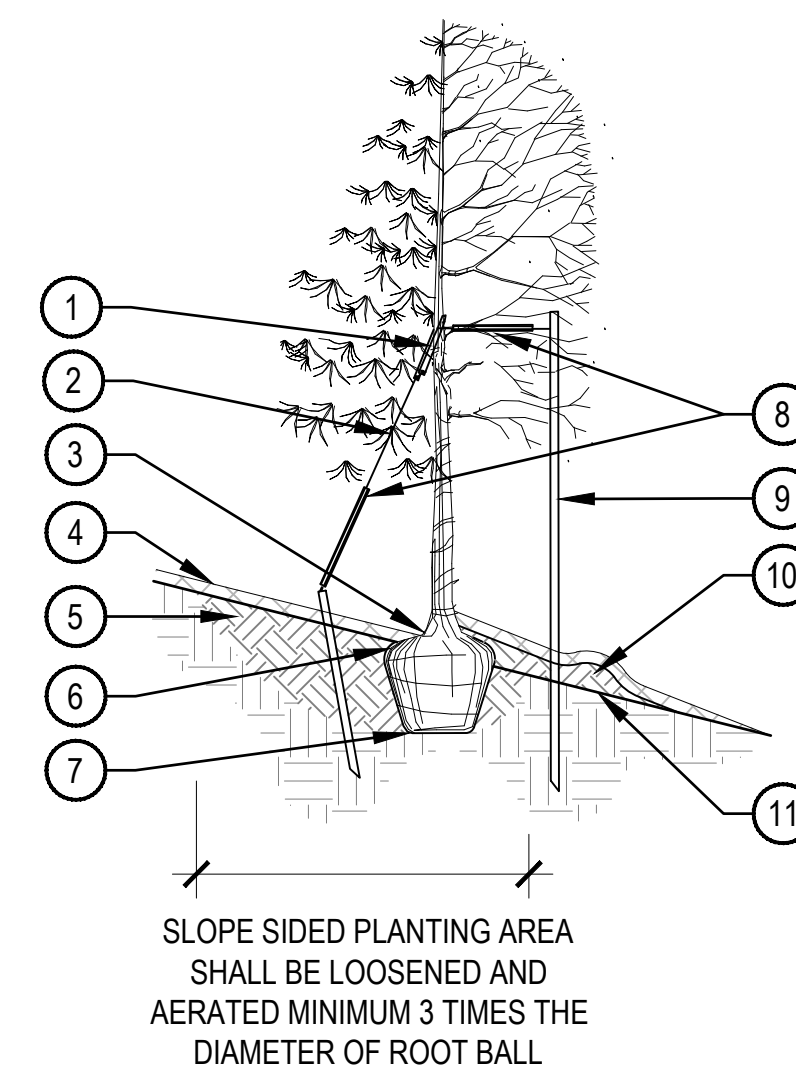
- EXCAVATE PLANTING HOLES WITH SLOPING SIDES. MAKE EXCAVATIONS AT LEAST THREE TIMES AS WIDE AS THE ROOT BALL DIAMETER AND LESS THAN THE DISTANCE FROM THE TOP MOST ROOT AND THE BOTTOM OF THE ROOT BALL. THE PLANTING AREA SHALL BE LOOSENED AND AERATED AT LEAST THREE TO FIVE TIMES THE DIAMETER OF THE ROOT BALL. REFERENCE TREE PLANTING DETAIL FOR BACKFILL NOTES.
- TREES SHALL BE PLANTED WITH THE TOP MOST ROOT IN THE ROOT BALL 3" TO 5" HIGHER THAN THE FINISHED LANDSCAPE GRADE. TREES WHERE THE TRUNK FLARE IS NOT VISIBLE SHALL BE REJECTED.
- FORM SOIL INTO A 3" TO 5" TALL WATERING RING (SAUCER) AROUND PLANTING AREA. THIS IS NOT NECESSARY IN IRRIGATED TURF AREAS. APPLY 3" TO 4" DEPTH OF SPECIFIED MULCH INSIDE WATERING RING.

**PRUNING NOTES:**

- ALL PRUNING SHALL COMPLY WITH ANSI A300 STANDARDS.
- DO NOT HEAVILY PRUNE TREE AT PLANTING. PRUNE ONLY CROSSOVER LIMBS, CO-DOMINANT LEADERS AND BROKEN BRANCHES. SOME INTERIOR TWIGS AND LATERAL BRANCHES MAY BE PRUNED. HOWEVER, DO NOT REMOVE THE TERMINAL BUDS OF BRANCHES THAT EXTEND TO THE EDGE OF THE CROWN.

**STAKING NOTES:**

- STAKE TREES PER DIAGRAM. AFTER A MINIMUM OF (3) THREE YEARS CONFIRM TREE IS ESTABLISHED. CHECK FOR ROOTBALL STABILITY. APPLY HAND PRESSURE TO TRUNK OF TREE, WHEN ROOTBALL DOES NOT MOVE, REMOVE STAKING.
  - 2" CALIPER SIZE AND UNDER DECIDUOUS AND ASPEN TREES - MINIMUM 2 STAKES - ONE ON N.W. SIDE, ONE ON S.W. SIDE (OR PREVAILING WIND SIDE AND 180° FROM THAT SIDE).
  - EVERGREEN TREES - 3 STAKES PER DIAGRAM.
  - 3" CALIPER SIZE AND LARGER - 3 STAKES PER DIAGRAM.
- WIRE OR CABLE SHALL BE MINIMUM 12 GAUGE, TIGHTEN WIRE OR CABLE ONLY ENOUGH TO KEEP FROM SLIPPING. ALLOW FOR SOME TRUNK MOVEMENT. NYLON STRAPS SHALL BE LONG ENOUGH TO ACCOMMODATE 1-1/2" OF GROWTH AND BUFFER ALL BRANCHES FROM WIRE.
- ADJUST STAKING, STRAPS AND GUY WIRES ANNUALLY.
- USE GUY ASSEMBLIES FOR EVERGREENS AND TREES OVER 3" CALIPER. ALL WIRE TO BE MINIMUM 12 GAUGE GALVANIZED.



SLOPE SIDED PLANTING AREA SHALL BE LOOSENED AND AERATED MINIMUM 3 TIMES THE DIAMETER OF ROOT BALL

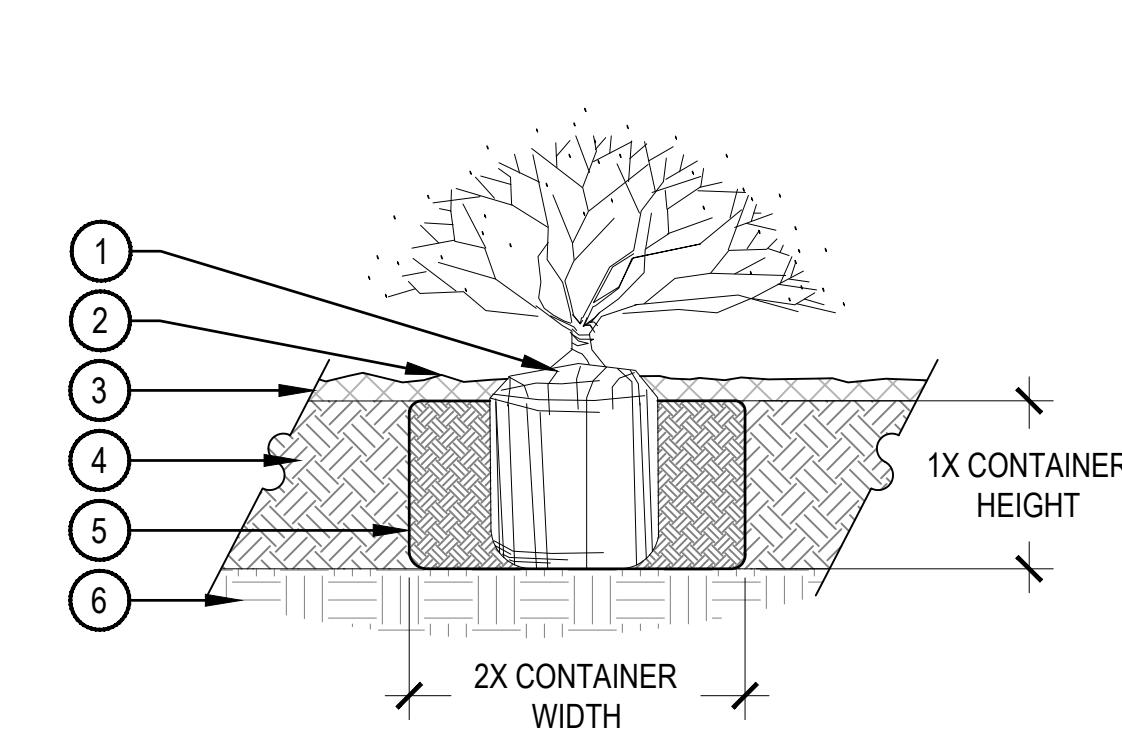
- GROMMETED NYLON STRAP
- GALVANIZED WIRE
- TOP MOST ROOT IN ROOTBALL: 1"-2" ABOVE EXISTING GRADE, UPHILL SIDE
- 2'-4" OF ORGANIC MULCH APPLIED OVER PLANTING AREA AND AWAY FROM THE TRUNK, REFER TO MATERIAL SCHEDULE, SHEET L-XXX, FINISHED GRADE REFERENCES TOP OF MULCH
- BACKFILL WITH PLANT MIX, PLANT MIX SHALL CONSIST OF EQUAL PARTS TOPSOIL, COMPOST, EXCAVATED SOIL, PLUS MYCORRHIZAL INOCULANT PER SPECIFICATIONS, WATER THOROUGHLY WHEN BACKFILLING
- REMOVE ALL TWINE, ROPE, BURLAP AND WIRE FROM ENTIRE ROOTBALL AND TRUNK
- PLACE ROOTBALL ON UNDISTURBED SOIL TO PREVENT SETTLEMENT, IF SOIL HAS BEEN IMPORTED, PROVIDE MODERATE FOOT PACKING OF SOIL DIRECTLY UNDER LOCATION OF ROOTBALL
- 24" X 3/4" P.V.C. MARKERS (TYPICAL) OVER WIRES
- TREATED WOOD POST, OR METAL T STAKE WITH CAP, WITH GROMMETED NYLON STRAPS, USE 2 GUY WIRES
- UNAMENDED TOPSOIL ADDED TO EXISTING GRADE ON DOWN HILL SIDE, REFER TO SPECIFICATIONS
- EXISTING GRADE

**3 TREE PLANTING ON SLOPE**

SCALE: 1/4" = 1'-0"

**4 ORNAMENTAL GRASS AND PERENNIAL PLANT LAYOUT**

SCALE: 1" = 1'-0"



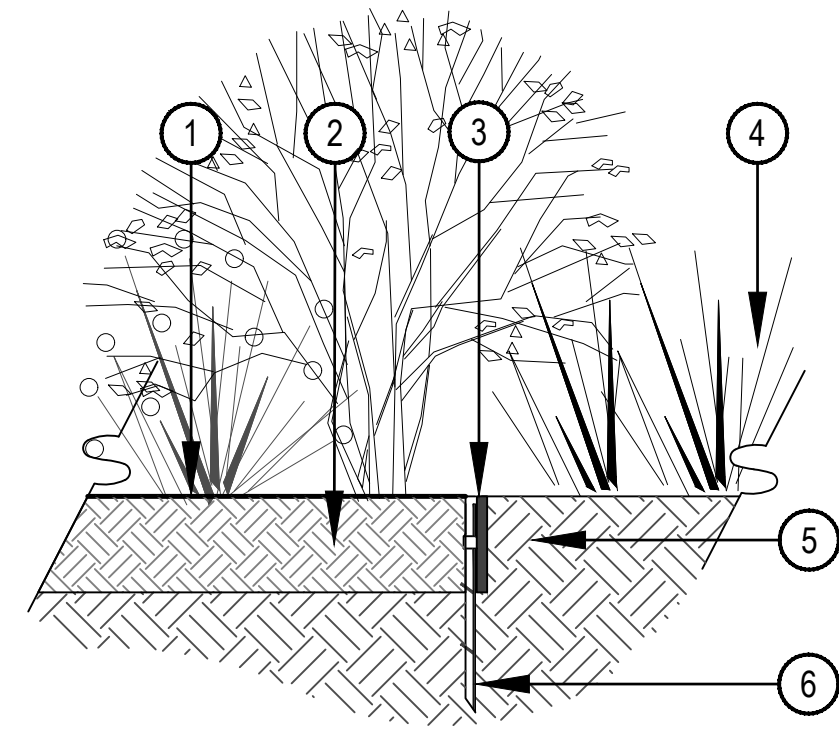
**NOTE:**

- BROKEN OR CRUMBLING ROOT-BALLS WILL BE REJECTED.
- CARE SHOULD BE TAKEN NOT TO DAMAGE THE SHRUB OR ROOT-BALL WHEN REMOVING IT FROM ITS CONTAINER.
- ALL JUNIPERS SHOULD BE PLANTED SO THE TOP OF THE ROOT-BALL OCCURS ABOVE THE FINISH GRADE OF THE MULCH LAYER.
- DIG PLANT PIT TWICE AS WIDE AND AS HIGH AS THE CONTAINER.
- PRUNE ALL DEAD OR DAMAGED WOOD PRIOR TO PLANTING, DO NOT PRUNE MORE THAN 20% OF LIMBS.

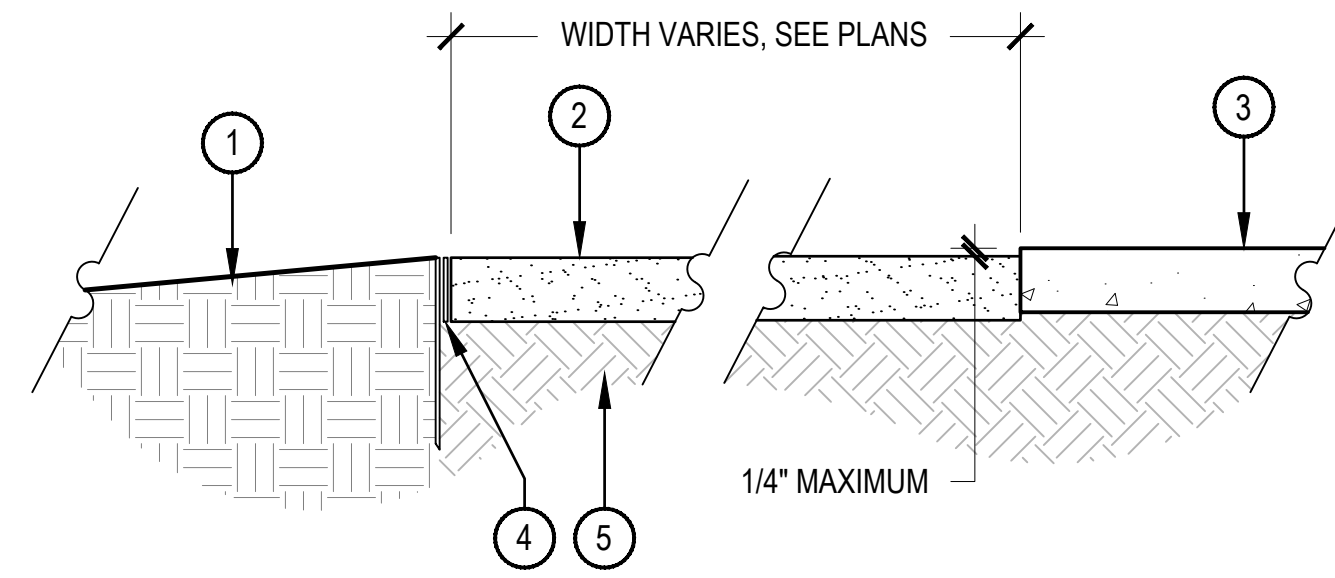
- SET SHRUB ROOT-BALL 1" HIGHER THAN FINISH GRADE
- FINISH GRADE (TOP OF MULCH)
- SPECIFIED MULCH, REFER TO MATERIAL SCHEDULE, SHEET L-XXX
- TILL IN SPECIFIED SOIL AMENDMENT TO A DEPTH OF 8" IN BED
- BACKFILLED AMENDED SOIL
- UNDISTURBED SOIL

**5 SHRUB PLANTING**

SCALE: 1 1/2" = 1'-0"

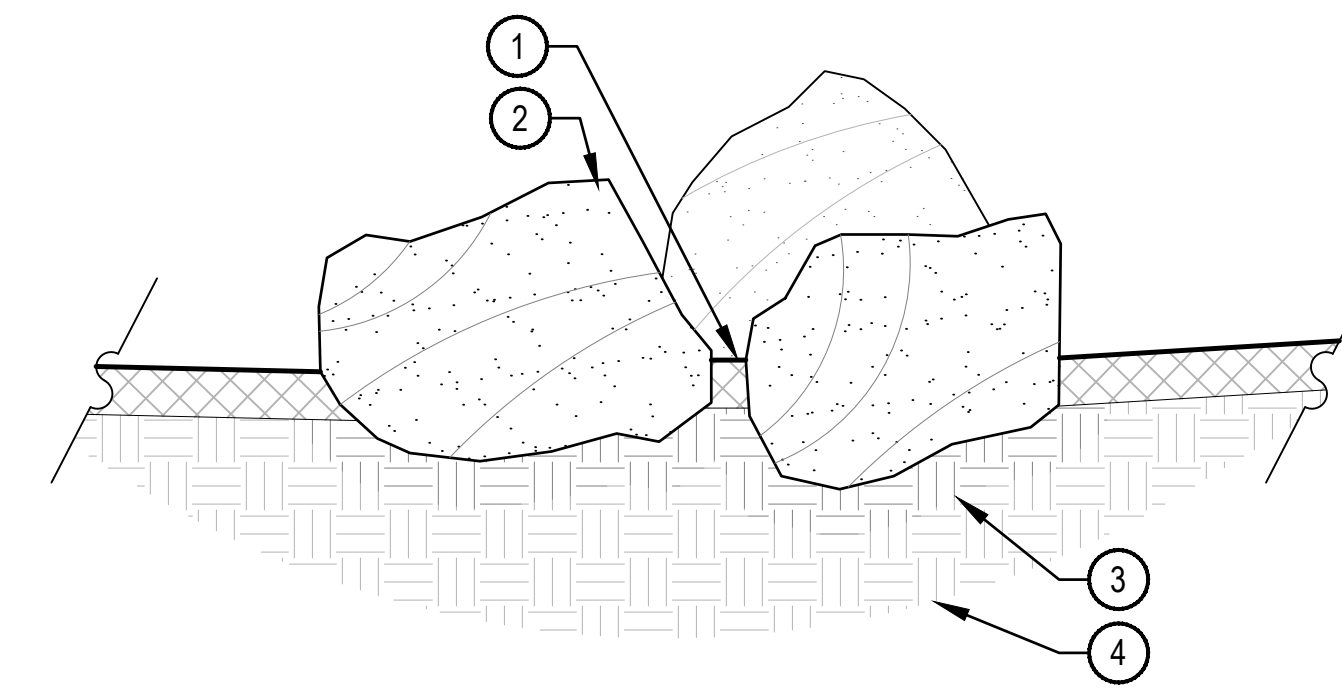


- 1 FINISH GRADE
- 2 ADJACENT LANDSCAPE, SEE LANDSCAPE PLANS FOR TYPE
- 3 1X6 BENDA BOARD EDGER BY EPIC PLASTICS, COLOR TO BE TEAK
- 4 NATIVE SEED OR ADJACENT LANDSCAPE, REFER TO LANDSCAPE PLANS
- 5 SUBGRADE
- 6 EDGER STAKE, INSTALL PER MANUFACTURER SPECIFICATIONS



- 1 ADJACENT LANDSCAPE, REFER TO PLAN
- 2 COMPACTED, STABILIZED CRUSHER FINES, RE: MATERIAL SCHEDULE, SHEET L-002
- 3 ADJACENT HARDSCAPE, REFER TO PLAN
- 4 PLASTIC EDGING
- 5 SUBGRADE COMPACTED TO 95% OF STANDARD PROCTOR DENSITY

- NOTES:
1. SLOPE ADJACENT LANDSCAPE AWAY FROM CRUSHER FINES PAVING.
  2. PAVING AREAS SHALL NOT DRAIN TOWARDS STRUCTURES.
  3. COMPACT CRUSHER FINES WET, COMPACT TO 95% STANDARD PROCTOR DENSITY. USE A RIDING ROLLER TO COMPACT TRAIL.
  4. CROWN OF 2% IN FLAT AREAS AS SHOWN.
  5. CROSS SLOPE TRAIL AT 1-2% WITH GRADE WHERE TOPOGRAPHY DICTATES.
  7. APPLY STABILIZER/TACKIFIER TO CRUSHER FINES SURFACE PER MATERIALS SCHEDULE AND SPECIFICATIONS.



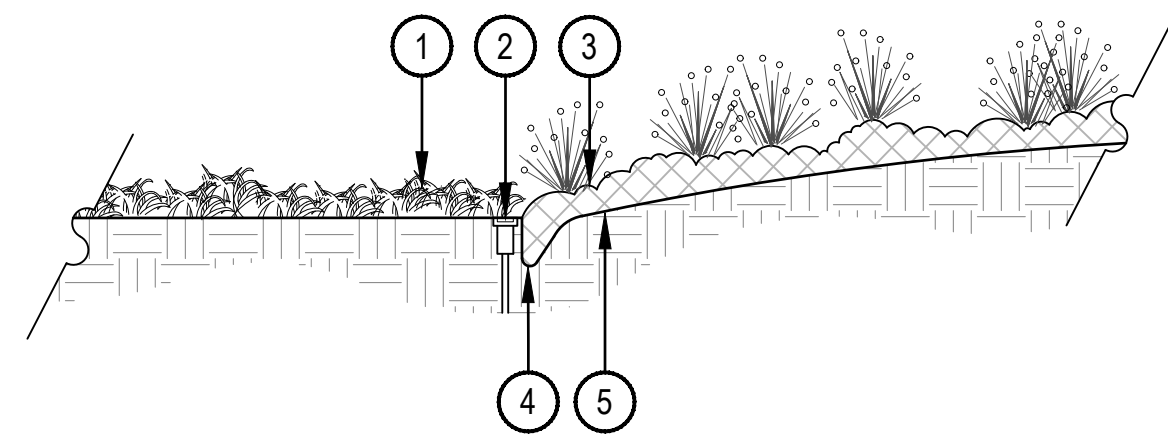
- 1 MULCH TO BOULDERS, NO GAPS
- 2 BOULDER, NATURALLY SET BOULDER SO THAT A MINIMUM 1/4 OF BOULDER IS BELOW FINISH GRADE
- 3 COMPACTED SUBGRADE
- 4 UNDISTURBED GRADE

- NOTES:
1. THE OWNERS REPRESENTATIVE SHALL APPROVE LOCATIONS AND SIZES OF ALL BOULDERS PRIOR TO PLACING.

1 BENDA BOARD EDGER

N.T.S.

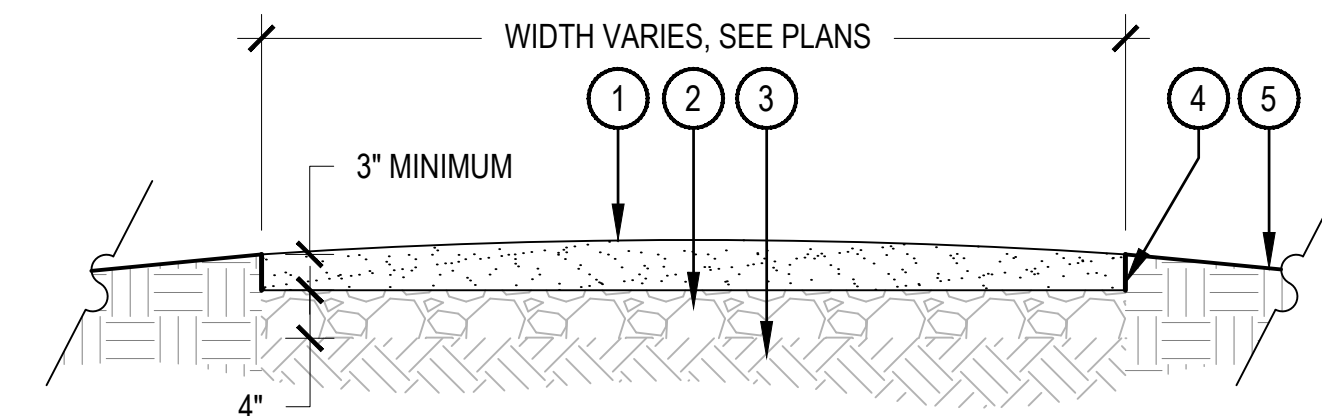
- 1 TURFGRASS OR DYLAND SEED
- 2 IRRIGATION HEADS SHOULD BE LOCATED ADJACENT TO MULCH BEDS, OFFSET HEAD INTO GRASS AREA TO ENSURE STABLE SUPPORT
- 3 PLANTING BED
- 4 VERTICAL SPADE CUT EDGE FILLED WITH SPECIFIED MULCH, TAPER EDGE OF BED SO MULCH IS DEEPER AGAINST SPADED EDGE
- 5 SPECIFIED DEPTH OF MULCH, TYPICALLY WOOD MULCH 3"-4" DEEP



4 CRUSHER FINES PAVING

SCALE: 1" = 1'-0"

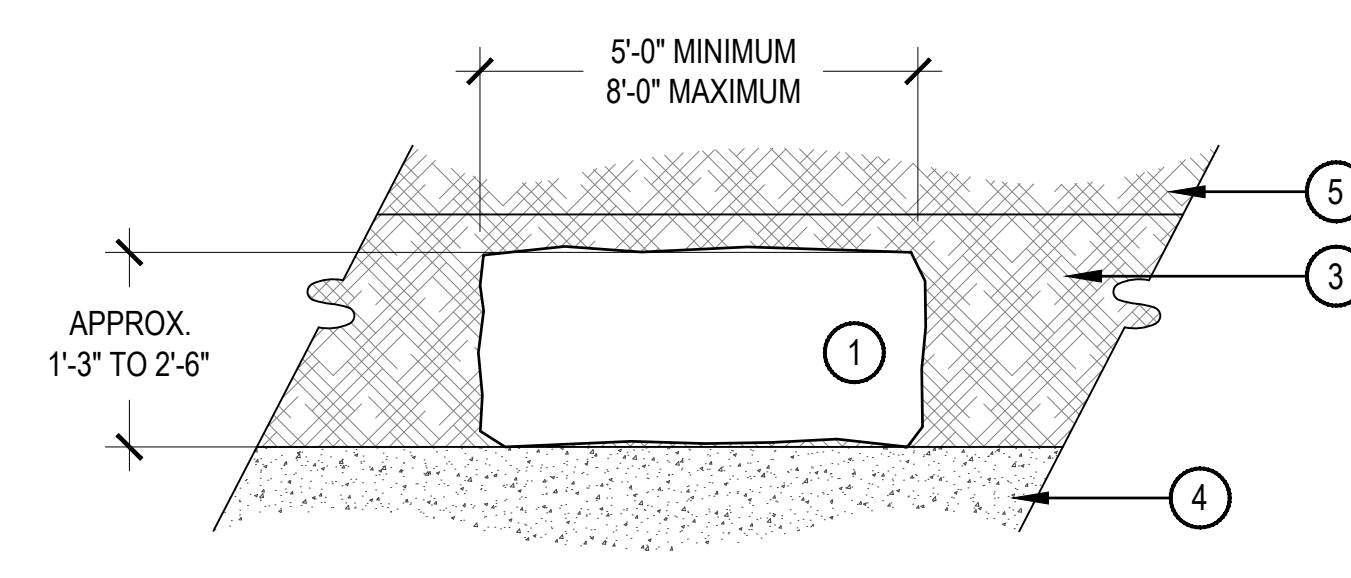
- 1 COMPACTED, STABILIZED CRUSHER FINES, RE: MATERIAL SCHEDULE, SHEET L-002
- 2 AGGREGATE BASE
- 3 SUBGRADE COMPACTED TO 95% STANDARD PROCTOR DENSITY
- 4 PLASTIC EDGER
- 5 SLOPE ADJACENT GRADE AWAY FROM TRAIL



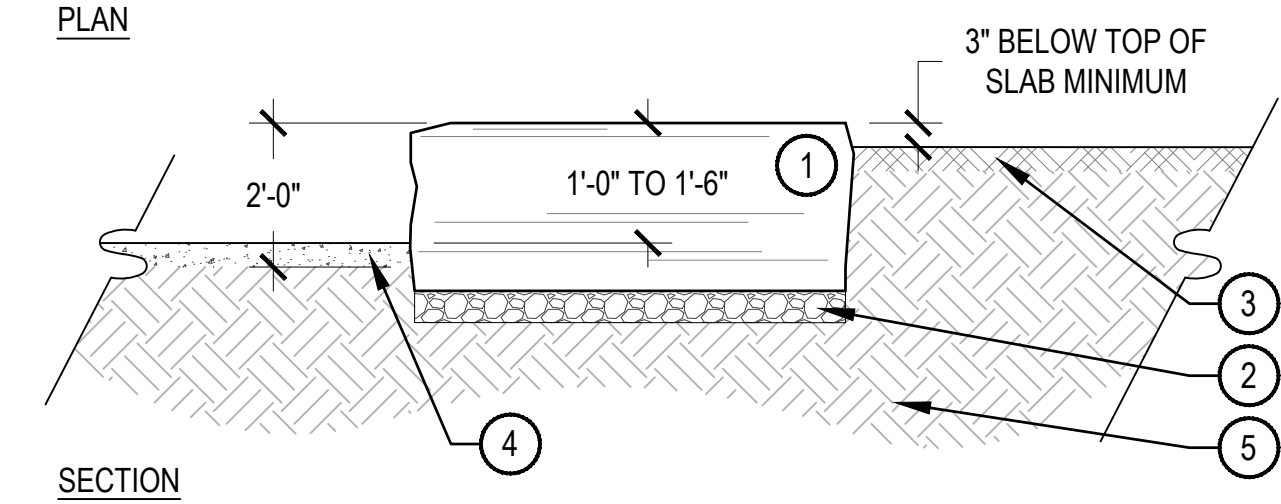
- NOTES:
1. SLOPE ADJACENT LANDSCAPE AWAY FROM CRUSHER FINES PAVING.
  2. COMPACT CRUSHER FINES WET, COMPACT TO 95% STANDARD PROCTOR DENSITY. USE A SMALL 4" RIDING ROLLER TO COMPACT TRAIL.
  3. CROWN OF 2% IN FLAT AREAS AS SHOWN.
  5. CROSS SLOPE TRAIL AT 1-2% WITH GRADE WHERE TOPOGRAPHY DICTATES.
  6. APPLY STABILIZER/TACKIFIER TO CRUSHER FINES SURFACE PER MATERIALS SCHEDULE AND SPECIFICATIONS.

7 BOULDER CROPPING

SCALE: 1" = 1'-0"



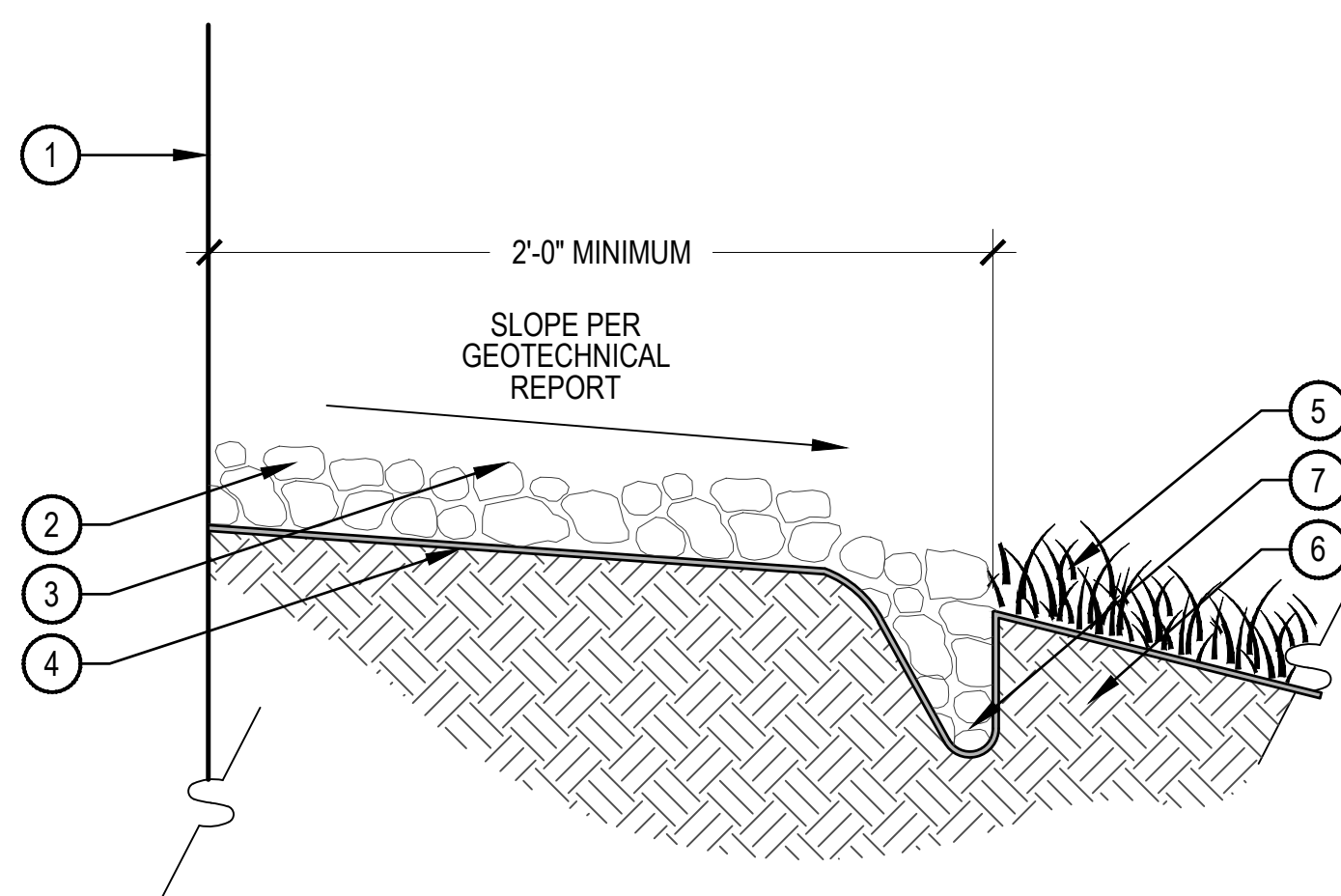
- 1 SILOAM STONE BEAM SLAB WITH NATURAL CLEFT FACE, 5'-8" W x 1'-6" - 2'-0" H x 1'-3" - 2'-6" D SAW CUT FINISH ON TOP, ALL OTHER SIDES SHALL HAVE A NATURAL CLEFT FACE. STONE SLAB SHALL BE SET TO BE AS LEVEL AS POSSIBLE. BURY BOULDER SLAB 6" DEPTH MINIMUM.
- 2 4" DEPTH COMPACTED ROAD BASE
- 3 ADJACENT PLANTING BED, REFER TO LANDSCAPE PLANS
- 4 ADJACENT PAVING REFER TO PLANS
- 5 UNDISTURBED SUBGRADE



2 SPADE CUT EDGE

SCALE: 1/2" = 1'-0"

- 1 ADJACENT BUILDING
- 2 RIVER ROCK COBBLE; REFER TO MATERIAL SCHEDULE
- 3 FINISHED GRADE
- 4 WEED BARRIER FABRIC, 24" MINIMUM LAP JOINT
- 5 ADJACENT NATIVE GRASS OR LANDSCAPE BED, REFER TO LANDSCAPE PLANS FOR ADJACENT TREATMENT TYPE
- 6 UNDISTURBED SUBGRADE
- 7 SPADE CUT EDGE OF DRIP LINE



- NOTES:
1. COBBLE DRIP LINE TO BE INCLUDED AROUND PERIMETER OF ALL BUILDINGS WHERE ROOF LINE EXTENDS AND SHEDS WATER / SNOW.

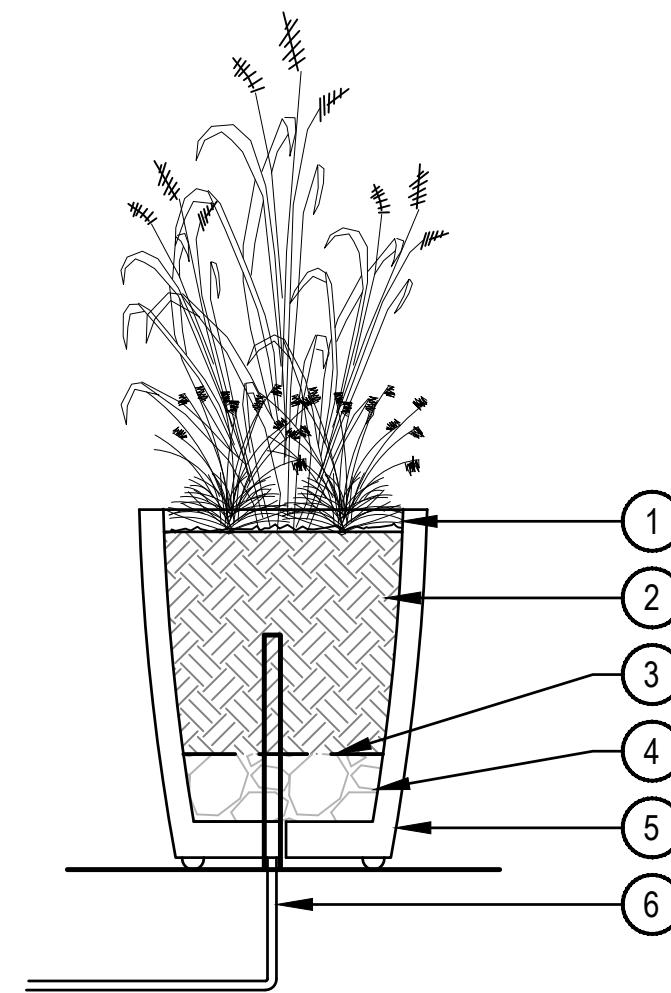
3 COBBLE DRIP LINE

SCALE: 1 1/2" = 1'-0"

5 CRUSHER FINES TRAIL WITH EDGER

SCALE: 3/4" = 1'-0"

- 1 2" DEPTH PLANTING MULCH, REFER TO MATERIAL SCHEDULE, SHEET L-002
- 2 AMENDED ANNUAL POT TOPSOIL PER SPECIFICATIONS
- 3 WEED CONTROL FABRIC
- 4 6" DEPTH CLEAN GRAVEL
- 5 PLANTER POT, REFER TO AMENITY SCHEDULE, SHEET L-002
- 6 PROVIDE SLEEVE FOR DRIP IRRIGATION, REFER TO IRRIGATION PLANS



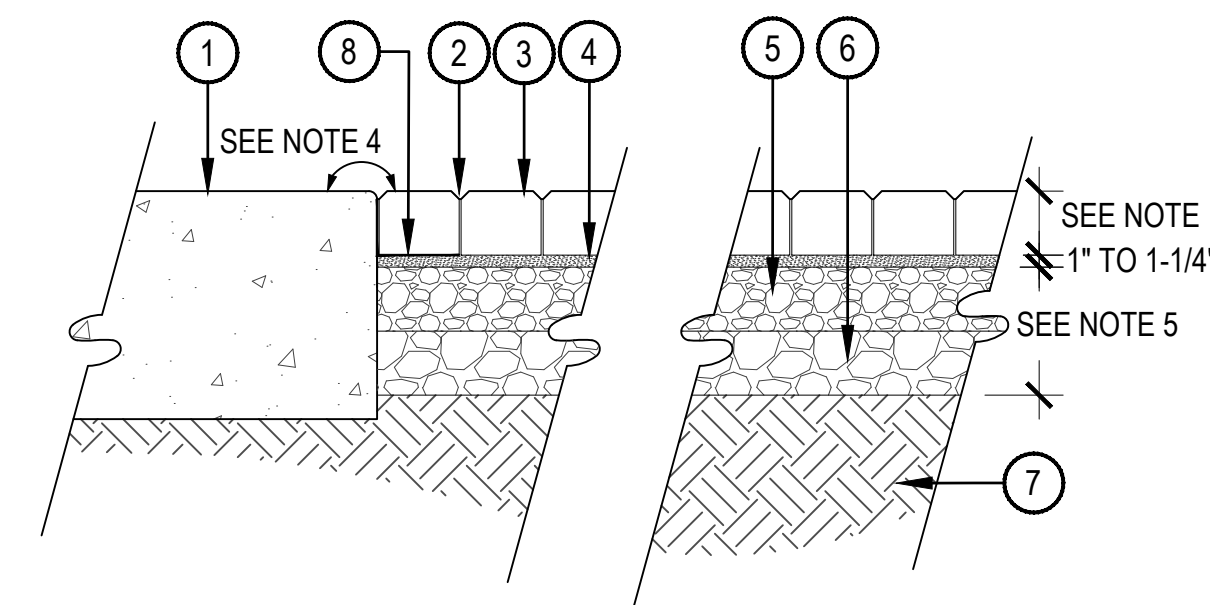
- NOTES:
1. SEE LANDSCAPE PLANS AND NOTES FOR PLANTING.
  2. REFER TO LANDSCAPE NOTES FOR ANNUAL FLOWER PLANTING PROGRAM.
  3. PROVIDE DRIP IRRIGATION, PROVIDE WEEP HOLES FOR DRAINAGE WHEN NECESSARY.

6 TYPICAL POT PLANTING DETAIL

SCALE: 3/4" = 1'-0"

8 STONE SLAB BENCH

SCALE: 1/2" = 1'-0"



- NOTES:
1. REFER TO LANDSCAPE MATERIAL SCHEDULE FOR PAVING MATERIAL, SPECIFICATIONS, COLOR, LAYOUT, AND ADDITIONAL INFORMATION. MINIMUM CUT PAVER LENGTH SHALL BE NO LESS THAN 3".
  2. JOINT SPACING SHALL BE CONSISTENT. JOINTS SHALL BE FULLY FILLED. SWEEP OFF EXCESS SAND AND REMOVE FROM SITE.
  4. FINAL ELEVATION OF PAVERS AFTER COMPACTION SHALL BE SET FLUSH WITH ADJACENT PAVING WITHIN 1/8" TOLERANCE MAXIMUM.
  5. FINAL BASE COURSE AND SETTING BED DEPTHS, COMPACTION AND SPECIFICATIONS SHALL BE PROVIDED BY CIVIL ENGINEER.

- 1 REFER TO CIVIL SITE PLANS FOR ADJACENT PAVING/CURB AND GRADING INFORMATION
- 2 HAND TIGHT JOINT, BETWEEN 1/8" TO 1/4", SWEEP JOINTS WITH POLYMERIC SAND
- 3 PAVERS - REFER TO LANDSCAPE MATERIALS SCHEDULE
- 4 1" TO 1-1/4" SCREED SAND SETTING BED. 1/2" SAND SET BASE, PER MANUFACTURER'S SPECIFICATIONS
- 5 COMPACTED AGGREGATE BASE / BITUMINOUS ASPHALT BASE PER CIVIL ENGINEER SPECIFICATIONS. GEOTEXTILE, MIRAFI 140N, OR APPROVED EQUAL SHALL BE USED FOR COMPACTED AGGREGATE BASE. TURN UP SIDES AT ABUTTING HARDSCAPE.
- 6 SETTING BED BASE COURSE PER CIVIL ENGINEER SPECIFICATIONS. SIEVE SIZE, DEPTH AND COMPACTION PER CIVIL ENGINEER SPECIFICATIONS.
- 7 UNDISTURBED OR COMPACTED SUB-BASE
- 8 GEOTEXTILE, MIRAFI 140N, OR APPROVED EQUAL SHALL BE USED FOR COMPACTED AGGREGATE BASE. TURN UP SIDES AT ABUTTING HARDSCAPE.

9 PAVING BRICK

SCALE: 1" = 1'-0"

101 WEST MAIN  
 101 WEST MAIN  
 FRISCO, COLORADO  
 SKETCH PLAN

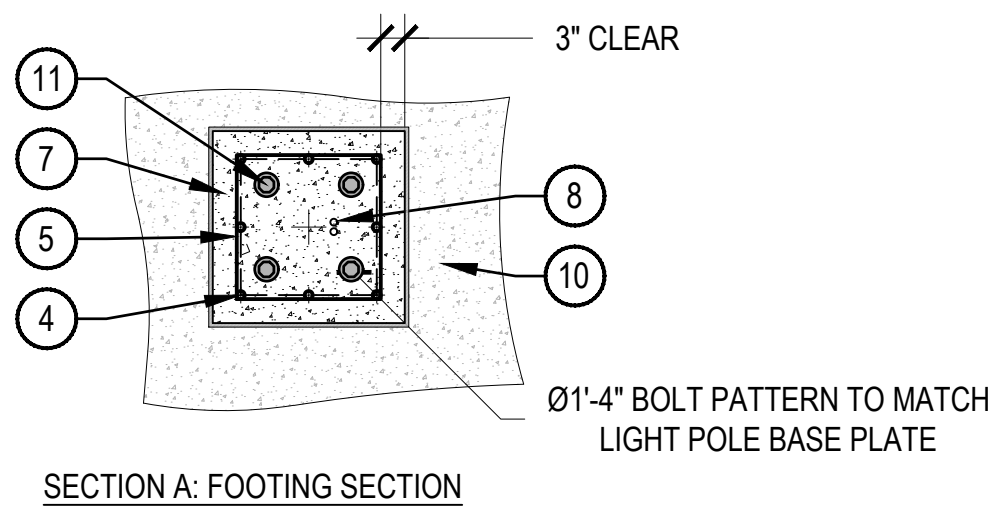
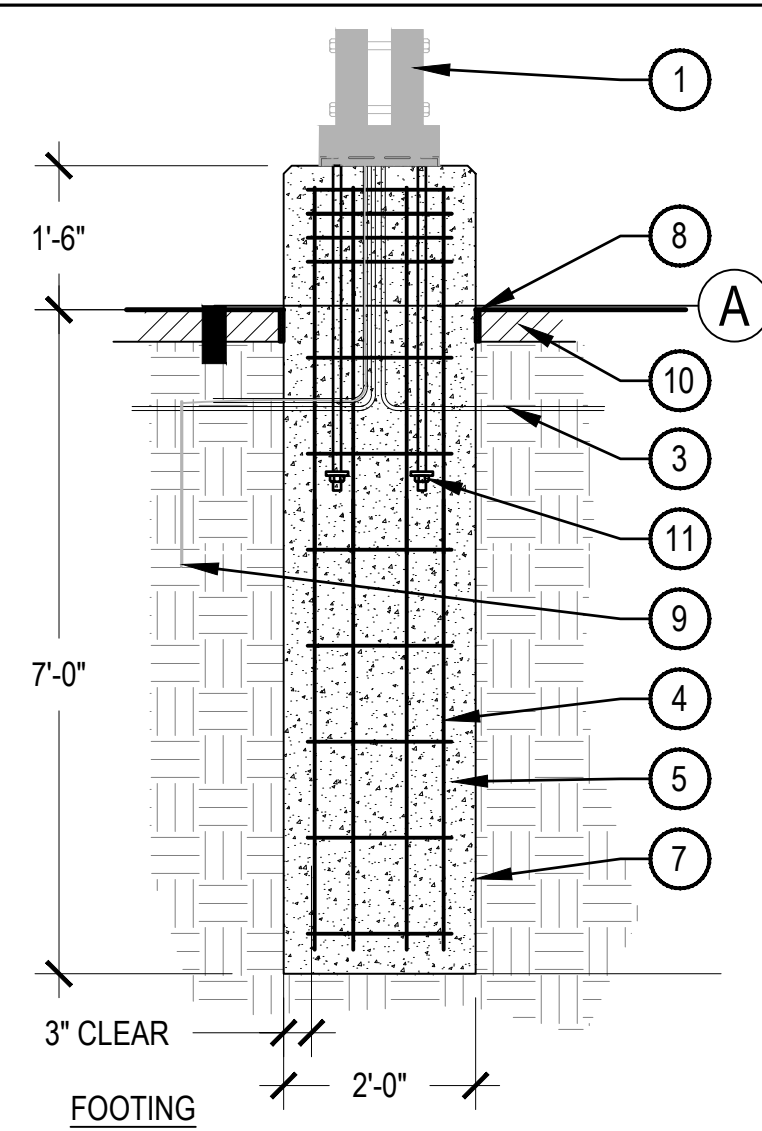
OWNER:  
 NHP FOUNDATION  
 122 EAST 42ND STREET  
 SUITE 4900  
 NEW YORK, NY 10168

NOT FOR CONSTRUCTION

811  
 Know what's below.  
 Call before you dig.

DATE:  
 12/11/23 MSP  
 05/13/24 MSP  
 05/31/24 MSP

SHEET TITLE:  
 LANDSCAPE  
 DETAILS



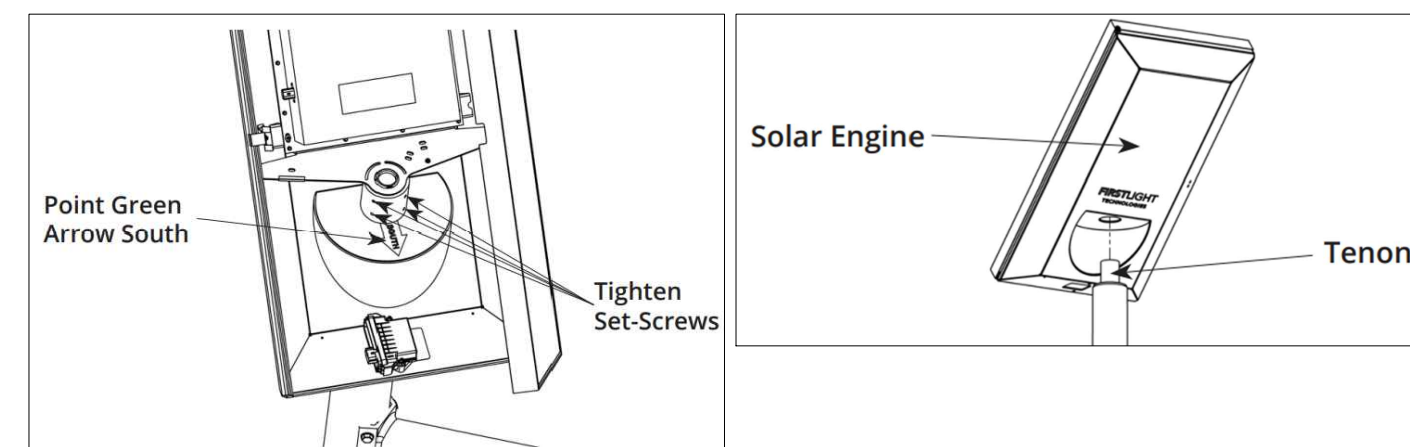
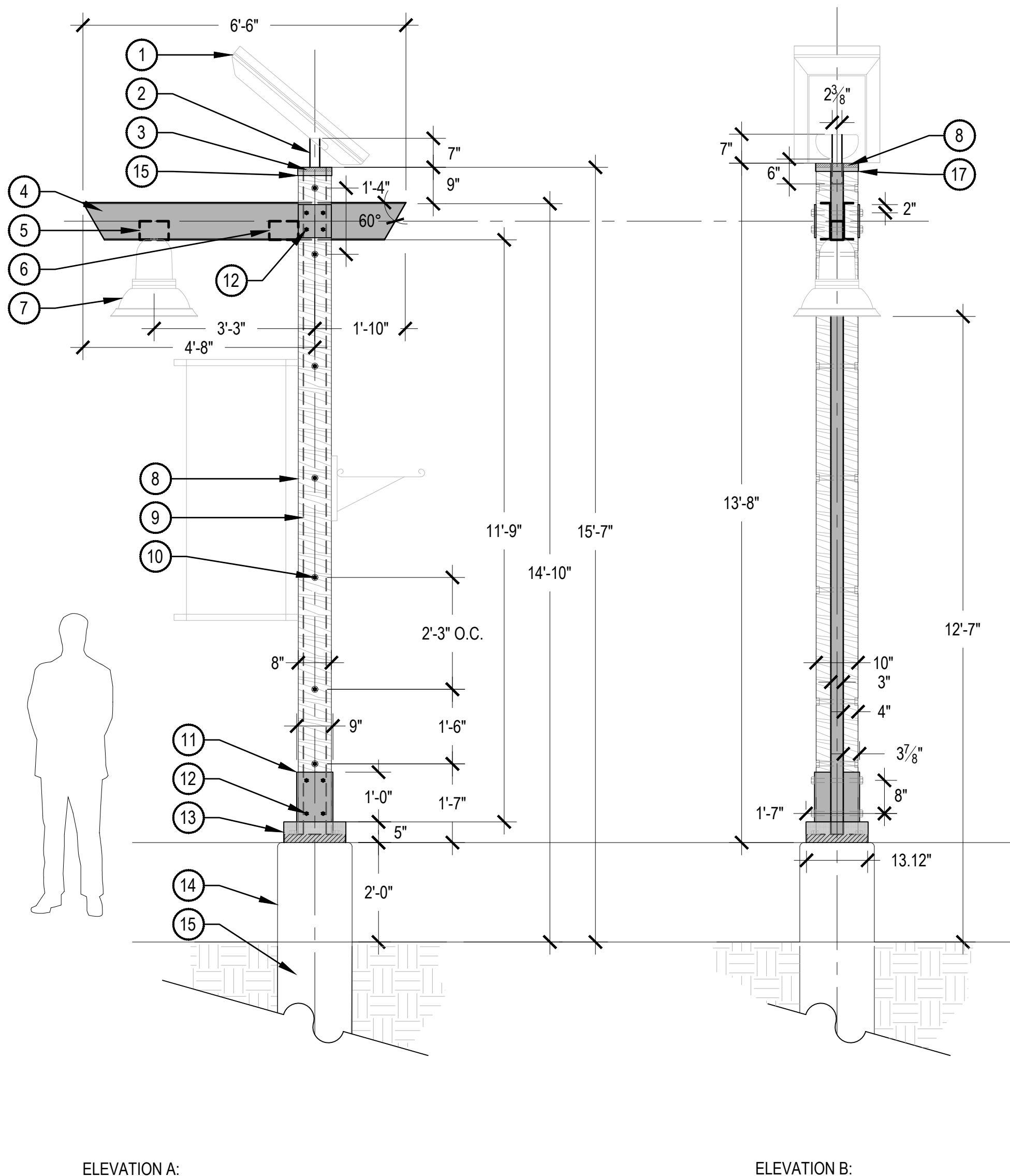
**NOTES:**

1. FOOTING TO BE REVIEWED AND STAMPED BY A LICENSED ENGINEER. BREAKAWAY TRANSFORMER BASE TO COMPLY WITH CDOT STANDARDS. STRUCTURAL ENGINEER TO ENSURE COMPLIANCE.
2. BREAKAWAY TRANSFORMER BASE TO BE DESIGNED TO WITHSTAND THE NECESSARY LOADING TO ACCOMMODATE THE TOWN OF FRISCO'S STANDARD STREET LIGHT DESIGN. CONTACT THE TOWN FOR DESIGN AND SPECIFICATIONS.

- ① FRISCO LIGHT POLE, DETAIL 4/ L-506
- ② NOT USED
- ③ 2" CONDUIT TO BE PLACED WITH TRACE WIRE
- ④ (8) # 5 VERTICAL REBAR, GRADE 60 MINIMUM
- ⑤ # 3 REBAR HOOPS, GRADE 60 MINIMUM, (4) 3" C/C TOP THEN (7) 12" C/C TO BOTTOM
- ⑥ CABLE ATTACHMENT, 3/8" STEEL PLATE, 3/16" CONTINUOUS WELDS
- ⑦ STANDARD CONCRETE, 4,000 PSI AT 28 DAYS
- ⑧ EXPANSION JOINT, DETAIL 2/ LSS01 WHEN FOOTING ABUTS ADJACENT CONCRETE POURS
- ⑨ GROUND WIRE WITH SEPARATE CONDUIT THROUGH BASE, REFER TO ELECTRICAL
- ⑩ ADJACENT MATERIALS, REFER TO PLANS
- ⑪ (4) 3/4" Ø F1554 HEADED ANCHOR RODS EMBEDDED 12" INTO CONCRETE FOOTING

**1 LIGHT POLE FOOTING**

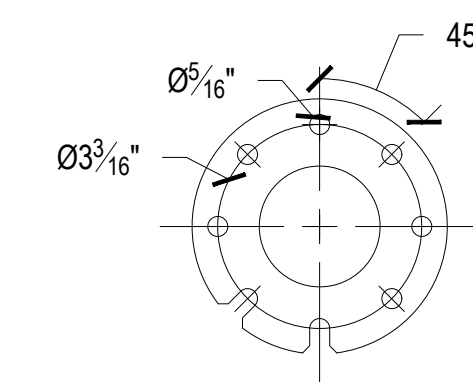
SCALE: 1/2" = 1'-0"



**FIRST LIGHT SOLAR ENGINE MOUNTING:**

**SCALE: NTS**

1. INSTALL PER MANUFACTURER SPECIFICATIONS.
2. ROUTE WIRING THROUGH THE TENON AND DOWN INTO THE TOP OF THE FIXTURE THROUGH THE FLANGE.



**FIRST LIGHT BELL LIGHT MOUNTING PATTERN:**

**SCALE: 1 1/2" = 1'-0"**

1. INSTALL PER MANUFACTURER SPECIFICATIONS.

**NOTES:**

1. NORRIS DESIGN HAS PROVIDED THIS DETAIL FOR MATERIAL AND DIMENSIONS, CONTRACTOR TO PROVIDE ENGINEERED SHOP DRAWINGS FOR REVIEW AND APPROVAL BY OWNER AND PERMITTING PURPOSES.
2. CONTRACTOR TO SUBMIT SHOP DRAWINGS FOR APPROVAL PRIOR TO CONSTRUCTION.
3. CONTRACTOR TO SUBMIT COLOR SAMPLES OF ALL FINISHES/MATERIALS FOR APPROVAL PRIOR TO FINALIZING ORDER.
4. CONTACT REPRESENTATIVE FOR DETAILS ON LIGHT FIXTURES AND SOLAR PANEL MODULE
- 4.1. REPRESENTATIVE: CHRIS DAVIS, THE LIGHTING AGENCY, 303.455.1012, cdavis@thelightingagency.com

- ① FIRST LIGHT TECHNOLOGIES SOLAR CELL, MOUNT SOLAR PANNEL TO FACE SOUTH.
- ② SOLAR ENGINE TENON MOUNT, 2-3/8" PIPE, EX TENON MUST EXTEND 7" BEYOND TOP OF POST CAP AND BE ALLOWED TO SWIVEL TO ALIGN SOLAR PANNEL TO FACE SOUTH.
- ③ 8"x10" - 3/4" THICK STEEL TENON SUPPORT PLATE WITH CENTERED 2.5" DIAMETER HOLE, WELD TO TOP OF STEEL CORE,
- ④ 9" C CHANNEL WELDED TO 3"x5" CORE, CUT ENDS OF CHANNEL AT 60 DEGREES
- ⑤ ELECTRICAL JUNCTION BOX AND LIGHT MOUNT, REFER TO BELL LIGHT MANUFACTURER FOR SPECIFICATIONS, CONFIRM MOUNTING PATTERN BEFORE FABRICATION
- ⑥ ADD ALTERNATE: ELECTRICAL JUNCTION BOX WITH GFI OUTLETS FOR SEASONAL LIGHTING
- ⑦ FIRST LIGHT TECHNOLOGIES BELL LIGHT; BFL-B, BLACK, 09 LIGHTING PROFILE 3000K, TYPE 4 OPTICS.
- ⑧ 4"x 8" CEDAR POST, STAIN TO MATCH EXISTING TOWN STANDARD
- ⑨ STEEL CORE, 3"x5" 1/4" THICK STEEL TUBE, BLACK POWDER COAT, PRE-PUNCH HOLES FOR ELECTRICAL OUTLETS AND BOLT LOCATIONS
- ⑩ 1" THICK GALVANIZED BOLTS WITH WASHERS PAINTED BLACK, 19" ABOVE BASE, 2'-3" O.C. X 6, 16", BOLTS SHALL BE RECESSED FLUSH WITH SURFACE
- ⑪ 3/16" STEEL PLATE, WRAP ON THREE SIDES, BLACK POWDER COAT, (4) 1" BOLTS
- ⑫ 1" THICK GALVANIZED BOLTS PAINTED BLACK, 4 BLOTS
- ⑬ 13.12" SQ x 5" - 1/16" THICK STEEL BASE CAP, BLACK POWDER COAT
- ⑭ 18" X 18" CONCRETE PEDESTAL, REFER TO DETAIL 503/3, LIGHT POLE FOOTER
- ⑮ 2" BLACK METAL POST CAP

**2 FRISCO LIGHT POLE**

SCALE: 1/2" = 1'-0"

**101 WEST MAIN**  
 101 WEST MAIN  
 FRISCO, COLORADO  
 SKETCH PLAN

**OWNER:**

NHP FOUNDATION  
122 EAST 42ND STREET  
SUITE 4900  
NEW YORK, NY 10168

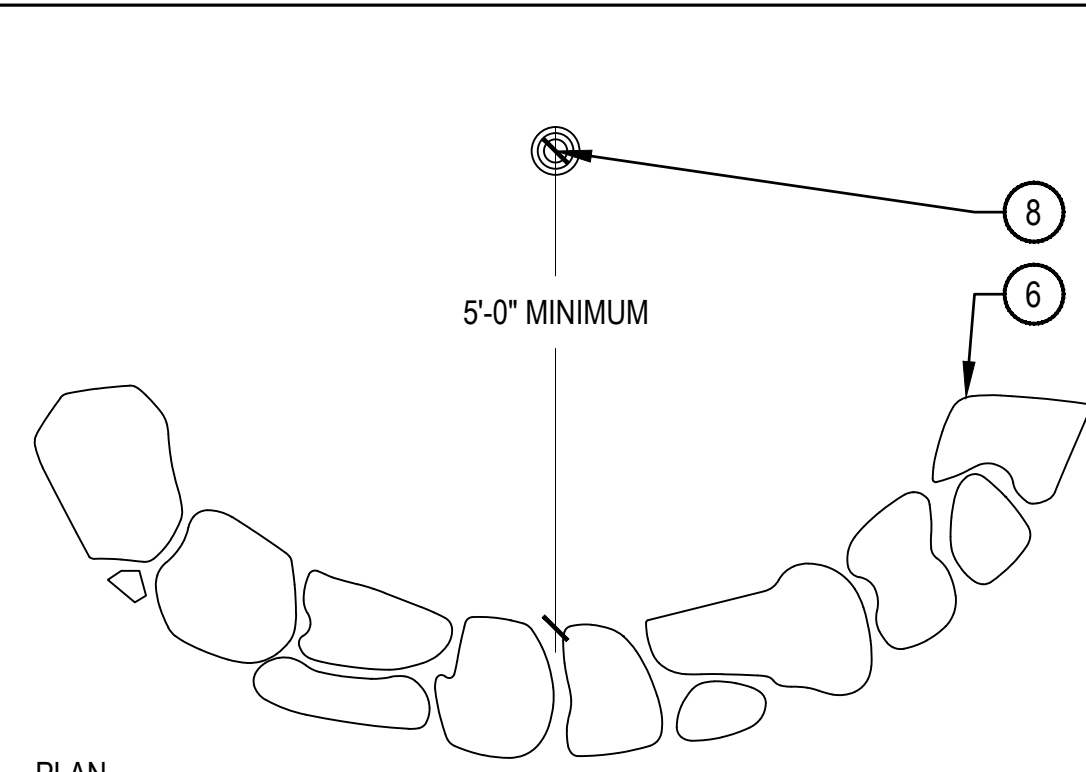
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05/13/24 MSP  
05/31/24 MSP

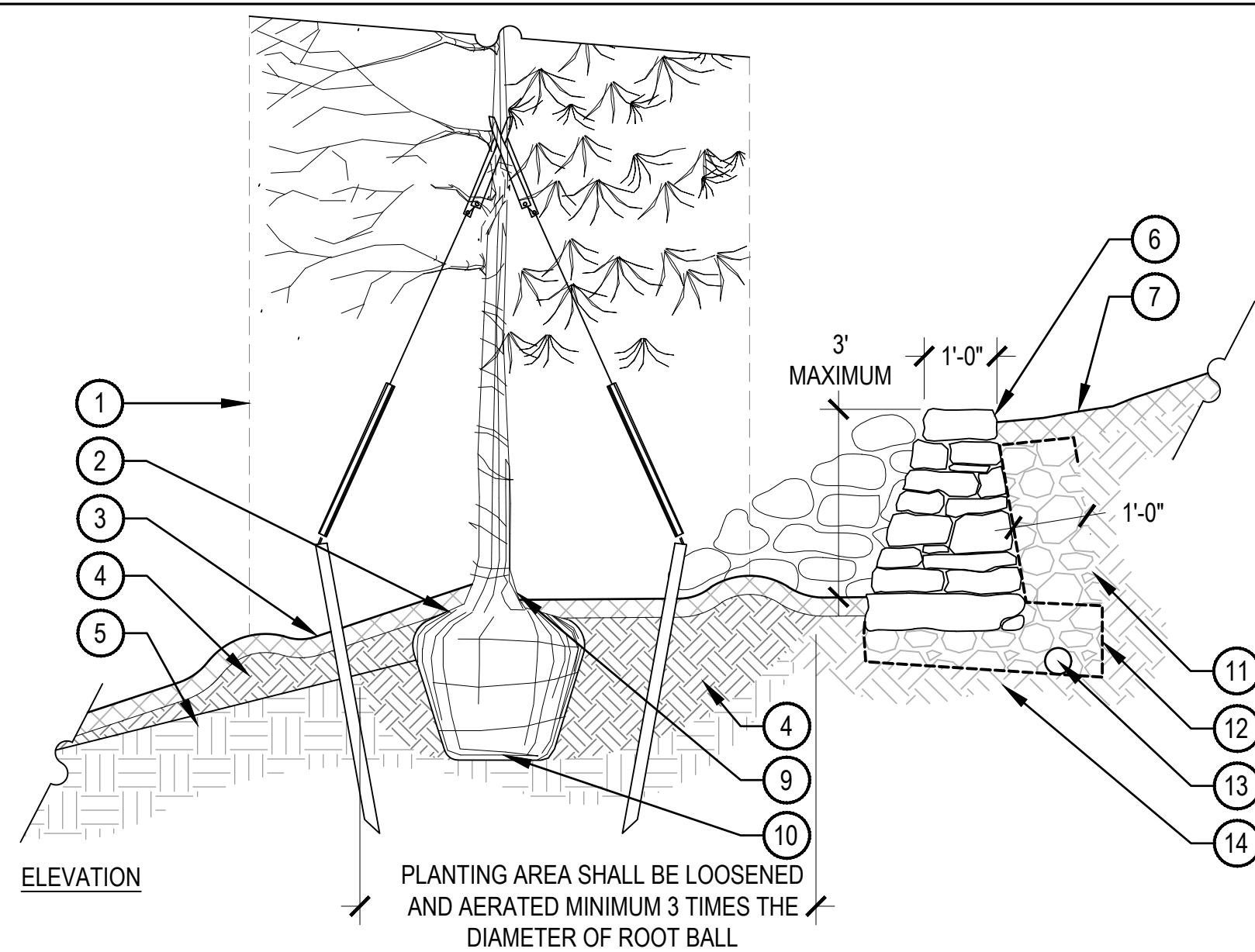
SHEET TITLE:  
LANDSCAPE  
DETAILS



PLAN

NOTES:

1. FOLLOW TYPICAL TREE PLANTING DETAILS FOR GUYING, WRAPPING, BACKFILL, ETC. REFER TO DETAIL X, SHEET L-XXX.
2. REFER TO PLAN FOR WALL HEIGHTS AND LAYOUT.



ELEVATION

PLANTING AREA SHALL BE LOOSENEED AND AERATED MINIMUM 3 TIMES THE DIAMETER OF ROOT BALL

- 1 TREE DRIP LINE
- 2 ROPES AT TOP OF BALL SHALL BE CUT, REMOVE TOP 1/3 OF BURLAP, NON-BIODEGRADABLE MATERIAL SHALL BE TOTALLY REMOVED
- 3 2-4" OF ORGANIC MULCH APPLIED OVER PLANTING AREA AND AWAY FROM THE TRUNK
- 4 BACKFILL
- 5 UNDISTURBED GRADE
- 6 DRYSTACKED GRANITE WALL, FRONT FACE OF WALL SHALL BE SMOOTH, FLAT FACE FORWARD, WITH JOINTS NO LARGER THAN 1", SLOPE AT A PITCH OF 3:1
- 7 ADJACENT LANDSCAPE, REFER TO PLAN
- 8 CENTER OF TREE
- 9 TOP MOST ROOT IN ROOTBALL: ABOVE EXISTING GRADE, UPHILL SIDE
- 10 ROOTBALL SITTING DIRECTLY ON TOP OF UNDISTURBED SOIL
- 11 AGGREGATE BACKFILL
- 12 MIRAFI 140 FILTER FABRIC PER GEOTECH
- 13 4" PERFORATED DRAIN PIPE, DRAIN TO DAYLIGHT
- 14 SUBGRADE COMPACTED TO 95% STANDARD PROCTOR DENSITY

1 BENCH TREE PLANTING

SCALE: 1/2" = 1'-0"

101 WEST MAIN  
101 WEST MAIN  
FRISCO, COLORADO  
SKETCH PLAN

OWNER:

NHP FOUNDATION  
122 EAST 42ND STREET  
SUITE 4900  
NEW YORK, NY 10168

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Know what's below.  
Call before you dig.

DATE:

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05/31/24 MSP

SHEET TITLE:

LANDSCAPE  
DETAILS

**Transportation Impact Analysis  
for  
101 West Main Street  
Frisco, Colorado**



**April 25, 2024  
Revised May 31, 2024**

PREPARED FOR:  
**Allen-Guerra Architecture**  
711 Granite Street, Unit B  
Frisco, CO 80443

PREPARED BY:  
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PO Box 4259  
Eagle, CO 81631  
970.623.0788



Contact: Kari J. McDowell Schroeder, PE, PTOE  
*Project Number: M1628*

## **Statement of Engineering Qualifications**

Kari J. McDowell Schroeder, PE, PTOE is a Transportation and Traffic Engineer for McDowell Engineering, LLC. Ms. McDowell Schroeder has over twenty-seven years of extensive traffic and transportation engineering experience. She has completed numerous transportation studies and roadway design projects throughout the State of Colorado. Ms. McDowell Schroeder is a licensed Professional Engineer in the State of Colorado and has her certification as a Professional Traffic Operations Engineer from the Institute of Transportation Engineers.



# Transportation Impact Analysis

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## 1.0 Project Description

McDowell Engineering has prepared this Level Three Auxiliary Transportation Impact Analysis for the proposed residential development at 101 West Main Street in Frisco, Colorado. The purpose of this transportation impact analysis is to forecast and analyze the impacts of the additional traffic volumes associated with the residential development on the surrounding roadway network.

The project site is located directly northeast of the Creekside Drive and West Main Street intersection. The proposed development will be constructed on a single lot. Several professional offices and businesses occupy the existing Westmain Professional building at 101 West Main Street project site. The owner is proposing to demolish the existing building and develop multifamily residential units with space for a small office building. The proposed site plan is shown in **Figure 1**.

### 1.1 Project Phasing

The residential development at 101 West Main Street is proposed to be constructed in one phase. This study analyzes a buildout condition of the subdivision with estimated completion in Year 2024. Analysis has been performed for both short-term buildout Year 2024 conditions as well as the long-range planning Year 2045 conditions.

### 1.2 Project Access Locations

The project site currently has one access located on the west side of the parcel with direct connectivity to Creekside Drive, opposite the Gateway Center shopping center access. The site plan shown in Error! Not a valid bookmark self-reference., shows the existing access being moved north to align with Streamside Lane and provide a greater stacking distance to West Main Street. The existing Creekside Drive and current site access intersection will become a three-legged intersection.

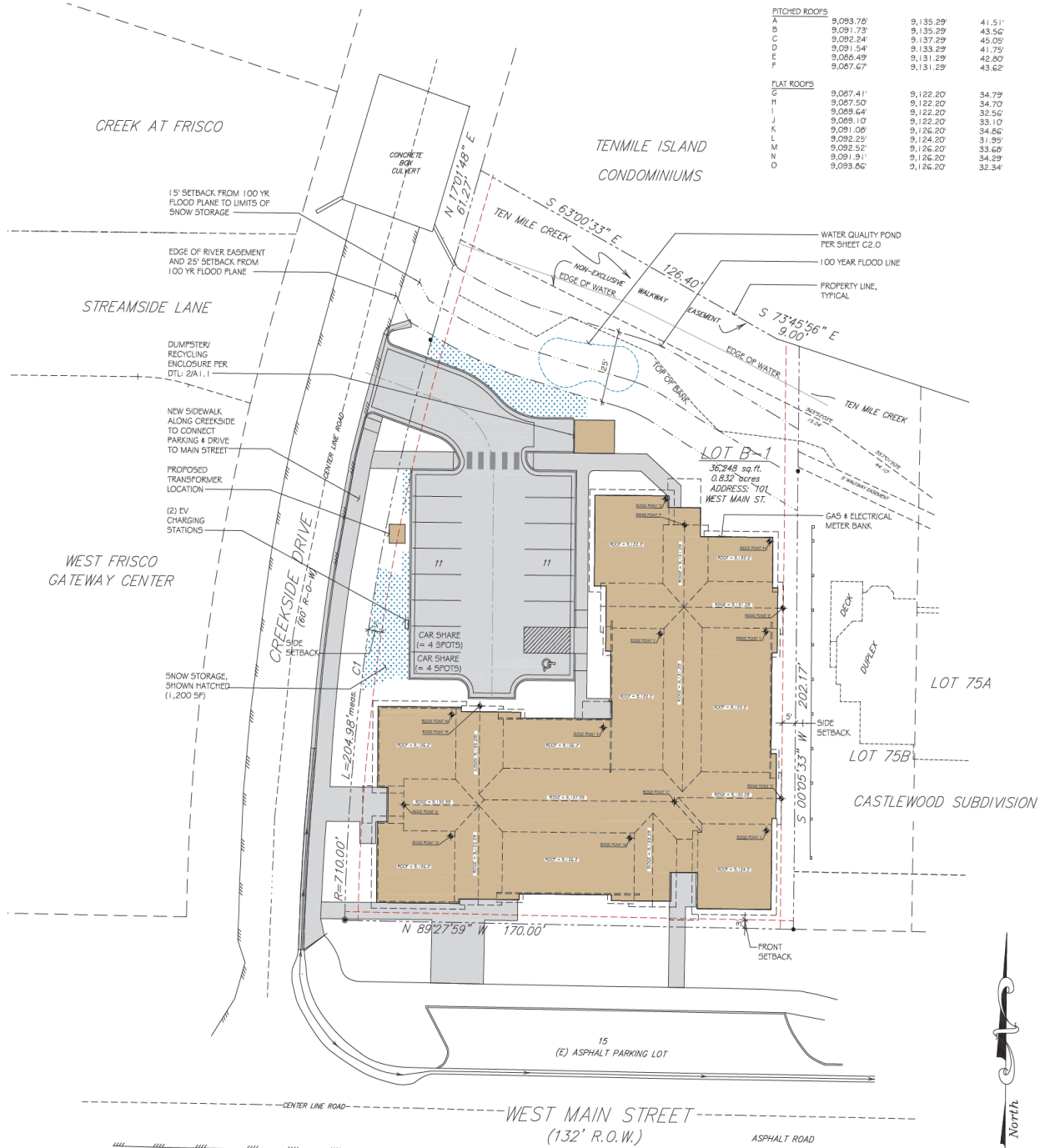
1. Creekside Drive & Proposed Site Access/Streamside Lane

### 1.3 Intersection Analysis Locations

In addition to the site accesses, this report also studies two additional off-site intersections:

1. Creekside Drive & Current Site Access
2. Creekside Drive & West Main Street

**Figure 1: Site Plan**



FITCHED ROOFS			
A	9,093.78'	9,135.29'	41.51'
B	9,091.73'	9,135.29'	43.56'
C	9,092.24'	9,137.29'	45.05'
D	9,091.54'	9,133.29'	41.75'
E	9,088.49'	9,131.29'	42.80'
F	9,087.67'	9,131.29'	43.62'
FLAT ROOFS			
G	9,087.41'	9,122.20'	34.79'
H	9,087.50'	9,122.20'	34.70'
I	9,089.64'	9,122.20'	32.56'
J	9,089.10'	9,122.20'	33.10'
K	9,091.06'	9,126.20'	34.86'
L	9,092.25'	9,124.20'	31.95'
M	9,092.52'	9,126.20'	33.68'
N	9,091.91'	9,126.20'	34.29'
O	9,093.66'	9,126.20'	32.34'

## 2.0 Existing Conditions

### 2.1 Road Network

West Main Street: West Main Street. is a two lane, east-west, paved roadway. This roadway is a major collected roadway that serves intracommunity traffic. The posted speed limit is 25mph near the project vicinity. West Main Street is traffic controlled by stop signs through the Town of Frisco.

Creekside Drive: Creekside Drive is a two lane, north-south, paved roadway. This roadway is a local roadway that serves neighborhood traffic over short distances to higher class roadways. Creekside Drive is stop-controlled and has a posted speed limit of 20mph in the project vicinity.

Streamside Lane: Streamside Lane is a two lane, east-west, paved roadway. This roadway is a local roadway that serves neighborhood traffic over a short distance to higher class roadways. Streamside Lane is stop-controlled and has a posted speed limit of 15mph in the project vicinity.

### 2.2 Traffic Data Collection

Current Year 2023 traffic data was collected at two intersections within the vicinity of the site. Weekday peak hour turning movement counts were taken on Thursday, November 09, 2023, from 7:00am – 9:00am and 4:00pm – 6:00pm. The weekday peak traffic occurred between 8:00am – 9:00am in the morning and 4:00pm – 5:00pm in the afternoon.

Traffic counts were not collected at the Creekside Drive & Streamside Lane intersection. Therefore, the traffic counts at the Creekside Drive and current site access intersection were used to estimate the traffic volumes at the Creekside Drive and Streamside Lane intersection.

These traffic counts were taken in November. However, historical traffic data shows that the traffic peak near Frisco, CO occurs in July. A seasonal adjustment factor was applied to the November traffic counts to equate them to peak season traffic counts. See **Section 3.4** for more details regarding the seasonal adjustment factor applied to the June traffic counts.

## 3.0 Infrastructure Assumptions

### 3.1 Existing & Committed Capital Improvement Projects

The Town of Frisco is not currently planning for any capital improvement projects near the project vicinity.

### 3.2 Planned or Existing Land Development Projects

There are currently no planned or existing land development projects near the project vicinity.

### 3.3 Background Traffic Growth

A traffic growth rate of 1.00% was used for the expected annual growth on the Town of Frisco's local roads. The same 1.00% traffic growth rate was used based on previous Town of Frisco transportation impact studies performed by McDowell Engineering. Additionally, many of the lots surrounding the roads studied in this analysis are nearly fully developed. Therefore, the 1.00% annual growth rate can be considered a conservative forecasting rate.

### 3.4 Seasonal Adjustment Factor

As mentioned in **Section 2.3**, a seasonal adjustment factor was used to convert the August 2023 counts to the peak July 2023 summer traffic volumes. CDOT's *OTIS*<sup>1</sup> has continuous traffic count data. The continuous traffic data was used to determine a 1.38 seasonal adjustment factor on State Highway 9 near Frisco, CO. This seasonal adjustment factor was then applied to the street network studied in this analysis. This factor was applied to the street network in this analysis. The continuous traffic count data used to derive the seasonal adjustment factor can be found in the **Appendix**.

### 3.5 Existing and Background Traffic Projections

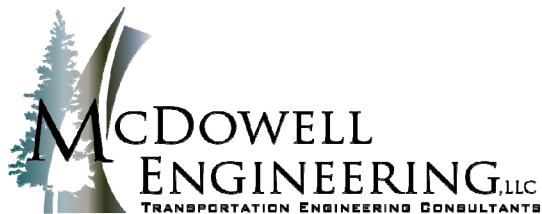
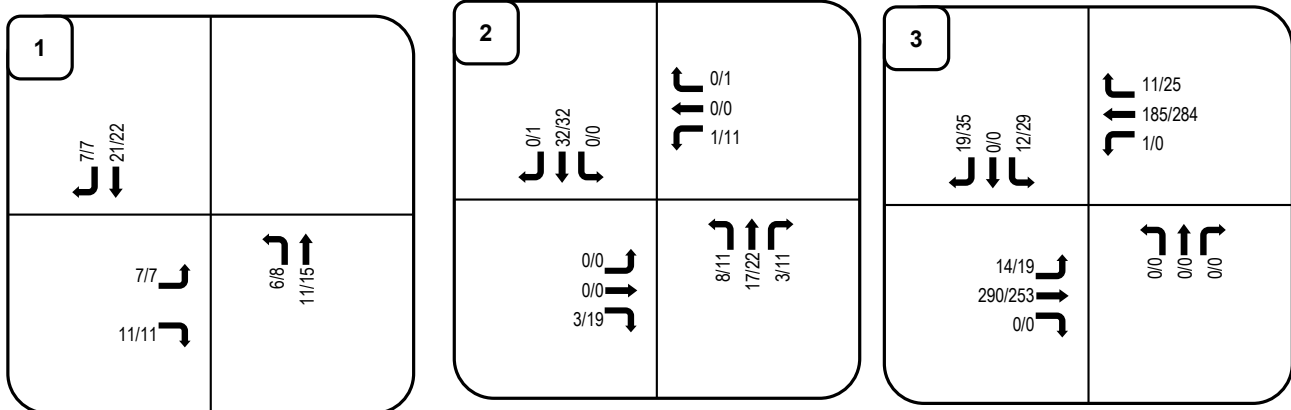
**Figure 2** below shows the Year 2023 seasonally adjusted existing traffic volumes. The raw traffic data collected can be found in the **Appendix**.

Projected Year 2024 and 2045 background traffic can be seen in **Figure 3** and **Figure 4**.

---

<sup>1</sup> Colorado Department of Transportation, Online Transportation Information System, 2023.

Figure 2: Year 2023 Seasonally Adjusted Existing Traffic

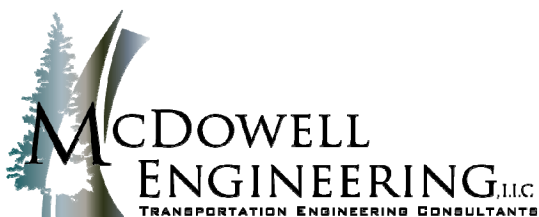
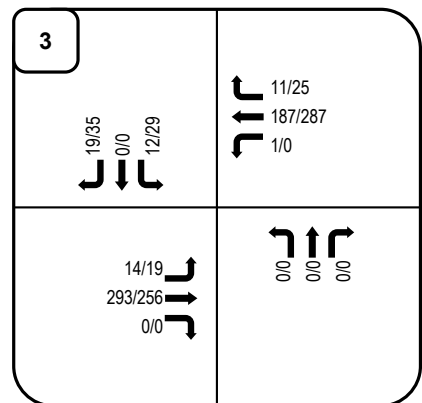
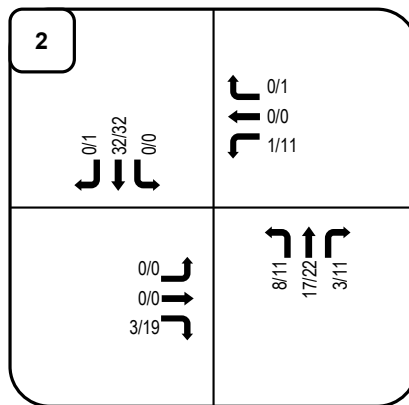
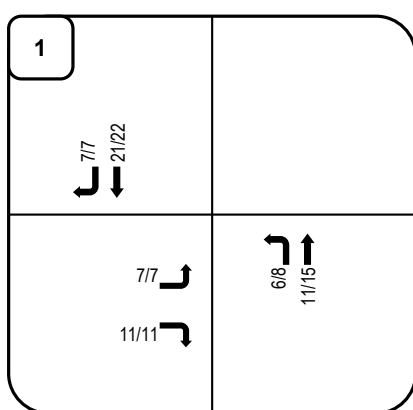


**LEGEND:**  
 Directional Distribution = Inbound% (Outbound %)  
 AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)  
 Turning Movements

Project Number  
 Prepared By

M1628  
 EP

Figure 3: Year 2024 Background Traffic



**LEGEND:**

Directional Distribution = Inbound% (Outbound %)

AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)

Turning Movements



Project Number

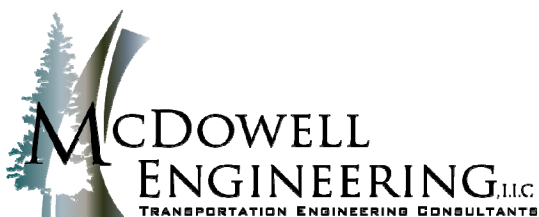
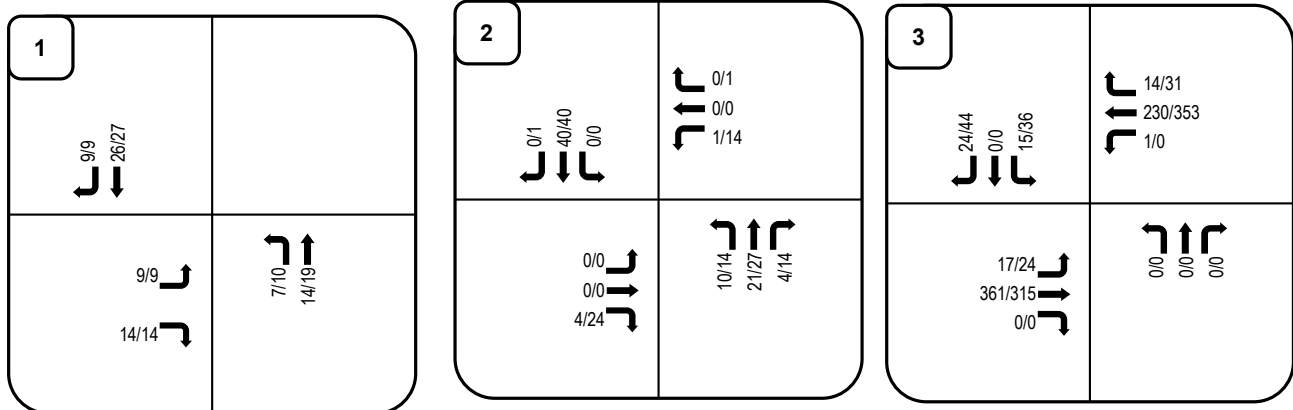
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EP



Figure 4: Year 2045 Background Traffic



**LEGEND:**

Directional Distribution = Inbound% (Outbound %)

AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)

Turning Movements



Project Number

M1628

Prepared By

EP

### 3.6 Background Intersection Traffic Levels of Service and Recommendations

Using *Highway Capacity Manual 6<sup>th</sup> Edition 2016<sup>2</sup> (HCM)* methodology, Synchro Version 10 software was used to determine the delay (in seconds) and Level of Service (LOS.) HCM LOS is defined by the following criteria:

Table 1: Year HCM Level of Service Criteria

LOS	Expected Delay to Minor Street Traffic	Average Signal Delay (Seconds/Vehicle)	Average Stop-Controlled Delay (Seconds/Vehicle)
A	Little or no delay.	0-10	0-10
B	Short traffic delays.	>10-20	>10-15
C	Average traffic delays.	>20-35	>15-25
D	Long traffic delays.	>35-55	>25-35
E	Very long traffic delays.	>55-80	>35-50
F	When volume exceeds the capacity of the lane extreme delays will be encountered with queuing that may cause severe congestion affecting other traffic movements in the intersection. This condition usually warrants improving the intersection.	>80	>50

**Table 2** shown below shows the resulting LOS as determined by HCM analysis:

Table 2: Background Traffic Level of Service

#	Int.	Traffic Control	Approach or Control Delay	Approach	Year 2023 Existing Level of Service (Delay in Seconds)		Year 2024 Background Level of Service (Delay in Seconds)		Year 2045 Background Level of Service (Delay in Seconds)	
					AM	PM	AM	PM	AM	PM
1	Creekside Dr & Streamside Dr (Future Site Acc.)	EB Stop	C	EB	A (8.6)	A (8.6)	A (8.6)	A (8.7)	A (8.7)	A (8.7)
			C	NB	A (2.2)	A (2.7)	A (2.7)	A (2.7)	A (2.6)	A (2.5)
			C	SB	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)
2	Creekside Dr & Current Site Acc.	EB/WB Stop	A	EB	A (8.4)	A (8.5)	A (8.5)	A (8.5)	A (8.5)	B (11.4)
			A	WB	A (8.8)	A (9.0)	A (9.0)	A (9.3)	A (9.1)	B (13.8)
			A	NB	A (2.3)	A (1.9)	A (2.2)	A (1.9)	A (2.2)	A (2.2)
			A	SB	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)
3	Creekside Dr & W Main St.	4-Way Stop	A	EB	A (8.7)	A (8.8)	A (10.0)	B (10.2)	B (11.5)	B (11.8)
			A	WB	A (8.1)	A (8.9)	A (8.8)	B (10.5)	A (9.6)	B (12.6)
			A	NB	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)
			A	SB	A (7.6)	A (8.0)	A (8.1)	A (8.7)	A (8.4)	A (9.3)

As can be seen in **Table 2**, all intersections are anticipated to operate at an acceptable overall LOS B or better through long-term Year 2045 background traffic conditions.

<sup>2</sup> Highway Capacity Manual, 6<sup>th</sup> Edition. Transportation Research Board, 2016.

Creekside Drive & Streamside Drive: This intersection is anticipated to operate at an acceptable LOS A through Year 2045 background traffic conditions.

Creekside Drive & Current Site Access: This intersection is anticipated to operate at an acceptable LOS B or better through Year 2045 background traffic conditions.

Creekside Drive & West Main Street: This intersection is anticipated to operate at an acceptable LOS B or better through Year 2045 background traffic conditions.

The corresponding Synchro reports can be found in the **Appendix**.

## 4.0 Project Traffic

### 4.1 Trip Generation

Existing: The existing Westmain Professional building currently has several professional offices and businesses. This existing building is estimated to be 12,169sf on the Summit County Assessor's records. The existing building is proposed to be demolished and replaced with the proposed residential development and small office space. The existing building falls under one land use code (LUC) per the Institute of Transportation Engineers' 11<sup>th</sup> Edition of the *Trip Generation Manual*<sup>3</sup> (*Trip Generation Manual*), #710 – General Office Building.

Proposed Residential Development: The owner is proposing to develop 52 residential dwelling units and a small 1,546sf office building. The proposed land uses fall under two LUC per the *Trip Generation Manual*<sup>4</sup>, #220 Multifamily Housing (Low-Rise), #712 – Small Office Building.

As per ITE's *Trip Generation Handbook*<sup>4</sup> methodology, the trip generation regression equations and rates were utilized as part of this analysis.

Multimodal Reduction: A 5% multimodal reduction was applied when calculating the total number of anticipated vehicular trips. The project site is located near several commercial/retail buildings. Biking or walking to these commercial/retail buildings is possible due to the proximity. The low-speed limits on West Main Street and Creekside Drive encourage multimodal modes of transportation.

The Summit Stage's Copper Mountain Route serves Frisco's Main Street, with a transit stop at West Main Street and Creekside Drive. This route also connects to the regional Frisco Transit Center, providing access to other Summit County and Front Range destinations.

Therefore, the 5% multimodal reduction is appropriate for the proposed residential development.

Project Trip Generation: The project is anticipated to generate 409 vehicle trips per day (vpd) on the average weekday. Peak hour traffic on a weekday at project buildout is anticipated to be 48 vehicles per hour (vph) during the morning peak hour (13 inbound + 35 outbound), 60vph during the evening peak hour (36 inbound + 24 outbound).

This is an anticipated increase of 223vpd from the existing uses in the Westmain Professional building. The morning traffic is anticipated to increase by 20vph, and the afternoon traffic volume generated by the site is anticipated to increase by 31vph.

Refer to **Table 3** for trip generation calculations and further breakdown of these trips.

---

<sup>3</sup> Trip Generation Manual, 11<sup>th</sup> Edition. Institute of Transportation Engineers, 2021.

<sup>4</sup> Trip Generation Handbook, An ITE Recommended Practice. Institute of Transportation Engineers, 2001.

**Table 3: Trip Generation Table**

ITE Code	Units <sup>2</sup>	Eq. Coef	ITE Trip Generation Equation <sup>3</sup>			Average Weekday Trips (vpd)	Morning Peak Hour		Evening Peak Hour	
			Avg. Weekday	AM Peak Hour	PM Peak Hour		Inbound	Outbound	Inbound	Outbound
<b>Existing Land Use</b>										
#710 - General Office Building	12.2 KSF	Type a= b=	B 0.87 3.05	B 0.86 1.16	B 0.83 1.29	186	88% 24	12% 4	17% 5	83% 24
<b>Existing Trips</b>						<b>186</b>	<b>24</b>	<b>4</b>	<b>5</b>	<b>24</b>
							<b>28</b>		<b>29</b>	
<b>Proposed Land Use</b>										
#220 - Multifamily Housing (Low-Rise)	52 DU	Type a= b=	A 6.41 75.31	A 0.35 28.13	A 0.42 34.78	409	24% 11	76% 35	62% 35	38% 22
#712 - Small Office Building	1.5 kSF	Type a= b=	Rate 14.39	Rate 2.61	Rate 3.15	22	60% 3	40% 2	42% 3	58% 3
<i>Multi-Modal Reduction</i>	<i>-5%</i>					<i>-22</i>	<i>-1</i>	<i>-2</i>	<i>-2</i>	<i>-1</i>
<b>Proposed Site Trips</b>						<b>409</b>	<b>13</b>	<b>35</b>	<b>36</b>	<b>24</b>
							<b>48</b>		<b>60</b>	
<b>Trip Generation Difference</b>						<b>223</b>	<b>-11</b>	<b>31</b>	<b>31</b>	<b>0</b>
							<b>20</b>		<b>31</b>	

**Notes:**

<sup>1</sup> Values obtained from *Trip Generation, 11th Edition*, Institute of Transportation Engineers, September 2021.

<sup>2</sup> DU = Dwelling Units, kSF = 1,000 Square Feet

<sup>3</sup> Fitted curve equations from ITE Land Uses - Equation Type A is  $T = a * X + b$ , Equation Type B is  $\ln(T) = a * \ln(X) + b$ , Rate is  $T = a * X$

## 4.2 Trip Distribution

The anticipated arrival and departure routes of project-generated traffic is influenced by several factors including the following:

- The location of the site relative to other facilities and the roadway network.
- The configuration of the existing and proposed adjacent roadway network.
- Relative location of neighboring population centers.

**Directional Distribution:** All the commercial developments and population centers are located south of the project site. Therefore, it was assumed 100% of the site-generated traffic would have destinations located south of the project site. Approximately 70% of the site-generated traffic was assumed to travel east towards Frisco at the West Main Street and Creekside Drive intersection. The remaining 30% was assumed to head west at the West Main Street and Creekside Drive intersection.

Refer to **Figure 5** for project-generated directional distribution breakdown.

### 4.3 Site-Generated Traffic

When the trip generation expected for the residential development (**Table 3**) is applied to the estimated trip distribution (**Figure 5**), the result is the anticipated assignment of trips on the roadway system. **Figure 6** depicts the new vehicle trips that are anticipated from the residential development.

### 4.4 Total Traffic

The total traffic anticipated is the sum of background traffic with the site-generated traffic.

For Year 2024, the background traffic (**Figure 3**) added to the site-generated traffic (**Figure 6**) yields the total Year 2024 traffic in **Figure 7**. For Year 2045, the background traffic (**Figure 4**) added to the site-generated traffic (**Figure 6**) yields the total Year 2045 traffic in **Figure 8**.

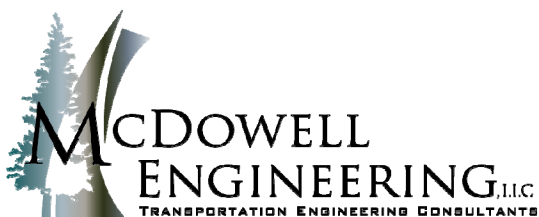
Figure 5: Project Generated Traffic Distribution (101 W Main St)



1	

2	

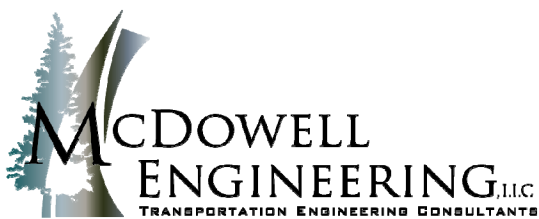
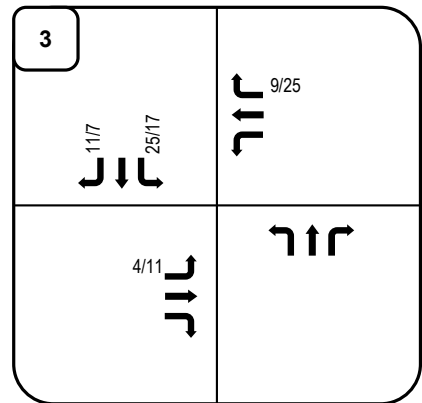
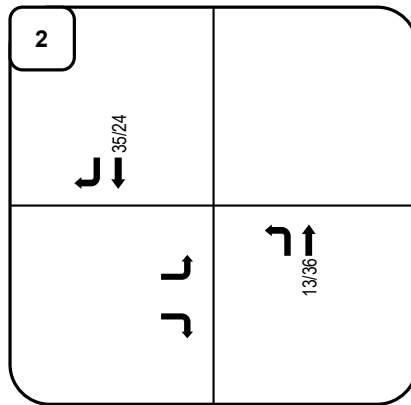
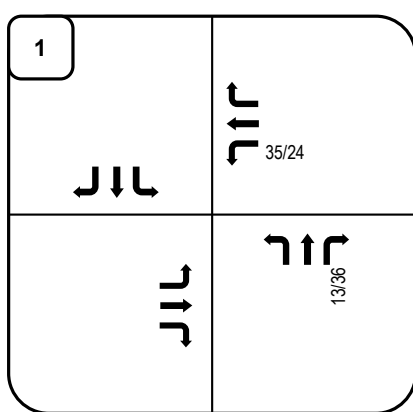
3	



**LEGEND:**  
 Directional Distribution = Inbound% (Outbound %)  
 AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)  
 Turning Movements

Project Number M1628  
 Prepared By EP

Figure 6: Project Generated Traffic Assignment (101 W Main St)



**LEGEND:**

Directional Distribution = Inbound% (Outbound %)

AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)

Turning Movements



Project Number

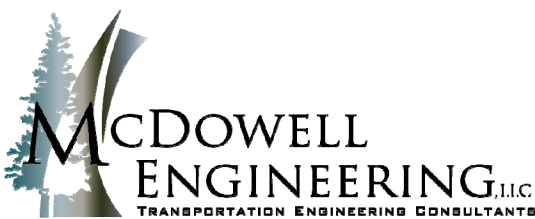
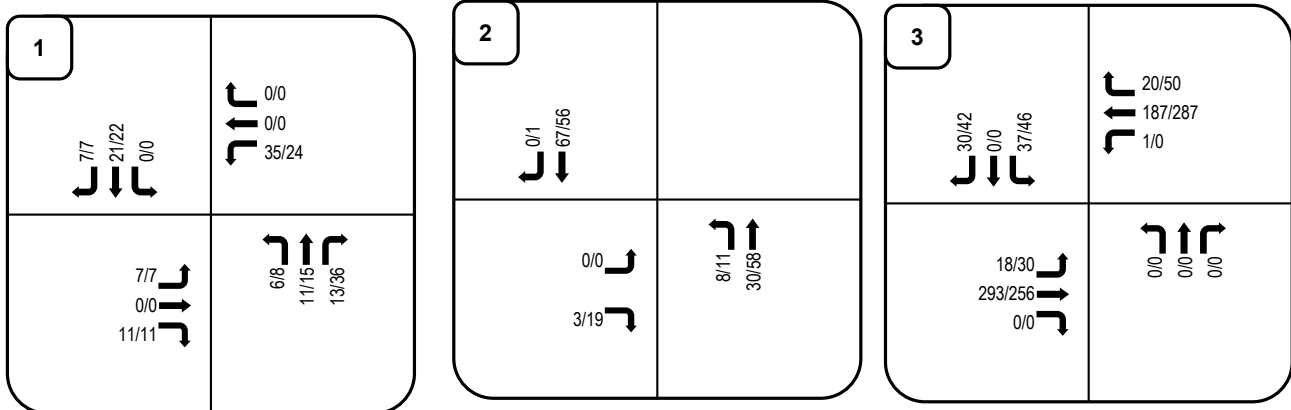
M1628

Prepared By

EP



Figure 7: Year 2024 Total Traffic

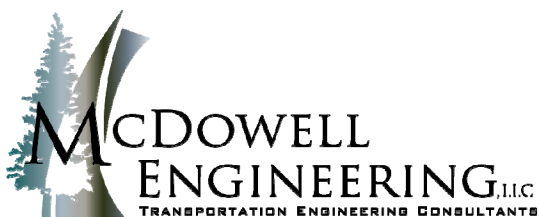


**LEGEND:**  
 Directional Distribution = Inbound% (Outbound %)  
 AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)  
 Turning Movements

Figure 8: Year 2045 Total Traffic



<p>1</p> <p>9/9 26/27 0/0</p> <p>0/0 0/0 35/24</p>	<p>2</p> <p>0/1 75/64</p> <p>0/0 4/24</p>	<p>3</p> <p>35/51 0/0 40/53</p> <p>23/56 230/353 1/0</p>
<p>9/9 0/0 14/14</p> <p>7/10 14/19 13/36</p>	<p>0/0 10/14 34/63</p>	<p>21/35 361/315 0/0</p> <p>0/0 0/0 0/0</p>



**LEGEND:**

Directional Distribution = Inbound% (Outbound %)

AM/PM/SAT Volumes = XX/XX/XX VPH (in PCEs)

Turning Movements

Project Number

M1628

Prepared By

EP

## 5.0 Traffic Analysis

### 5.1 Auxiliary Turn Lane Analysis

The results of the HCM modeling were used to determine auxiliary turn lane recommendations. Additionally, the roadway classification, posted speed limits, traffic volumes, and safety/operations were considered.

Creekside Drive & Streamside Lane/Future Site Access: This existing low-volume intersection of the site access with a local roadway does not anticipate a 95<sup>th</sup> percentile queue of more than one vehicle on any of the turning movements under future Year 2045 total traffic conditions. Therefore, no auxiliary turn lanes are recommended at this intersection.

Creekside Drive & Current Site Access: This proposed low-volume intersection of the site access with a local roadway does not anticipate a 95<sup>th</sup> percentile queue of more than one vehicle on any of the turning movements under future Year 2045 total traffic conditions. Therefore, no auxiliary turn lanes are recommended at this intersection.

Creekside Drive & Main Street: This proposed low-volume intersection of Creekside Drive (local roadway) and West Main Street (collector roadway) does not anticipate a 95<sup>th</sup> percentile queue of more than one vehicle on any of the turning movements under future Year 2045 total traffic conditions. Therefore, no auxiliary turn lanes are recommended at this intersection.

### 5.2 Total Traffic Level of Service

An *HCM* analysis under total traffic conditions was performed for the proposed site access under both short-term Year 2024 and long-term Year 2045 traffic conditions. The results can be seen in **Table 4**.

Table 4: HCM Total Traffic LOS

#	Int.	Traffic Control	Approach or Control Delay	Approach	Year 2024 Total Level of Service (Delay in Seconds)		Year 2045 Total Level of Service (Delay in Seconds)	
					AM	PM	AM	PM
1	Creekside Dr & Streamside Dr (Future Site Acc.)	EB/WB Stop	C	EB	A (8.7)	A (8.7)	A (8.7)	A (8.8)
			C	WB	A (9.1)	A (9.2)	A (9.3)	A (9.4)
			C	NB	A (1.6)	A (1.1)	A (1.6)	A (1.2)
			C	SB	A (0.0)	A (0.0)	A (0.0)	A (0.0)
2	Creekside Dr & Current Site Acc.	EB Stop	A	EB	A (8.7)	A (8.7)	A (8.7)	A (8.7)
			A	NB	A (1.5)	A (1.2)	A (1.7)	A (1.4)
			A	SB	A (0.0)	A (0.0)	A (0.0)	A (0.0)
3	Creekside Dr & W Main St.	4-Way Stop	A	EB	B (10.4)	B (10.7)	B (12.2)	B (12.7)
			A	WB	A (9.1)	B (11.1)	A (10.0)	B (13.8)
			A	NB	A (0.0)	A (0.0)	A (0.0)	A (0.0)
			A	SB	A (8.6)	A (9.1)	A (9.0)	A (9.8)

As can be seen in **Table 4**, all intersections are anticipated to operate at an acceptable overall LOS B or better through long-term Year 2045 total traffic conditions.

Creekside Drive & Streamside Drive: This intersection is anticipated to operate at an acceptable LOS A through Year 2045 total traffic conditions.

Creekside Drive & Current Site Access: This intersection is anticipated to operate at an acceptable LOS A through Year 2045 total traffic conditions.

Creekside Drive & West Main Street: This intersection is anticipated to operate at an acceptable LOS B or better through Year 2045 total traffic conditions.

The Synchro reports can be found in the **Appendix**.

### 5.3 Site Accesses Sight Distance

Sight distance requirements are determined by **Section 3.2.2** of the *American Association of State Highway and Transportation Officials (AASHTO): A Policy on Geometric Design of Highways and Streets*<sup>5</sup> (AASHTO’s Greenbook).

*Table 3-1 Stopping Sight Distance on Level Roadways*<sup>3</sup> identifies sight distance requirements based on speed limits. A roadway with a posted speed limit of 20mph requires a 115ft of sight distance. The civil and landscape design shall keep sight distance triangles clear of grading and landscaping that would impede a driver’s line of sight.

<sup>5</sup> AASHTO’s A policy on Geometric Design of Highways and Streets

## 6.0 Summary and Recommendations

McDowell Engineering has prepared this Level Three Auxiliary Transportation Impact Analysis for the proposed residential development at 101 West Main Street in Frisco, Colorado. The purpose of this transportation impact analysis is to forecast and analyze the impacts of the additional traffic volumes associated with the residential development on the surrounding roadway network.

The project site is located directly northeast of the Creekside Drive and West Main Street intersection. The owner is proposing to demolish the existing building and develop multifamily residential units with space for a small office building.

Trip Generation: The project is anticipated to generate 409vpd on the average weekday. Peak hour traffic on a weekday at project buildout is anticipated to be 48vph during the morning peak hour (13 inbound + 35 outbound), 60vph during the evening peak hour (36 inbound + 24 outbound).

This is an anticipated increase of 223vpd from the existing uses in the Westmain Professional building. The morning traffic is anticipated to increase by 20vph, and the afternoon traffic volume generated by the site is anticipated to increase by 31vph.

Site Access: The project site currently has one access located on the west side of the parcel with direct connectivity to Creekside Drive, opposite the Gateway Center shopping center access. The site plan shows the existing access being moved north to align with Streamside Lane and provide a greater stacking distance to West Main Street. The existing Creekside Drive and current site access intersection will become a three-legged intersection.

Background and Total Level of Service: As can be seen in **Table 2** and **Table 4**, all intersections are anticipated to operate at an acceptable overall LOS B or better through long-term Year 2045 total traffic conditions.

Site Access Sight Distance: A sight distance triangle of 115ft is required at the site access. Civil and landscape design shall keep sight distance triangles clear of grading and landscaping that would impede a driver's line of sight.

Turn Lane Analysis: Based upon traffic modeling results, no significant queues are anticipated at the study's analyzed intersections. Therefore, no auxiliary turn lanes are recommended at the study area intersections.

Based upon the analysis presented in this report, the proposed development at 101 West Main Street, Frisco, CO is anticipated to be successfully incorporated into the existing roadway network.

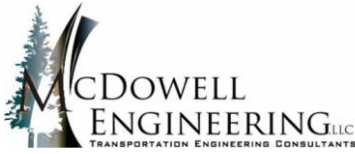
## 7.0 Appendix

### 7.1 Reference Documents

1. *State Highway Access Code*. State of Colorado, 2002.
2. Colorado Department of Transportation, Online Transportation Information System, 2023.
3. Highway Capacity Manual, 6<sup>th</sup> Edition. Transportation Research Board, 2016.
4. Trip Generation Manual, 11<sup>th</sup> Edition. Institute of Transportation Engineers, 2021.
5. Trip Generation Handbook, An ITE Recommended Practice. Institute of Transportation Engineers, 2001.
6. American Association of State Highway and Transportation Officials: A policy on Geometric Design of Highways and Streets 7<sup>th</sup> Edition, 2018.

### 7.2 Included Documents

1. McDowell Engineering Scoping Form
2. IDAX Traffic Counts
3. Seasonal Adjustment Factor Calculations
4. Summit County Assessor Data for Existing Site
5. Synchro reports



# Traffic Study Scoping Form

Contact Information	
Consultant Name:	McDowell Engineering
Tele:	(970)623-0788
E-mail:	<a href="mailto:kari@mcdowelleng.com">kari@mcdowelleng.com</a>
Developer/Owner Name:	NHP Foundation

Project Information <i>(Attach proposed site plan.)</i>								
Project Name:								
Project Location:		101 W Main Street, Frisco, CO 80443						
Project Description: Application type (rezoning, subdivision), acreage, new or re-development, etc.		Developing multifamily residential homes. 48 residential units, deed restricted housing (30 -100 ami). Potential for 4,000 sq. ft. of office space.						
Existing Land Uses	ITE Code	#units or Size	Proposed Land Uses	ITE Code	#units or Size	Existing / Proposed Land Uses	ITE Code	#units or Size
General Office Building	#710	10 KSF	Multifamily Housing Low Rise	#221	52 DU			
			Small Office Building	#712	1.5 KSF			
<i>Please attach Trip Generation Summary table for large or mixed use projects.</i>								

Assumptions			
Study Horizons	Current Year: 2023	Buildout Year: 2024	Long Term Year: 2045
Study Area Boundaries <i>(Attach map if needed.)</i>	North: Streamside Ln	South: W Main St	
	East: Existing Building	West: Creekside Dr	
Intersections to be Evaluated <i>(Attach map if needed.)</i>	1. W Main St & Creekside Dr	6.	
	2. Creekside Dr & Site Access	7.	
	3.	8.	
	4.	9.	
	5.	10.	
Trip Distribution	<i>See attached sketch.</i>		
Trip Reductions*	Internal Capture	Use: 0%	Pass By
	Multimodal Reduction	Use: 5%	Use: 0%
<i>*Include in Trip Generation table if provided. Submit calculations based upon ITE's Trip Generation Handbook.</i>			

**McDowell Engineering Traffic Study Scoping Form**

<b>Assumptions (continued)</b>			
Anticipated Future Traffic Growth Rates <i>(Describe methodology.)</i>	1.0% growth rate based on previous McDowell Engineering TIS studies in Frisco, CO	Study Time Periods <i>(Check all that apply.)</i>	<input checked="" type="checkbox"/> AM (7-9) <input checked="" type="checkbox"/> PM (4-6) <input type="checkbox"/> SAT (noon) <input type="checkbox"/> Other:
Other Factors <i>(Proposed/assumed transportation improvements, other studies, nearby proposed developments, etc.)</i>	Will apply a seasonal adjustment factor to convert the traffic counts to peak traffic volumes which are during the month of July. Trip generation table will be included in TIS report with the confirmed existing and proposed land uses.		
Analysis Methods & Issues <i>(Check all that apply.)</i>	<input checked="" type="checkbox"/> Synchro <input type="checkbox"/> HCS <input type="checkbox"/> aaSidra or Rodel <input type="checkbox"/> Intersections <input type="checkbox"/> Roadway Sections <input type="checkbox"/> Signal Warrants <input type="checkbox"/> Safety/Sight Distance <input type="checkbox"/> Queuing & Storage <input type="checkbox"/> CDOT (Access Permit, etc.) <input checked="" type="checkbox"/> Identify Bicycle, Pedestrian & Transit Accomodations <input type="checkbox"/> TDM <input type="checkbox"/> Neighborhood Impacts <input type="checkbox"/> Other:		

**Attachments, Notes, & Other Assumptions:**

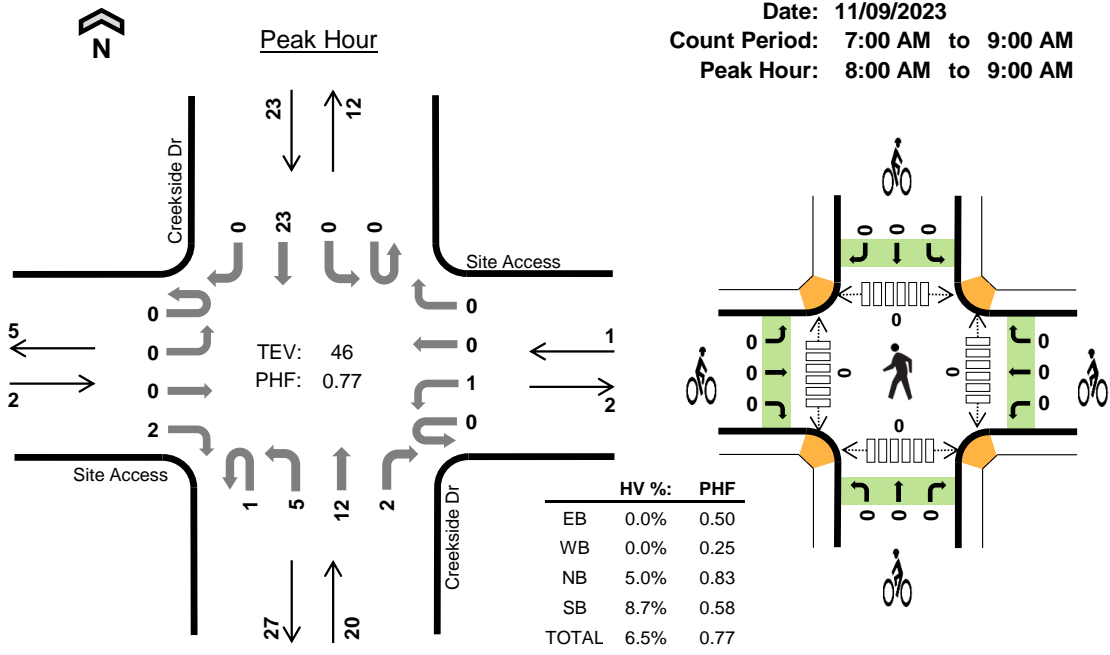
Signed: (Applicant or Consultant)  Print Name: (Applicant or Consultant)  Date:	Review Agency: Department:  Signed:  Print Name: Date:
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### Creekside Dr Site Access



Date: 11/09/2023  
 Count Period: 7:00 AM to 9:00 AM  
 Peak Hour: 8:00 AM to 9:00 AM



#### Two-Hour Count Summaries

Interval Start	Site Access Eastbound				Site Access Westbound				Creekside Dr Northbound				Creekside Dr Southbound				15-min Total	Rolling One Hour	
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT			
7:00 AM	0	0	0	1	0	0	0	0	0	0	3	0	0	0	3	0	7	0	
7:15 AM	0	0	0	2	0	0	0	0	0	0	2	0	0	0	1	0	5	0	
7:30 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	2	3	0	
7:45 AM	0	0	0	0	0	0	0	0	0	0	3	1	0	0	3	0	7	22	
8:00 AM	0	0	0	0	0	0	0	0	0	0	1	5	0	0	0	3	9	24	
8:15 AM	0	0	0	0	0	0	0	0	0	0	2	3	0	0	0	5	10	29	
8:30 AM	0	0	0	1	0	1	0	0	0	0	0	1	2	0	0	10	15	41	
8:45 AM	0	0	0	1	0	0	0	0	0	1	2	3	0	0	0	5	12	46	
Count Total	0	0	0	5	0	1	0	0	0	1	6	20	3	0	0	32	68	0	
Peak Hour	All	0	0	0	2	0	1	0	0	1	5	12	2	0	0	23	0	46	0
	HV	0	0	0	0	0	0	0	0	0	0	1	0	0	0	2	0	3	0
	HV%	-	-	-	0%	-	0%	-	-	0%	0%	8%	0%	-	-	9%	-	7%	0

Note: Two-hour count summary volumes include heavy vehicles but exclude bicycles in overall count.

Interval Start	Heavy Vehicle Totals					Bicycles					Pedestrians (Crossing Leg)				
	EB	WB	NB	SB	Total	EB	WB	NB	SB	Total	East	West	North	South	Total
7:00 AM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	2	0	1	3
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Count Total	1	0	2	2	5	0	0	0	0	0	0	2	0	1	3
Peak Hour	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0

<b>Two-Hour Count Summaries - Heavy Vehicles</b>																		
Interval Start	Site Access				Site Access				Creekside Dr				Creekside Dr				15-min Total	Rolling One Hour
	Eastbound				Westbound				Northbound				Southbound					
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT		
7:00 AM	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	1	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
8:15 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	2	2
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	3
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
Count Total	0	0	0	1	0	0	0	0	0	0	2	0	0	0	2	0	5	0
Peak Hour	0	0	0	0	0	0	0	0	0	0	1	0	0	0	2	0	3	0

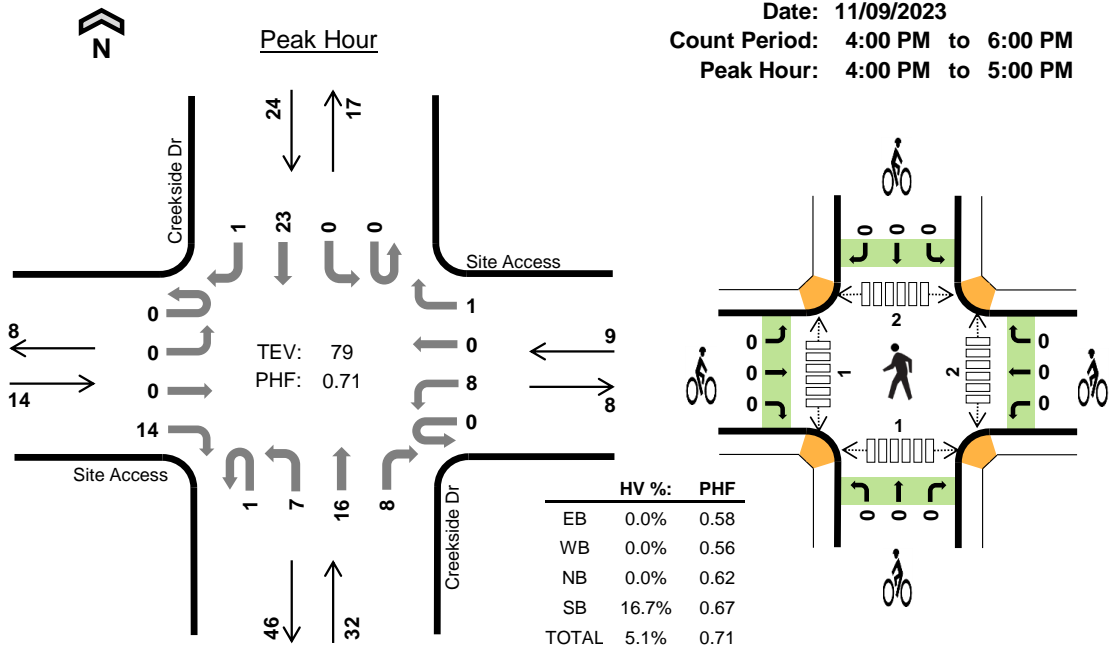
<b>Two-Hour Count Summaries - Bikes</b>																	
Interval Start	Site Access			Site Access			Creekside Dr			Creekside Dr			15-min Total	Rolling One Hour			
	Eastbound			Westbound			Northbound			Southbound							
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT					
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Count Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Note: U-Turn volumes for bikes are included in Left-Turn, if any.

### Creekside Dr Site Access



Date: 11/09/2023  
 Count Period: 4:00 PM to 6:00 PM  
 Peak Hour: 4:00 PM to 5:00 PM



#### Two-Hour Count Summaries

Interval Start	Site Access Eastbound				Site Access Westbound				Creekside Dr Northbound				Creekside Dr Southbound				15-min Total	Rolling One Hour	
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT			
4:00 PM	0	0	0	5	0	1	0	0	1	3	6	3	0	0	8	1	28	0	
4:15 PM	0	0	0	2	0	3	0	1	0	0	3	2	0	0	4	0	15	0	
4:30 PM	0	0	0	1	0	3	0	0	0	1	4	1	0	0	7	0	17	0	
4:45 PM	0	0	0	6	0	1	0	0	0	3	3	2	0	0	4	0	19	79	
5:00 PM	0	0	0	2	0	1	0	0	0	0	5	0	0	0	4	0	12	63	
5:15 PM	0	1	0	3	0	2	0	0	0	2	3	0	0	0	1	0	12	60	
5:30 PM	0	0	0	1	0	5	0	0	0	0	5	0	0	0	6	0	17	60	
5:45 PM	0	0	0	1	0	1	0	0	0	2	0	2	0	0	1	0	7	48	
Count Total	0	1	0	21	0	17	0	1	1	11	29	10	0	0	35	1	127	0	
Peak Hour	All	0	0	0	14	0	8	0	1	1	7	16	8	0	0	23	1	79	0
	HV	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	1	4	0
	HV%	-	-	-	0%	-	0%	-	0%	0%	0%	0%	0%	-	-	13%	100%	5%	0

Note: Two-hour count summary volumes include heavy vehicles but exclude bicycles in overall count.

Interval Start	Heavy Vehicle Totals					Bicycles					Pedestrians (Crossing Leg)					
	EB	WB	NB	SB	Total	EB	WB	NB	SB	Total	East	West	North	South	Total	
4:00 PM	0	0	0	2	2	0	0	0	0	0	0	0	0	0	1	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	1	1	0	0	0	0	0	1	1	2	0	4	4
4:45 PM	0	0	0	1	1	0	0	0	0	0	1	0	0	0	1	1
5:00 PM	0	0	0	0	0	0	0	0	0	0	1	1	0	0	2	2
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Count Total	0	0	0	4	4	0	0	0	0	0	4	2	2	1	9	9
Peak Hour	0	0	0	4	4	0	0	0	0	0	2	1	2	1	6	6

Two-Hour Count Summaries - Heavy Vehicles														15-min Total	Rolling One Hour			
Interval Start	Site Access				Site Access				Creekside Dr				Creekside Dr					
	Eastbound				Westbound				Northbound				Southbound					
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	2	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	4
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Count Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	1	4	0
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	1	4	0

Two-Hour Count Summaries - Bikes														15-min Total	Rolling One Hour
Interval Start	Site Access			Site Access			Creekside Dr			Creekside Dr					
	Eastbound			Westbound			Northbound			Southbound					
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT			
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Count Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	

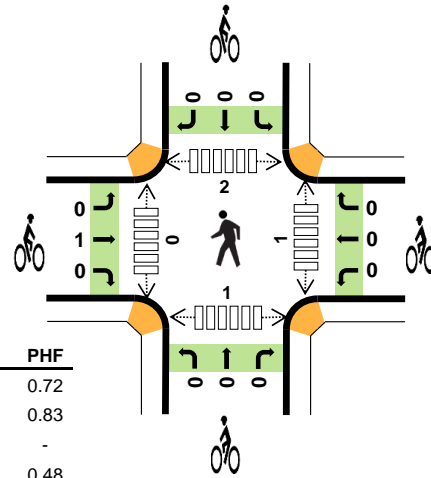
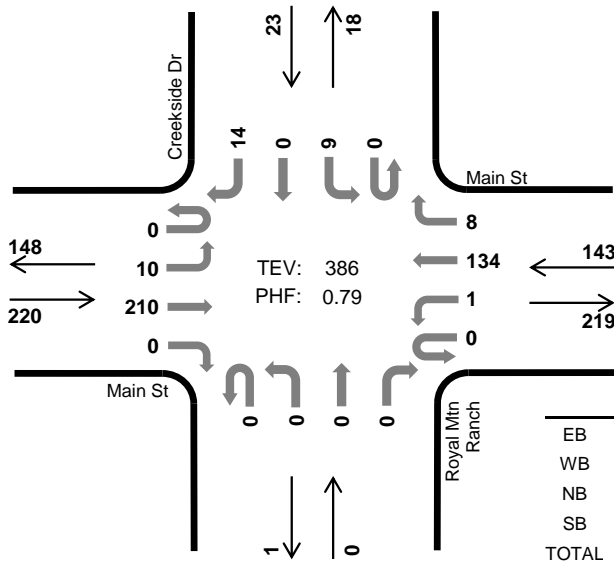
Note: U-Turn volumes for bikes are included in Left-Turn, if any.

### Creekside Dr Main St



Peak Hour

Date: 11/09/2023  
Count Period: 7:00 AM to 9:00 AM  
Peak Hour: 7:45 AM to 8:45 AM



	HV %:	PHF
EB	2.3%	0.72
WB	5.6%	0.83
NB	-	-
SB	8.7%	0.48
TOTAL	3.9%	0.79

#### Two-Hour Count Summaries

Interval Start	Main St Eastbound				Main St Westbound				Royal Mtn Ranch Northbound				Creekside Dr Southbound				15-min Total	Rolling One Hour	
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT			
7:00 AM	0	3	23	0	0	0	20	0	0	0	0	0	0	3	0	1	50	0	
7:15 AM	0	2	31	0	0	0	23	0	0	0	0	0	0	1	0	2	59	0	
7:30 AM	0	0	49	0	0	0	21	1	0	0	0	0	0	0	0	2	73	0	
<b>7:45 AM</b>	<b>0</b>	<b>4</b>	<b>72</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>42</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>122</b>	<b>304</b>	
8:00 AM	0	4	47	0	0	0	34	2	0	0	0	0	0	1	0	2	90	344	
8:15 AM	0	2	38	0	0	0	24	3	0	0	0	0	0	4	0	1	72	357	
8:30 AM	0	0	53	0	0	0	34	3	0	0	0	0	0	4	0	8	102	386	
8:45 AM	0	3	44	0	0	0	27	3	0	0	0	0	0	6	0	1	84	348	
Count Total	0	18	357	0	0	1	225	12	0	0	0	0	0	19	0	20	652	0	
Peak Hour	All	0	10	210	0	0	1	134	8	0	0	0	0	0	9	0	14	386	0
	HV	0	0	5	0	0	0	7	1	0	0	0	0	0	1	0	1	15	0
	HV%	-	0%	2%	-	-	0%	5%	13%	-	-	-	-	-	11%	-	7%	4%	0

Note: Two-hour count summary volumes include heavy vehicles but exclude bicycles in overall count.

Interval Start	Heavy Vehicle Totals					Bicycles					Pedestrians (Crossing Leg)				
	EB	WB	NB	SB	Total	EB	WB	NB	SB	Total	East	West	North	South	Total
7:00 AM	3	1	0	1	5	0	0	0	0	0	0	0	0	0	0
7:15 AM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0
7:30 AM	1	2	0	0	3	0	0	0	0	0	0	0	1	0	1
<b>7:45 AM</b>	<b>1</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>3</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>2</b>
8:00 AM	3	2	0	0	5	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	2	0	1	3	0	0	0	0	0	0	0	1	0	1
8:30 AM	1	2	0	1	4	1	0	0	0	1	0	0	1	0	1
8:45 AM	3	3	0	0	6	0	0	0	0	0	0	0	0	0	0
Count Total	13	14	0	3	30	1	0	0	0	1	1	0	3	1	5
Peak Hour	5	8	0	2	15	1	0	0	0	1	1	0	2	1	4

<b>Two-Hour Count Summaries - Heavy Vehicles</b>																		
Interval Start	Main St				Main St				Royal Mtn Ranch				Creekside Dr				15-min Total	Rolling One Hour
	Eastbound				Westbound				Northbound				Southbound					
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT		
7:00 AM	0	0	3	0	0	0	1	0	0	0	0	0	0	1	0	0	5	0
7:15 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0
7:30 AM	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	0	3	0
<b>7:45 AM</b>	<b>0</b>	<b>0</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>3</b>	12
8:00 AM	0	0	3	0	0	0	2	0	0	0	0	0	0	0	0	0	5	12
8:15 AM	0	0	0	0	0	0	1	1	0	0	0	0	0	1	0	0	3	14
8:30 AM	0	0	1	0	0	0	2	0	0	0	0	0	0	0	0	1	4	15
8:45 AM	0	0	3	0	0	0	3	0	0	0	0	0	0	0	0	0	6	18
Count Total	0	1	12	0	0	0	13	1	0	0	0	0	0	2	0	1	30	0
Peak Hour	0	0	5	0	0	0	7	1	0	0	0	0	0	1	0	1	15	0

<b>Two-Hour Count Summaries - Bikes</b>																		
Interval Start	Main St			Main St			Royal Mtn Ranch			Creekside Dr			15-min Total	Rolling One Hour				
	Eastbound			Westbound			Northbound			Southbound								
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT						
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>7:45 AM</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
Count Total	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0
Peak Hour	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0

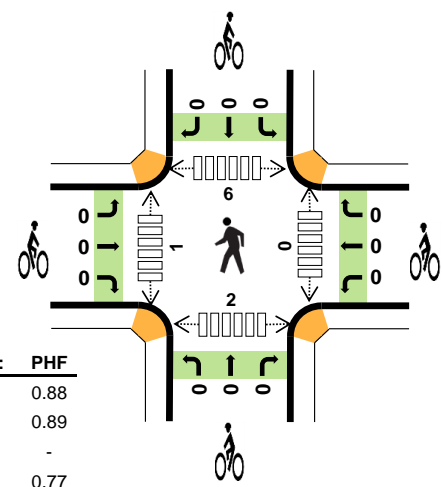
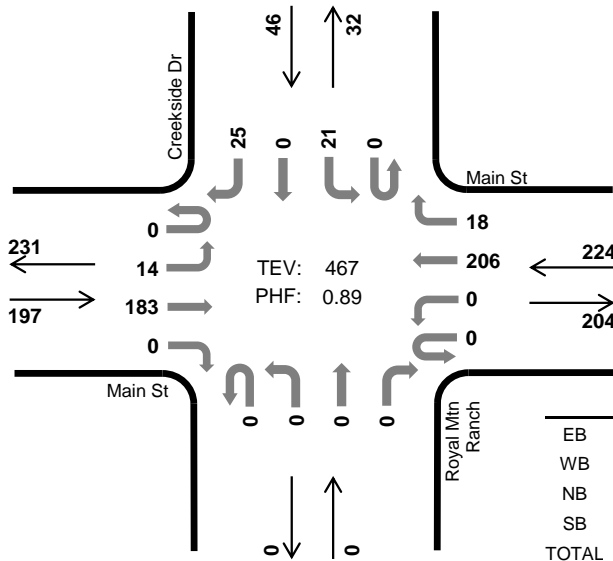
Note: U-Turn volumes for bikes are included in Left-Turn, if any.

### Creekside Dr Main St



Peak Hour

Date: 11/09/2023  
Count Period: 4:00 PM to 6:00 PM  
Peak Hour: 4:00 PM to 5:00 PM



	HV %:	PHF
EB	2.5%	0.88
WB	0.9%	0.89
NB	-	-
SB	6.5%	0.77
TOTAL	2.1%	0.89

#### Two-Hour Count Summaries

Interval Start	Main St Eastbound				Main St Westbound				Royal Mtn Ranch Northbound				Creekside Dr Southbound				15-min Total	Rolling One Hour	
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT			
4:00 PM	0	6	50	0	0	0	53	7	0	0	0	0	0	7	0	8	131	0	
4:15 PM	0	3	45	0	0	0	61	2	0	0	0	0	0	4	0	5	120	0	
4:30 PM	0	1	45	0	0	0	51	5	0	0	0	0	0	4	0	7	113	0	
4:45 PM	0	4	43	0	0	0	41	4	0	0	0	0	0	6	0	5	103	467	
5:00 PM	0	2	41	0	0	0	52	3	0	0	0	1	0	5	0	2	106	442	
5:15 PM	0	1	46	0	0	1	40	4	0	0	0	0	0	5	0	1	98	420	
5:30 PM	0	4	33	0	0	0	42	2	0	0	0	0	0	11	0	1	93	400	
5:45 PM	0	0	46	0	0	0	51	3	0	0	0	1	0	1	0	2	104	401	
Count Total	0	21	349	0	0	1	391	30	0	0	0	2	0	43	0	31	868	0	
Peak Hour	All	0	14	183	0	0	0	206	18	0	0	0	0	0	21	0	25	467	0
	HV	0	0	5	0	0	0	2	0	0	0	0	0	0	0	0	3	10	0
	HV%	-	0%	3%	-	-	-	1%	0%	-	-	-	-	-	0%	-	12%	2%	0

Note: Two-hour count summary volumes include heavy vehicles but exclude bicycles in overall count.

Interval Start	Heavy Vehicle Totals					Bicycles					Pedestrians (Crossing Leg)				
	EB	WB	NB	SB	Total	EB	WB	NB	SB	Total	East	West	North	South	Total
4:00 PM	4	0	0	1	5	0	0	0	0	0	0	0	3	1	4
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	1	1	0	1	3	0	0	0	0	0	0	1	3	1	5
4:45 PM	0	1	0	1	2	0	0	0	0	0	0	0	0	0	0
5:00 PM	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0
5:15 PM	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0
5:30 PM	1	3	0	0	4	0	0	0	0	0	0	0	2	0	2
5:45 PM	2	0	0	0	2	0	0	0	0	0	0	0	1	0	1
Count Total	12	5	0	3	20	0	0	0	0	0	0	1	9	2	12
Peak Hour	5	2	0	3	10	0	0	0	0	0	0	1	6	2	9

Two-Hour Count Summaries - Heavy Vehicles														15-min Total	Rolling One Hour			
Interval Start	Main St				Main St				Royal Mtn Ranch				Creekside Dr					
	Eastbound				Westbound				Northbound				Southbound					
	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT	UT	LT	TH	RT		
4:00 PM	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	1	5	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	1	3	0
4:45 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	2	10
5:00 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	7
5:15 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	9
5:30 PM	0	0	1	0	0	0	3	0	0	0	0	0	0	0	0	0	4	10
5:45 PM	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	10
Count Total	0	0	12	0	0	0	5	0	0	0	0	0	0	0	0	3	20	0
Peak Hour	0	0	5	0	0	0	2	0	0	0	0	0	0	0	0	3	10	0

Two-Hour Count Summaries - Bikes														15-min Total	Rolling One Hour		
Interval Start	Main St			Main St			Royal Mtn Ranch			Creekside Dr							
	LT	TH	RT	LT	TH	RT	LT	TH	RT	LT	TH	RT					
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Count Total	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Peak Hour	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

Note: U-Turn volumes for bikes are included in Left-Turn, if any.



## Monthly Summary Data

CDOT OTIS Station ID 000240, ON US 9 South of Frisco

CALYR	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
2023	25561	26129	25423	20266	17932	22107	26085	24365				
2022	25343	26314	25528	19385	17755	22419	25576	25157	23340	20126	19542	23392
2021	23613	23681	24455	19981	18729	23885	26569	24052	22303	19357	19779	23740
2020	26091	24334	16206	8459	12873	20096	25398	24184	23870	21272	18851	22557
2019	26864	25558	25043	19475	17420	19707	22715	25287	23769	18932	19522	23106
2018	24454	23112	23746	17638	16681	21491	25586	23805	21848	17993	19613	24572
2017	22314	22238	22640	16863	15739	20133	23872	22365	20694	17736	17914	22213
2016	20067	20166	19771	15583	15315	20234	24369	22538	21058	17606	17498	20596
2015	22254	24105	22746	16768	14626	19975	24189	22612	20612	17216	16072	18628
2014	22074	21741	22695	17203								21545
2013	19202	21802	21597	15304	14226	18639	23409	22113	18964	16174	17346	20515
2012	21020	20867	21856	14988	13970	18252	21689	21020	18465	15317	14911	16463
2011	19904	19013	19970	14963	12362	17049	20609	20745	18187	15322	15089	20645
2010	21080	20612	21125	15130	13030	17052	21181	19662	18019	14514	15010	19458
2009	21034	20678	20526	15689	13641	17657	21077	19819	17562	14586	15975	19679
2008	20905	20794	21778	16242	14218	17918	21285	20247	18055	15894	15800	19566
2007	17702	17620	18759	14050	12673				18771	16492	17633	19734
2006		20998	21722	16262	14383	18215	21499	19876	17816	14343	16474	17318
2005	18989	20995	21210	15207	13908	18387	21607	19988	17201	14418	14704	16870
2004	19416	19455	20094	14881	13235	16055	23816	22623	20655	16435	11737	19643
2003	20478	20373	20468	15362	13761	18156	20179	19480	16843	14964	14829	18654
2002	20789	21729	22454	16137	14536	17957	22030	20763	18125	15505	16223	19895
Average	21,864	21,923	21,810	16,174	14,810	19,269	23,137	22,035	19,808	16,710	16,726	20,419

## Seasonal Adjustment Factors

CDOT OTIS Station ID 000240, ON US 9 South of Frisco

	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
	21,864	21,923	21,810	16,174	14,810	19,269	23,137	22,035	19,808	16,710	16,726	20,419
Jan	21,864	1.00	1.00	0.74	0.68	0.88	1.06	1.01	0.91	0.76	0.76	0.93
Feb	21,923	1.00	1.00	0.74	0.68	0.88	1.06	1.01	0.90	0.76	0.76	0.93
Mar	21,810	1.00	1.01	1.00	0.74	0.68	0.88	1.06	0.91	0.77	0.77	0.94
Apr	16,174	1.35	1.36	1.35	1.00	0.92	1.19	1.43	1.36	1.22	1.03	1.26
May	14,810	1.48	1.48	1.47	1.09	1.00	1.30	1.56	1.49	1.34	1.13	1.38
Jun	19,269	1.13	1.14	1.13	0.84	0.77	1.00	1.20	1.14	1.03	0.87	1.06
Jul	23,137	0.95	0.95	0.94	0.70	0.64	0.83	1.00	0.95	0.86	0.72	0.88
Aug	22,035	0.99	0.99	0.99	0.73	0.67	0.87	1.05	1.00	0.90	0.76	0.93
Sep	19,808	1.10	1.11	1.10	0.82	0.75	0.97	1.17	1.11	1.00	0.84	1.03
Oct	16,710	1.31	1.31	1.31	0.97	0.89	1.15	1.38	1.32	1.19	1.00	1.22
Nov	16,726	1.31	1.31	1.30	0.97	0.89	1.15	1.38	1.32	1.18	1.00	1.22
Dec	20,419	1.07	1.07	1.07	0.79	0.73	0.94	1.13	1.08	0.97	0.82	1.00

## Monthly Summary Data from CDOT OTIS:

<https://dtdapps.coloradodot.info/otis/TrafficData#ui/0/0/1/station/000126/criteria//19/false/true/>

Data Retrieved on November 28, 2023

Schedule # 1100169

Property Desc: **LOT B-1 WEST FRISCO 70 SUB # 2**  
 Phys. Address: **101 W MAIN ST**

Ownership  
 Primary: **NHPF WEST MAIN LLC,**  
 Secondary:

Mailing Address  
 C/O **C/O THE NHP FOUNDATION**  
 Addr: **122 E 42ND ST STE 4900**  
 CSZ: **NEW YORK NY 101680002**

Subdiv Filing Phase Block Lot  
 SubCode **1760** **2** **0** **0** **B-1**  
 PPI: **2097-3413-07-001**  
 Econ: **2** **Frisco**  
 Nhood: **33200** **Frs Wst Mn**  
 TaxArea: **11** 2023 Tship: **5**  
 AssdVal: **631321** **Est. Tax/Tax Rate:** **57.936** Range: **78**

Reception	Sale Date	Document Type	Sale Price
1312894	6/15/2023	AGM	0
1312893	6/16/2023	HRC	0
1312889	6/22/2023	STM	0
1312888	6/14/2023	SWD	\$5,004,500
1124476	10/11/2016	QCD	0
1124310	10/4/2016	QCD	0
1123015	8/22/2016	QCD	0
1120137	8/26/2016	QCD	0
1120088	8/26/2016	QCD	0
1117682	7/15/2016	QCD	0
882612	2/21/2008	QCD	0
881296	2/21/2008	QCD	0
862276	7/17/2007	WD	\$1,978,800
419205	12/30/1991	WD	0
377041	9/26/1989	WD	\$400,000
368988	4/1/1989	PTD	0
335248	4/1/1987	WD	\$512,000
309231	12/1/1985	WD	\$1,000,000
274/355	1/1/1901	0	0

2023 Actual Value		
2120	OFFICE LAND	\$1,602,300
2220	OFFICE STRUCTURE	\$690,500
2290	SB22-238 COMMERCIAL REDUCTION	-\$30,000
Total Value:		\$2,262,800

2022 Actual Value		
2120	OFFICE LAND	\$1,547,300
2220	OFFICE STRUCTURE	\$645,300
Total Value:		\$2,192,600

















Improvement Info:	First: <b>6915</b>	FinHalf: <b>0</b>	Beds: <b>0</b>	Units: <b>0</b>	Impstyle: <b>Unknown</b>	Grade: <b>Average</b>	
Imp#:	<b>1</b>	Second: <b>5254</b>	FinBsm: <b>0</b>	Baths: <b>0.00</b>	Gar Sqft: <b>0</b>	Bsm: <b>Inground</b>	Cond: <b>Unknown</b>
YearBlt:	<b>1979</b>	Third: <b>0</b>	SFLA: <b>0</b>	Rooms: <b>0</b>	Cars: <b>0</b>	Garage: <b>Unknown</b>	Xwall: <b>WoodSiding</b>
AdjYr:	<b>1979</b>	Addr: <b>4106</b>	UnBsm: <b>6635</b>	Lofts: <b>0</b>	Heat: <b>Forced Air</b>		
Acre: <b>0.8300</b>	Access: <b>Pub Paved</b>	Topography: <b>Level</b>	View: <b>Good</b>	Tree Cover: <b>Minimal</b>			
Hydrology: <b>Creek/Pond</b>	Sewer: <b>Public Swr</b>	Water: <b>Public Wtr</b>	Utilities: <b>Elec &amp; Gas</b>	Misc: <b>ON HIGHWAY</b>			

1: Creekside Dr/Creekside Dr  
 Year 2023 Existing AM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	5	8	4	8	15	5
Future Volume (Veh/h)	5	8	4	8	15	5
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	9	4	9	16	5
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	36	18	21			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	36	18	21			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	100			
cM capacity (veh/h)	975	1060	1595			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	14	13	21			
Volume Left	5	4	0			
Volume Right	9	0	5			
cSH	1028	1595	1700			
Volume to Capacity	0.01	0.00	0.01			
Queue Length 95th (ft)	1	0	0			
Control Delay (s)	8.6	2.2	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	2.2	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			3.1			
Intersection Capacity Utilization			14.0%	ICU Level of Service	A	
Analysis Period (min)			15			

2: Creekside Dr /Creekside Dr & Business Acc. /Current Site Acc.  
 Year 2023 Existing AM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	2	1	0	0	6	12	2	0	23	0
Future Volume (Veh/h)	0	0	2	1	0	0	6	12	2	0	23	0
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	0	2	1	0	0	7	13	2	0	25	0
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	53	54	25	55	53	14	25			15		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	53	54	25	55	53	14	25			15		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	100	100	100	100	100	100	100			100		
cM capacity (veh/h)	942	834	1051	938	835	1066	1589			1603		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	2	1	22	25								
Volume Left	0	1	7	0								
Volume Right	2	0	2	0								
cSH	1051	938	1589	1603								
Volume to Capacity	0.00	0.00	0.00	0.00								
Queue Length 95th (ft)	0	0	0	0								
Control Delay (s)	8.4	8.8	2.3	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.4	8.8	2.3	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			16.1%		ICU Level of Service				A			
Analysis Period (min)			15									

3: Creekside Dr & W Main St  
 Year 2023 Existing AM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	10	210	0	1	134	8	0	0	0	9	0	14
Future Volume (vph)	10	210	0	1	134	8	0	0	0	9	0	14
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	11	228	0	1	146	9	0	0	0	10	0	15
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	239	156	0	25								
Volume Left (vph)	11	1	0	10								
Volume Right (vph)	0	9	0	15								
Hadj (s)	0.04	0.00	0.00	-0.25								
Departure Headway (s)	4.2	4.2	4.8	4.5								
Degree Utilization, x	0.28	0.18	0.00	0.03								
Capacity (veh/h)	853	843	694	726								
Control Delay (s)	8.7	8.1	7.8	7.6								
Approach Delay (s)	8.7	8.1	0.0	7.6								
Approach LOS	A	A	A	A								
Intersection Summary												
Delay			8.4									
Level of Service			A									
Intersection Capacity Utilization			27.8%	ICU Level of Service	A							
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr  
 Year 2023 Existing PM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	5	8	6	11	16	5
Future Volume (Veh/h)	5	8	6	11	16	5
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	5	9	7	12	17	5
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	46	20	22			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	46	20	22			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	100			
cM capacity (veh/h)	960	1058	1593			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	14	19	22			
Volume Left	5	7	0			
Volume Right	9	0	5			
cSH	1021	1593	1700			
Volume to Capacity	0.01	0.00	0.01			
Queue Length 95th (ft)	1	0	0			
Control Delay (s)	8.6	2.7	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	2.7	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			3.1			
Intersection Capacity Utilization			15.9%	ICU Level of Service	A	
Analysis Period (min)			15			

2: Creekside Dr /Creekside Dr & Business Acc. /Current Site Acc.  
 Year 2023 Existing PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	14	8	0	1	8	16	8	0	23	1
Future Volume (Veh/h)	0	0	14	8	0	1	8	16	8	0	23	1
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	0	15	9	0	1	9	17	9	0	25	1
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type												
								None			None	
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	66	70	26	80	66	22	26			26		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	66	70	26	80	66	22	26			26		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	100	100	99	99	100	100	99			100		
cM capacity (veh/h)	922	816	1050	891	821	1056	1588			1588		
Direction, Lane #												
	EB 1	WB 1	NB 1	SB 1								
Volume Total	15	10	35	26								
Volume Left	0	9	9	0								
Volume Right	15	1	9	1								
cSH	1050	905	1588	1588								
Volume to Capacity	0.01	0.01	0.01	0.00								
Queue Length 95th (ft)	1	1	0	0								
Control Delay (s)	8.5	9.0	1.9	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.5	9.0	1.9	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			3.3									
Intersection Capacity Utilization			22.1%		ICU Level of Service					A		
Analysis Period (min)			15									

3: Creekside Dr & W Main St  
 Year 2023 Existing PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	14	183	0	0	206	18	0	0	0	21	0	25
Future Volume (vph)	14	183	0	0	206	18	0	0	0	21	0	25
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	15	199	0	0	224	20	0	0	0	23	0	27
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	214	244	0	50								
Volume Left (vph)	15	0	0	23								
Volume Right (vph)	0	20	0	27								
Hadj (s)	0.05	-0.02	0.00	-0.20								
Departure Headway (s)	4.3	4.2	5.0	4.7								
Degree Utilization, x	0.26	0.29	0.00	0.07								
Capacity (veh/h)	815	825	658	691								
Control Delay (s)	8.8	8.9	8.0	8.0								
Approach Delay (s)	8.8	8.9	0.0	8.0								
Approach LOS	A	A	A	A								
Intersection Summary												
Delay			8.8									
Level of Service			A									
Intersection Capacity Utilization			31.2%		ICU Level of Service				A			
Analysis Period (min)			15									



















1: Creekside Dr/Creekside Dr  
 Year 2024 Background AM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	7	11	6	11	21	7
Future Volume (Veh/h)	7	11	6	11	21	7
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	12	7	12	23	8
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	53	27	31			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	53	27	31			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	100			
cM capacity (veh/h)	951	1048	1582			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	20	19	31			
Volume Left	8	7	0			
Volume Right	12	0	8			
cSH	1007	1582	1700			
Volume to Capacity	0.02	0.00	0.02			
Queue Length 95th (ft)	2	0	0			
Control Delay (s)	8.6	2.7	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.6	2.7	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			3.2			
Intersection Capacity Utilization			15.9%	ICU Level of Service	A	
Analysis Period (min)			15			

2: Creekside Dr /Creekside Dr & Business Acc. /Current Site Acc.  
 Year 2024 Background AM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	3	1	0	0	8	17	3	0	32	0
Future Volume (Veh/h)	0	0	3	1	0	0	8	17	3	0	32	0
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	0	3	1	0	0	9	18	3	0	35	0
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type												
								None			None	
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	72	74	35	76	72	20	35			21		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	72	74	35	76	72	20	35			21		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	100	100	100	100	100	100	99			100		
cM capacity (veh/h)	914	812	1038	908	813	1058	1576			1595		
Direction, Lane #												
	EB 1	WB 1	NB 1	SB 1								
Volume Total	3	1	30	35								
Volume Left	0	1	9	0								
Volume Right	3	0	3	0								
cSH	1038	908	1576	1595								
Volume to Capacity	0.00	0.00	0.01	0.00								
Queue Length 95th (ft)	0	0	0	0								
Control Delay (s)	8.5	9.0	2.2	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.5	9.0	2.2	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			18.2%		ICU Level of Service				A			
Analysis Period (min)			15									

3: Creekside Dr & W Main St  
 Year 2024 Background AM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	14	293	0	1	187	11	0	0	0	12	0	19
Future Volume (vph)	14	293	0	1	187	11	0	0	0	12	0	19
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	15	318	0	1	203	12	0	0	0	13	0	21
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	333	216	0	34								
Volume Left (vph)	15	1	0	13								
Volume Right (vph)	0	12	0	21								
Hadj (s)	0.04	0.00	0.00	-0.26								
Departure Headway (s)	4.3	4.3	5.1	4.8								
Degree Utilization, x	0.39	0.26	0.00	0.05								
Capacity (veh/h)	833	808	630	662								
Control Delay (s)	10.0	8.8	8.1	8.1								
Approach Delay (s)	10.0	8.8	0.0	8.1								
Approach LOS	A	A	A	A								
Intersection Summary												
Delay			9.4									
Level of Service			A									
Intersection Capacity Utilization			35.4%	ICU Level of Service	A							
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr  
 Year 2024 Background PM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	7	11	8	15	22	7
Future Volume (Veh/h)	7	11	8	15	22	7
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	12	9	16	24	8
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	62	28	32			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	62	28	32			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	99			
cM capacity (veh/h)	939	1047	1580			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	20	25	32			
Volume Left	8	9	0			
Volume Right	12	0	8			
cSH	1001	1580	1700			
Volume to Capacity	0.02	0.01	0.02			
Queue Length 95th (ft)	2	0	0			
Control Delay (s)	8.7	2.7	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	2.7	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			3.1			
Intersection Capacity Utilization			17.9%	ICU Level of Service	A	
Analysis Period (min)			15			

2: Creekside Dr /Creekside Dr & Business Acc. /Current Site Acc.  
 Year 2024 Background PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	19	11	0	1	11	22	11	0	32	1
Future Volume (Veh/h)	0	0	19	11	0	1	11	22	11	0	32	1
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	0	21	12	0	1	12	24	12	0	35	1
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type												
								None			None	
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	90	96	36	110	90	30	36			36		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	90	96	36	110	90	30	36			36		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	100	100	98	99	100	100	99			100		
cM capacity (veh/h)	888	788	1037	845	794	1044	1575			1575		
Direction, Lane #												
	EB 1	WB 1	NB 1	SB 1								
Volume Total	21	13	48	36								
Volume Left	0	12	12	0								
Volume Right	21	1	12	1								
cSH	1037	858	1575	1575								
Volume to Capacity	0.02	0.02	0.01	0.00								
Queue Length 95th (ft)	2	1	1	0								
Control Delay (s)	8.5	9.3	1.9	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.5	9.3	1.9	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			3.3									
Intersection Capacity Utilization			23.1%		ICU Level of Service					A		
Analysis Period (min)			15									

3: Creekside Dr & W Main St  
 Year 2024 Background PM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	19	256	0	0	287	25	0	0	0	29	0	35
Future Volume (vph)	19	256	0	0	287	25	0	0	0	29	0	35
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	21	278	0	0	312	27	0	0	0	32	0	38
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	299	339	0	70								
Volume Left (vph)	21	0	0	32								
Volume Right (vph)	0	27	0	38								
Hadj (s)	0.05	-0.01	0.00	-0.20								
Departure Headway (s)	4.5	4.4	5.4	5.1								
Degree Utilization, x	0.37	0.41	0.00	0.10								
Capacity (veh/h)	780	793	582	624								
Control Delay (s)	10.2	10.5	8.4	8.7								
Approach Delay (s)	10.2	10.5	0.0	8.7								
Approach LOS	B	B	A	A								
Intersection Summary												
Delay			10.2									
Level of Service			B									
Intersection Capacity Utilization			39.5%	ICU Level of Service	A							
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr  
 Year 2045 Background AM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	9	14	7	14	26	9
Future Volume (Veh/h)	9	14	7	14	26	9
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	10	15	8	15	28	10
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	64	33	38			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	64	33	38			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	99			
cM capacity (veh/h)	937	1041	1572			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	25	23	38			
Volume Left	10	8	0			
Volume Right	15	0	10			
cSH	996	1572	1700			
Volume to Capacity	0.03	0.01	0.02			
Queue Length 95th (ft)	2	0	0			
Control Delay (s)	8.7	2.6	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	2.6	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			3.2			
Intersection Capacity Utilization			16.9%	ICU Level of Service	A	
Analysis Period (min)			15			

2: Creekside Dr /Creekside Dr & Business Acc. /Current Site Acc.  
 Year 2045 Background AM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	4	1	0	0	10	21	4	0	40	0
Future Volume (Veh/h)	0	0	4	1	0	0	10	21	4	0	40	0
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	0	4	1	0	0	11	23	4	0	43	0
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type												
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	90	92	43	94	90	25	43			27		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	90	92	43	94	90	25	43			27		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	100	100	100	100	100	100	99			100		
cM capacity (veh/h)	890	792	1027	881	794	1051	1566			1587		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	4	1	38	43								
Volume Left	0	1	11	0								
Volume Right	4	0	4	0								
cSH	1027	881	1566	1587								
Volume to Capacity	0.00	0.00	0.01	0.00								
Queue Length 95th (ft)	0	0	1	0								
Control Delay (s)	8.5	9.1	2.2	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.5	9.1	2.2	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			1.5									
Intersection Capacity Utilization			18.6%		ICU Level of Service				A			
Analysis Period (min)			15									



3: Creekside Dr & W Main St  
 Year 2045 Background AM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	17	361	0	1	230	14	0	0	0	15	0	24
Future Volume (vph)	17	361	0	1	230	14	0	0	0	15	0	24
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	18	392	0	1	250	15	0	0	0	16	0	26
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	410	266	0	42								
Volume Left (vph)	18	1	0	16								
Volume Right (vph)	0	15	0	26								
Hadj (s)	0.04	0.00	0.00	-0.26								
Departure Headway (s)	4.3	4.4	5.5	5.1								
Degree Utilization, x	0.49	0.33	0.00	0.06								
Capacity (veh/h)	817	787	582	618								
Control Delay (s)	11.5	9.6	8.5	8.4								
Approach Delay (s)	11.5	9.6	0.0	8.4								
Approach LOS	B	A	A	A								
Intersection Summary												
Delay			10.6									
Level of Service			B									
Intersection Capacity Utilization			41.4%		ICU Level of Service				A			
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr  
 Year 2045 Background PM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	9	14	10	19	27	9
Future Volume (Veh/h)	9	14	10	19	27	9
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	10	15	11	21	29	10
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	77	34	39			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	77	34	39			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	99	99	99			
cM capacity (veh/h)	920	1039	1571			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	25	32	39			
Volume Left	10	11	0			
Volume Right	15	0	10			
cSH	988	1571	1700			
Volume to Capacity	0.03	0.01	0.02			
Queue Length 95th (ft)	2	1	0			
Control Delay (s)	8.7	2.5	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	2.5	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			3.1			
Intersection Capacity Utilization			18.2%	ICU Level of Service	A	
Analysis Period (min)			15			

















2: Creekside Dr /Creekside Dr & Business Acc. /Current Site Acc.  
 Year 2045 Background PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	0	0	24	14	0	1	14	27	14	0	40	1
Future Volume (Veh/h)	0	0	24	14	0	1	14	27	14	0	40	1
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	0	26	15	0	1	15	29	15	0	43	1
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type												
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	111	118	44	136	110	36	44			44		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	111	118	44	136	110	36	44			44		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	100	100	97	98	100	100	99			100		
cM capacity (veh/h)	860	765	1027	808	772	1036	1564			1564		
Direction, Lane #												
	EB 1	WB 1	NB 1	SB 1								
Volume Total	26	16	59	44								
Volume Left	0	15	15	0								
Volume Right	26	1	15	1								
cSH	1027	819	1564	1564								
Volume to Capacity	0.03	0.02	0.01	0.00								
Queue Length 95th (ft)	2	1	1	0								
Control Delay (s)	8.6	9.5	1.9	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.6	9.5	1.9	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			3.4									
Intersection Capacity Utilization			23.9%	ICU Level of Service						A		
Analysis Period (min)			15									

3: Creekside Dr & W Main St  
 Year 2045 Background PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	24	315	0	0	353	31	0	0	0	36	0	44
Future Volume (vph)	24	315	0	0	353	31	0	0	0	36	0	44
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	26	342	0	0	384	34	0	0	0	39	0	48
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	368	418	0	87								
Volume Left (vph)	26	0	0	39								
Volume Right (vph)	0	34	0	48								
Hadj (s)	0.05	-0.01	0.00	-0.21								
Departure Headway (s)	4.7	4.6	5.9	5.5								
Degree Utilization, x	0.48	0.53	0.00	0.13								
Capacity (veh/h)	751	768	523	577								
Control Delay (s)	11.8	12.6	8.9	9.3								
Approach Delay (s)	11.8	12.6	0.0	9.3								
Approach LOS	B	B	A	A								
Intersection Summary												
Delay			11.9									
Level of Service			B									
Intersection Capacity Utilization			47.7%		ICU Level of Service				A			
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr & Streamside Lane /Future Site Acc.  
 Year 2024 Total AM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	0	11	35	0	0	6	11	13	0	21	7
Future Volume (Veh/h)	7	0	11	35	0	0	6	11	13	0	21	7
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	0	12	38	0	0	7	12	14	0	23	8
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type												
Median storage veh												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	60	67	27	72	64	19	31			26		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	60	67	27	72	64	19	31			26		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	99	100	99	96	100	100	100			100		
cM capacity (veh/h)	933	820	1048	905	823	1059	1582			1588		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	20	38	33	31								
Volume Left	8	38	7	0								
Volume Right	12	0	14	8								
cSH	999	905	1582	1588								
Volume to Capacity	0.02	0.04	0.00	0.00								
Queue Length 95th (ft)	2	3	0	0								
Control Delay (s)	8.7	9.1	1.6	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.7	9.1	1.6	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			4.7									
Intersection Capacity Utilization			17.8%		ICU Level of Service				A			
Analysis Period (min)			15									

2: Creekside Dr /Creekside Dr & Business Acc.  
 Year 2024 Total AM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	3	8	30	67	0
Future Volume (Veh/h)	0	3	8	30	67	0
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	3	9	33	73	0
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	124	73	73			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	124	73	73			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	100	99			
cM capacity (veh/h)	866	989	1527			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	3	42	73			
Volume Left	0	9	0			
Volume Right	3	0	0			
cSH	989	1527	1700			
Volume to Capacity	0.00	0.01	0.04			
Queue Length 95th (ft)	0	0	0			
Control Delay (s)	8.7	1.6	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	1.6	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			0.8			
Intersection Capacity Utilization			18.5%	ICU Level of Service	A	
Analysis Period (min)			15			

3: Creekside Dr & W Main St  
 Year 2024 Total AM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	18	293	0	1	187	20	0	0	0	37	0	30
Future Volume (vph)	18	293	0	1	187	20	0	0	0	37	0	30
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	20	318	0	1	203	22	0	0	0	40	0	33
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	338	226	0	73								
Volume Left (vph)	20	1	0	40								
Volume Right (vph)	0	22	0	33								
Hadj (s)	0.05	-0.02	0.00	-0.13								
Departure Headway (s)	4.4	4.4	5.3	5.0								
Degree Utilization, x	0.41	0.28	0.00	0.10								
Capacity (veh/h)	803	782	608	641								
Control Delay (s)	10.4	9.1	8.3	8.6								
Approach Delay (s)	10.4	9.1	0.0	8.6								
Approach LOS	B	A	A	A								
Intersection Summary												
Delay			9.8									
Level of Service			A									
Intersection Capacity Utilization			38.9%	ICU Level of Service	A							
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr & Streamside Lane /Future Site Acc.  
 Year 2024 Total PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	7	0	11	24	0	0	8	15	35	0	22	7
Future Volume (Veh/h)	7	0	11	24	0	0	8	15	35	0	22	7
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	8	0	12	26	0	0	9	16	38	0	24	8
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	81	100	28	93	85	35	32			54		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	81	100	28	93	85	35	32			54		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	99	100	99	97	100	100	99			100		
cM capacity (veh/h)	903	786	1047	877	801	1038	1580			1551		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	20	26	63	32								
Volume Left	8	26	9	0								
Volume Right	12	0	38	8								
cSH	984	877	1580	1551								
Volume to Capacity	0.02	0.03	0.01	0.00								
Queue Length 95th (ft)	2	2	0	0								
Control Delay (s)	8.7	9.2	1.1	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.7	9.2	1.1	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			3.4									
Intersection Capacity Utilization			20.0%		ICU Level of Service				A			
Analysis Period (min)			15									



















2: Creekside Dr /Creekside Dr & Business Acc.  
 Year 2024 Total PM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	19	11	57	56	1
Future Volume (Veh/h)	0	19	11	57	56	1
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	21	12	62	61	1
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	148	62	62			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	148	62	62			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	98	99			
cM capacity (veh/h)	838	1004	1541			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	21	74	62			
Volume Left	0	12	0			
Volume Right	21	0	1			
cSH	1004	1541	1700			
Volume to Capacity	0.02	0.01	0.04			
Queue Length 95th (ft)	2	1	0			
Control Delay (s)	8.7	1.2	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	1.2	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			1.7			
Intersection Capacity Utilization			20.3%	ICU Level of Service	A	
Analysis Period (min)			15			

3: Creekside Dr & W Main St  
 Year 2024 Total PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	30	256	0	0	287	50	0	0	0	46	0	42
Future Volume (vph)	30	256	0	0	287	50	0	0	0	46	0	42
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	33	278	0	0	312	54	0	0	0	50	0	46
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	311	366	0	96								
Volume Left (vph)	33	0	0	50								
Volume Right (vph)	0	54	0	46								
Hadj (s)	0.06	-0.05	0.00	-0.15								
Departure Headway (s)	4.6	4.5	5.6	5.3								
Degree Utilization, x	0.40	0.45	0.00	0.14								
Capacity (veh/h)	755	780	557	606								
Control Delay (s)	10.7	11.1	8.6	9.1								
Approach Delay (s)	10.7	11.1	0.0	9.1								
Approach LOS	B	B	A	A								
Intersection Summary												
Delay			10.7									
Level of Service			B									
Intersection Capacity Utilization			48.4%	ICU Level of Service	A							
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr & Streamside Lane /Future Site Acc.  
 Year 2045 Total AM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	9	0	14	35	0	0	7	14	13	0	26	9
Future Volume (Veh/h)	9	0	14	35	0	0	7	14	13	0	26	9
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	10	0	15	38	0	0	8	15	14	0	28	10
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	71	78	33	86	76	22	38			29		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	71	78	33	86	76	22	38			29		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	99	100	99	96	100	100	99			100		
cM capacity (veh/h)	917	808	1041	884	810	1055	1572			1584		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	25	38	37	38								
Volume Left	10	38	8	0								
Volume Right	15	0	14	10								
cSH	987	884	1572	1584								
Volume to Capacity	0.03	0.04	0.01	0.00								
Queue Length 95th (ft)	2	3	0	0								
Control Delay (s)	8.7	9.3	1.6	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.7	9.3	1.6	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			4.6									
Intersection Capacity Utilization			18.8%		ICU Level of Service				A			
Analysis Period (min)			15									

2: Creekside Dr /Creekside Dr & Business Acc.  
 Year 2045 Total AM.syn



















Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	4	10	34	75	0
Future Volume (Veh/h)	0	4	10	34	75	0
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	4	11	37	82	0
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	141	82	82			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	141	82	82			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	100	99			
cM capacity (veh/h)	846	978	1515			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	4	48	82			
Volume Left	0	11	0			
Volume Right	4	0	0			
cSH	978	1515	1700			
Volume to Capacity	0.00	0.01	0.05			
Queue Length 95th (ft)	0	1	0			
Control Delay (s)	8.7	1.7	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	1.7	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			0.9			
Intersection Capacity Utilization			19.0%	ICU Level of Service	A	
Analysis Period (min)			15			

3: Creekside Dr & W Main St  
 Year 2045 Total AM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	21	361	0	1	230	23	0	0	0	40	0	35
Future Volume (vph)	21	361	0	1	230	23	0	0	0	40	0	35
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	23	392	0	1	250	25	0	0	0	43	0	38
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	415	276	0	81								
Volume Left (vph)	23	1	0	43								
Volume Right (vph)	0	25	0	38								
Hadj (s)	0.05	-0.02	0.00	-0.14								
Departure Headway (s)	4.5	4.6	5.6	5.3								
Degree Utilization, x	0.52	0.35	0.00	0.12								
Capacity (veh/h)	787	761	559	600								
Control Delay (s)	12.2	10.0	8.6	9.0								
Approach Delay (s)	12.2	10.0	0.0	9.0								
Approach LOS	B	A	A	A								
Intersection Summary												
Delay			11.1									
Level of Service			B									
Intersection Capacity Utilization			45.5%		ICU Level of Service	A						
Analysis Period (min)			15									

1: Creekside Dr/Creekside Dr & Streamside Lane /Future Site Acc.

Year 2045 Total PM.syn

















												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	9	0	14	24	0	0	10	19	35	0	27	9
Future Volume (Veh/h)	9	0	14	24	0	0	10	19	35	0	27	9
Sign Control		Stop			Stop			Free			Free	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	10	0	15	26	0	0	11	21	38	0	29	10
Pedestrians												
Lane Width (ft)												
Walking Speed (ft/s)												
Percent Blockage												
Right turn flare (veh)												
Median type								None			None	
Median storage (veh)												
Upstream signal (ft)												
pX, platoon unblocked												
vC, conflicting volume	96	115	34	111	101	40	39			59		
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	96	115	34	111	101	40	39			59		
tC, single (s)	7.1	6.5	6.2	7.1	6.5	6.2	4.1			4.1		
tC, 2 stage (s)												
tF (s)	3.5	4.0	3.3	3.5	4.0	3.3	2.2			2.2		
p0 queue free %	99	100	99	97	100	100	99			100		
cM capacity (veh/h)	882	770	1039	850	784	1031	1571			1545		
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	25	26	70	39								
Volume Left	10	26	11	0								
Volume Right	15	0	38	10								
cSH	970	850	1571	1545								
Volume to Capacity	0.03	0.03	0.01	0.00								
Queue Length 95th (ft)	2	2	1	0								
Control Delay (s)	8.8	9.4	1.2	0.0								
Lane LOS	A	A	A									
Approach Delay (s)	8.8	9.4	1.2	0.0								
Approach LOS	A	A										
Intersection Summary												
Average Delay			3.4									
Intersection Capacity Utilization			20.4%		ICU Level of Service				A			
Analysis Period (min)			15									

2: Creekside Dr /Creekside Dr & Business Acc.  
 Year 2045 Total PM.syn



Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	0	24	14	62	64	1
Future Volume (Veh/h)	0	24	14	62	64	1
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	0	26	15	67	70	1
<b>Pedestrians</b>						
Lane Width (ft)						
Walking Speed (ft/s)						
Percent Blockage						
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	168	70	71			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	168	70	71			
tC, single (s)	6.4	6.2	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	100	97	99			
cM capacity (veh/h)	815	992	1529			
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>NB 1</b>	<b>SB 1</b>			
Volume Total	26	82	71			
Volume Left	0	15	0			
Volume Right	26	0	1			
cSH	992	1529	1700			
Volume to Capacity	0.03	0.01	0.04			
Queue Length 95th (ft)	2	1	0			
Control Delay (s)	8.7	1.4	0.0			
Lane LOS	A	A				
Approach Delay (s)	8.7	1.4	0.0			
Approach LOS	A					
<b>Intersection Summary</b>						
Average Delay			1.9			
Intersection Capacity Utilization			20.7%	ICU Level of Service	A	
Analysis Period (min)			15			

3: Creekside Dr & W Main St  
 Year 2045 Total PM.syn

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Sign Control		Stop			Stop			Stop			Stop	
Traffic Volume (vph)	35	315	0	0	353	56	0	0	0	53	0	51
Future Volume (vph)	35	315	0	0	353	56	0	0	0	53	0	51
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	38	342	0	0	384	61	0	0	0	58	0	55
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total (vph)	380	445	0	113								
Volume Left (vph)	38	0	0	58								
Volume Right (vph)	0	61	0	55								
Hadj (s)	0.05	-0.05	0.00	-0.16								
Departure Headway (s)	4.8	4.7	6.1	5.6								
Degree Utilization, x	0.51	0.58	0.00	0.18								
Capacity (veh/h)	719	753	496	562								
Control Delay (s)	12.7	13.8	9.1	9.8								
Approach Delay (s)	12.7	13.8	0.0	9.8								
Approach LOS	B	B	A	A								
Intersection Summary												
Delay			12.9									
Level of Service			B									
Intersection Capacity Utilization			56.6%	ICU Level of Service	B							
Analysis Period (min)			15									